

# CITY OF PINOLE

# THREE CORRIDORS SPECIFIC PLAN

**NOVEMBER 2010** 

**UPDATED JUNE 2020** 



# CITY OF PINOLE

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NOVEMBER 2010

ADOPTED NOVEMBER 16, 2010 BY ORDINANCE #2010-02

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### **ACKNOWLEDGEMENTS**

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# 1.0 INTRODUCTION AND BACKGROUND

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# CHAPTER 1 – INTRODUCTION AND BACKGROUND

#### 1.1 PLAN PURPOSE AND INTENT

#### WHAT IS A SPECIFIC PLAN?

A Specific Plan is a regulatory and economic development tool that local governments use to implement their General Plan and to guide development in a well-defined sub-area of the community. While the General Plan is the primary guide for growth and development in a community, a Specific Plan is able to focus on the unique characteristics of a special area by customizing the planning process and land use regulations for that area. A Specific Plan is prepared pursuant to the procedures set forth in Article 8, Section 65450 to 65457 of the California Government Code, and implements the General Plan by further refining the objectives for the Project Area.

#### **PURPOSE AND INTENT**

The purpose of the Specific Plan is to facilitate revitalization of the San Pablo Avenue, the Pinole Valley Road, and the Appian Way commercial corridors. The City of Pinole has designated these corridors as Priority Development Areas (PDAs) through FOCUS, a regional development and conservation strategy that promotes more compact land use patterns for the Bay Area. PDAs are locally identified, infill development opportunity areas where there is local commitment to developing more housing along with facilities and services to meet the day-to-day needs of residents in a pedestrian-friendly environment served by transit. This Specific Plan will be used as a regulatory and economic development tool that the City of Pinole can use to guide development along all three corridors to take advantage of these underutilized properties and other assets.

While the General Plan is the primary guide for growth and development within Pinole, this Specific Plan seeks to establish a direct connection between the City of Pinole's General Plan and economic and revitalization opportunities within the three Specific Plan corridors. An overall goal is the orderly development of three project area(s) – San Pablo Avenue, Pinole Valley Road, and Appian Way –consistent with the City's General Plan and, more specifically, with the community's vision developed through the Specific Plan public outreach process.

The Specific Plan will work to realize the community's vision to build upon the following:

San Pablo Avenue's diverse history as a major thoroughfare in the East Bay, home to important
industrial and light industrial land use, and community aims of attracting new retail and service
industry, while achieving more diverse residential development that can be served by transit.

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- Pinole Valley Road's history as a shopping and service corridor, attracting new retail, medical facilities, and higher-density residential development, while simultaneously improving open space access, enhancing Pinole Creek, and improving automobile flow and pedestrian and bicycle circulation.
- Appian Way's history as a large-scale shopping, medical care, and service corridor, attracting new retail and higher-density residential development, while simultaneously improving open space access, automobile flow, and pedestrian and bicycle circulation.

Together, the Specific Plan for these corridors will preserve the character of Pinole, while:

- Supporting commercial and residential developments that can function as the catalyst for economic revitalization and further the City's goals and objectives;
- Enhancing Old Town Pinole as a vibrant, pedestrian-oriented commercial destination with a strong civic identity;
- Encouraging Transit Oriented Development (TOD) within the Priority Development Areas (PDA) on San Pablo Avenue, Pinole Valley Road, and Appian Way;
- Supporting economic development that will bring more housing, retail, and employment opportunities to the community.

#### **BACKGROUND**

Located in the San Francisco Bay Area and 20 miles northeast of San Francisco, the City of Pinole is situated on the shores of San Pablo Bay in west Contra Costa County (see Figure 1.1). It is well connected to the cities in central Contra Costa County (Martinez, Concord, and Pleasant Hill) and the rest of the Bay Area (San Francisco and Oakland) by State Route 4 and Interstate 80 (I-80). Pinole is close to major employment centers in Oakland and San Francisco via the I-80 freeway. Under good traffic conditions, the drive to either Oakland or San Francisco takes approximately 20 minutes, making Pinole a desirable community to live and raise a family in. Similarly, Oakland International Airport and the Port of Oakland are a 30- and 25-minute drive respectively from the heart of Pinole, providing easy access to international destinations for retail customers, suppliers, and leisure travelers. Although Pinole is not connected to the rest of the Bay Area via BART or Bay Area Ferry Service, it does benefit from regional public transit provided by WestCAT and AC Transit.

#### 1

## CHAPTER 1 – INTRODUCTION AND BACKGROUND

Communities surrounding Pinole include the unincorporated areas of Montalvin Manor, Bayview, Tara Hills, and El Sobrante and the cities of San Pablo, Richmond, and Hercules. The City of Pinole is largely built out with development opportunities only for infill housing and commercial development.

#### **SPECIFIC PLAN BOUNDARIES**

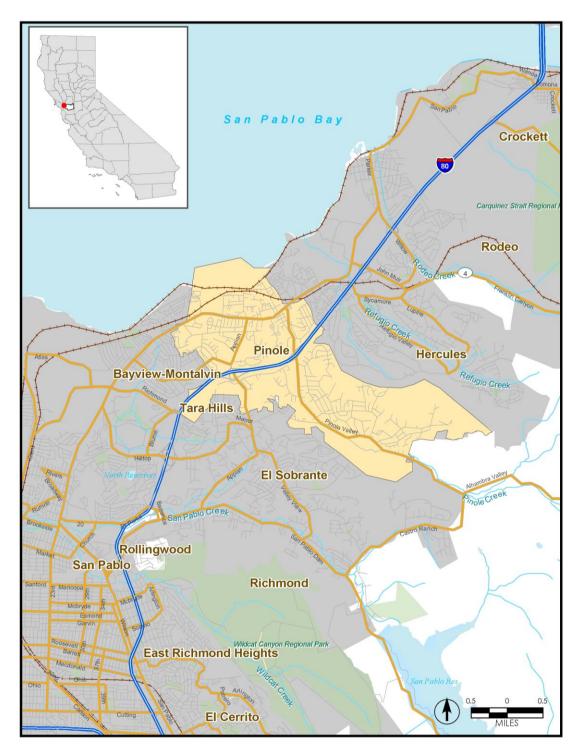
The Specific Plan Area contains the San Pablo Avenue, Pinole Valley Road, and Appian Way corridors (see Figure 1.2A). The San Pablo Avenue Specific Plan Area extends north and south along the San Pablo Avenue corridor between Dursey Drive to the west and the Burlington Northern Santa Fe rail line to the east. San Pablo Avenue is a Route of Regional Significance that has been designated as a Priority Development Area. As such, this corridor contains unique circulation challenges, areas for economic development and revitalization, and areas that would benefit from more activity generated by higher-density residential development (see Figure 1.2B).

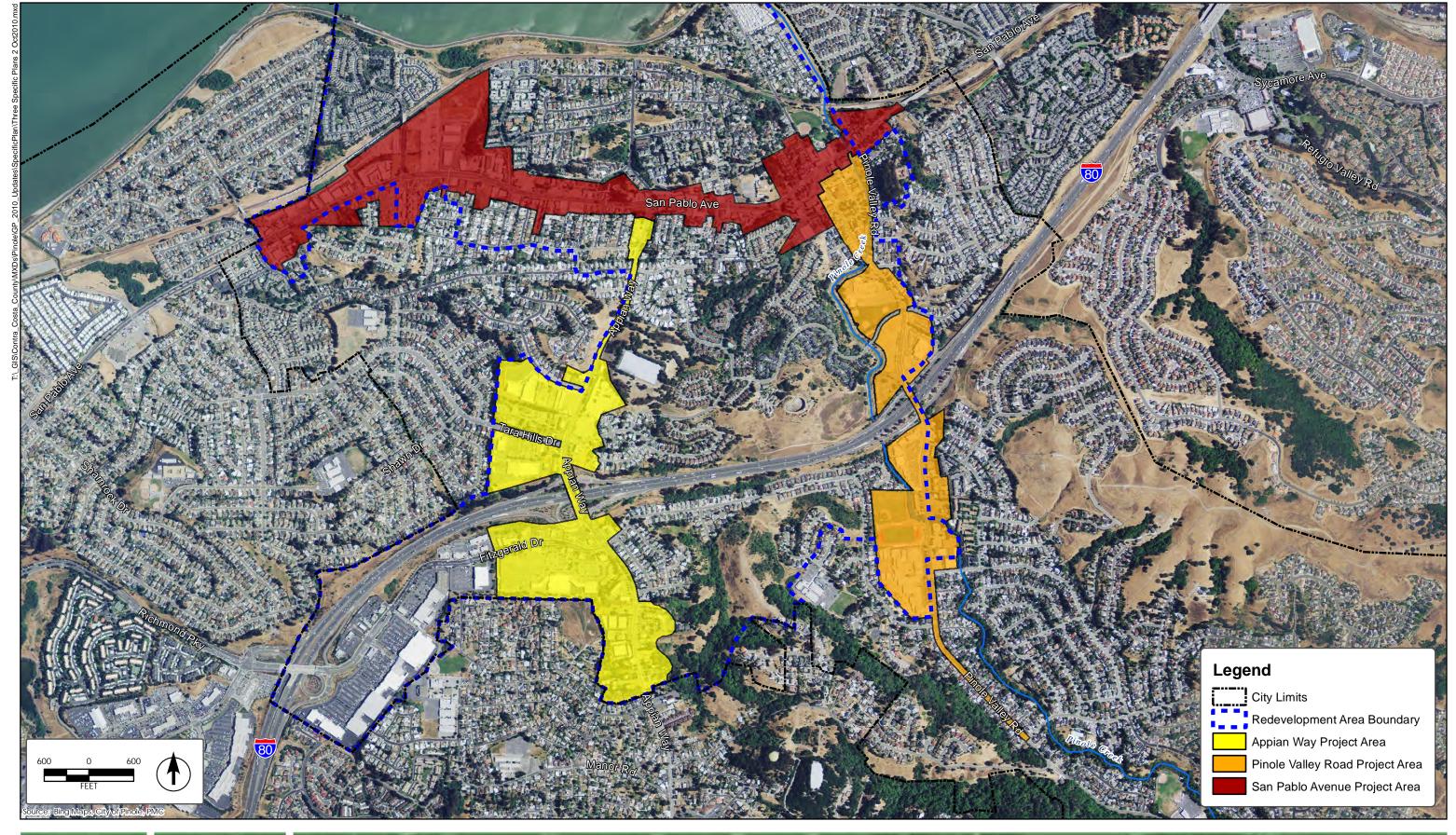
The Pinole Valley Road Specific Plan Area extends east and west along the Pinole Valley Road corridor between San Pablo Avenue to the north and Simas Avenue to the south. The Pinole Valley Road corridor contains diverse civic, commercial, residential, recreational, and public and private school uses. Recent renovation of the Pinole Valley Shopping Center and the opening of the Kaiser Medical Office Building will revitalize this corridor. To optimize land use in the corridor, the City will have to address unique circulation challenges in order to capitalize on areas of economic opportunity and revitalization and to promote diverse residential development (see **Figure 1.2C**).

The Appian Way Specific Plan Area extends east and west along the Appian Way corridor between San Pablo Avenue to the north and the City of Pinole boundary to the south. The Appian Way corridor contains the now vacant Doctors Medical Center facility, regional-serving retail and service uses, and presents unique circulation challenges, areas of economic opportunity and revitalization, and areas that can accommodate increased residential development (see **Figure 1.2D**).

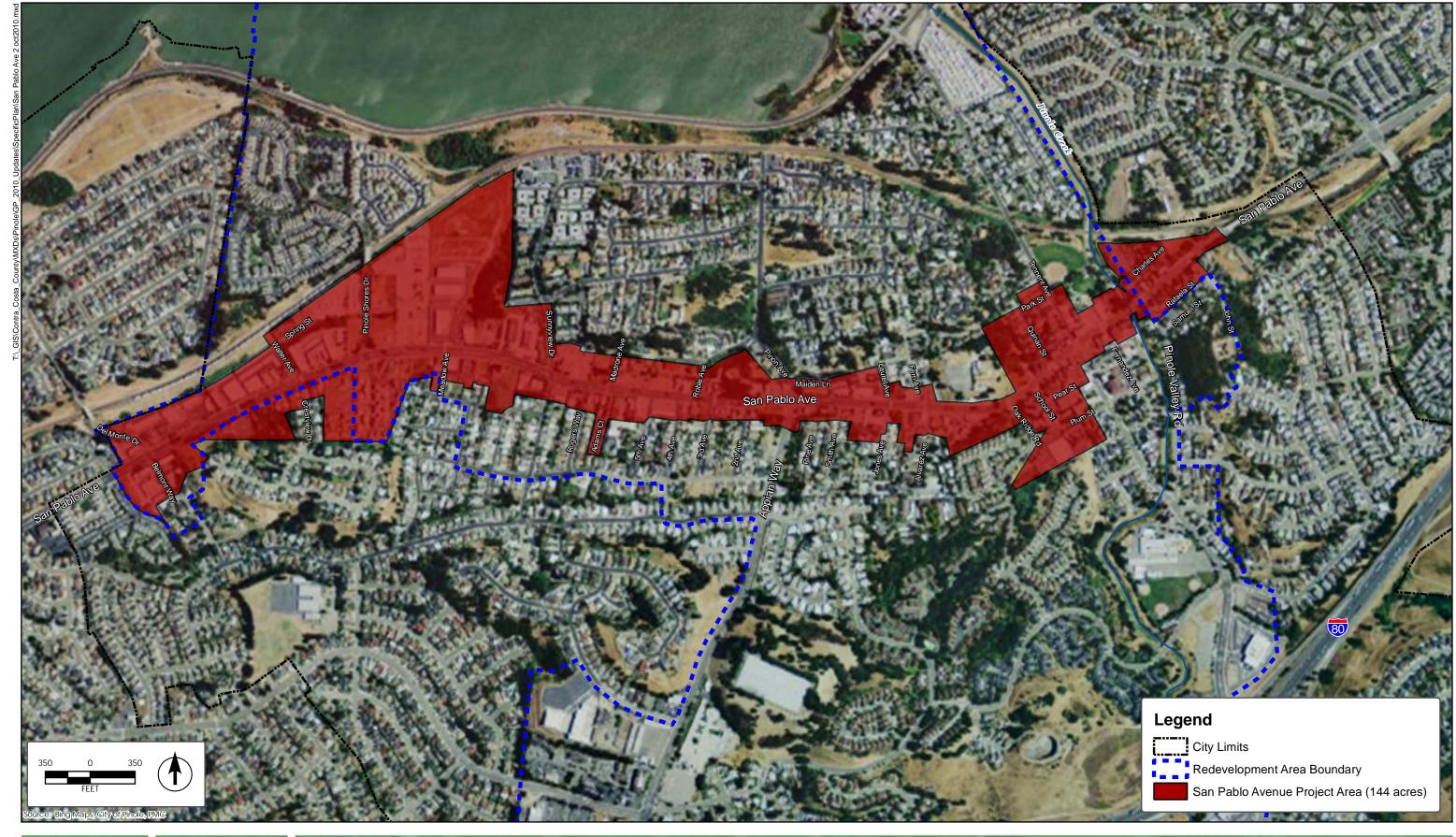
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Figure 1.1 Regional Location Map

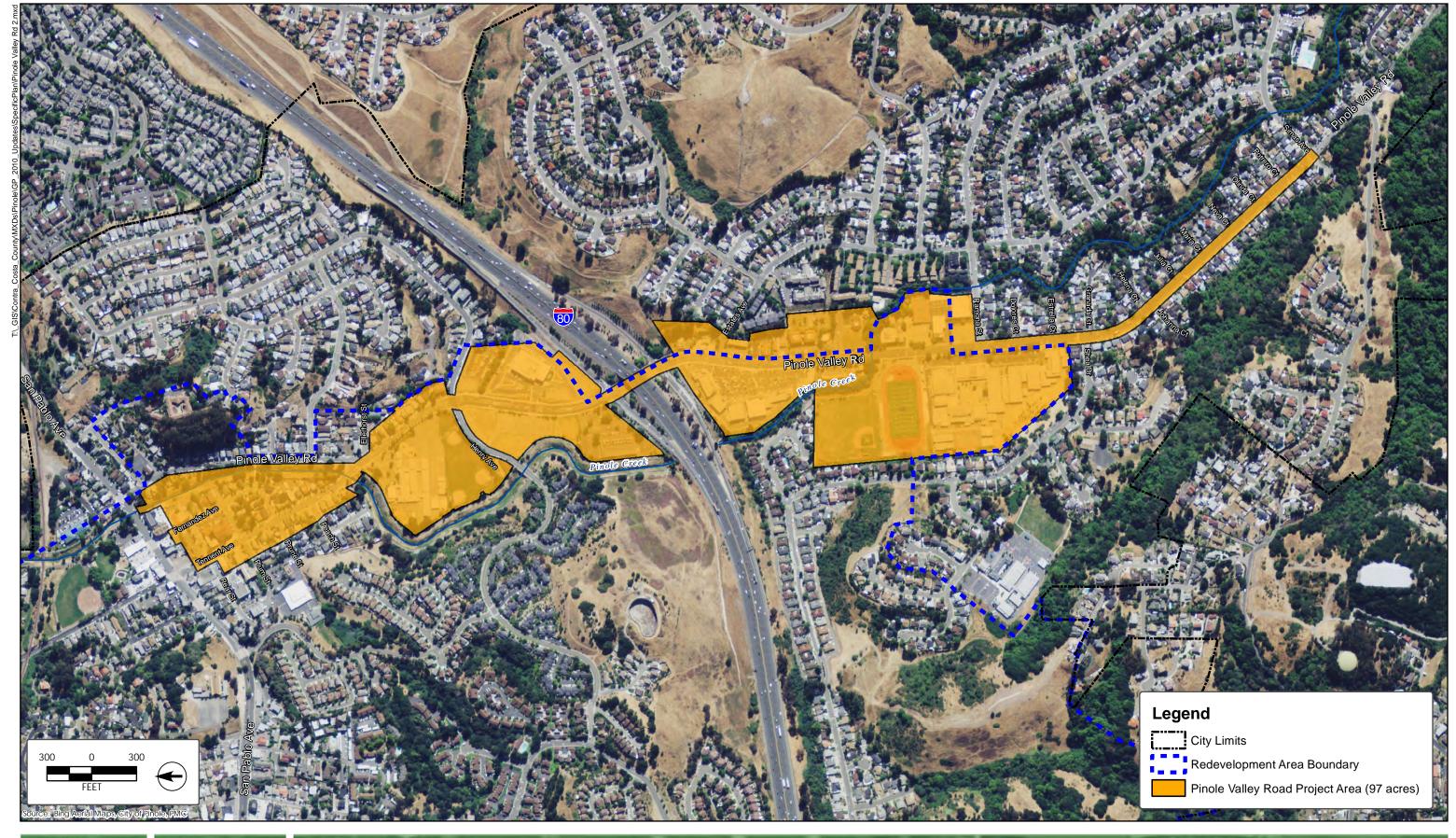




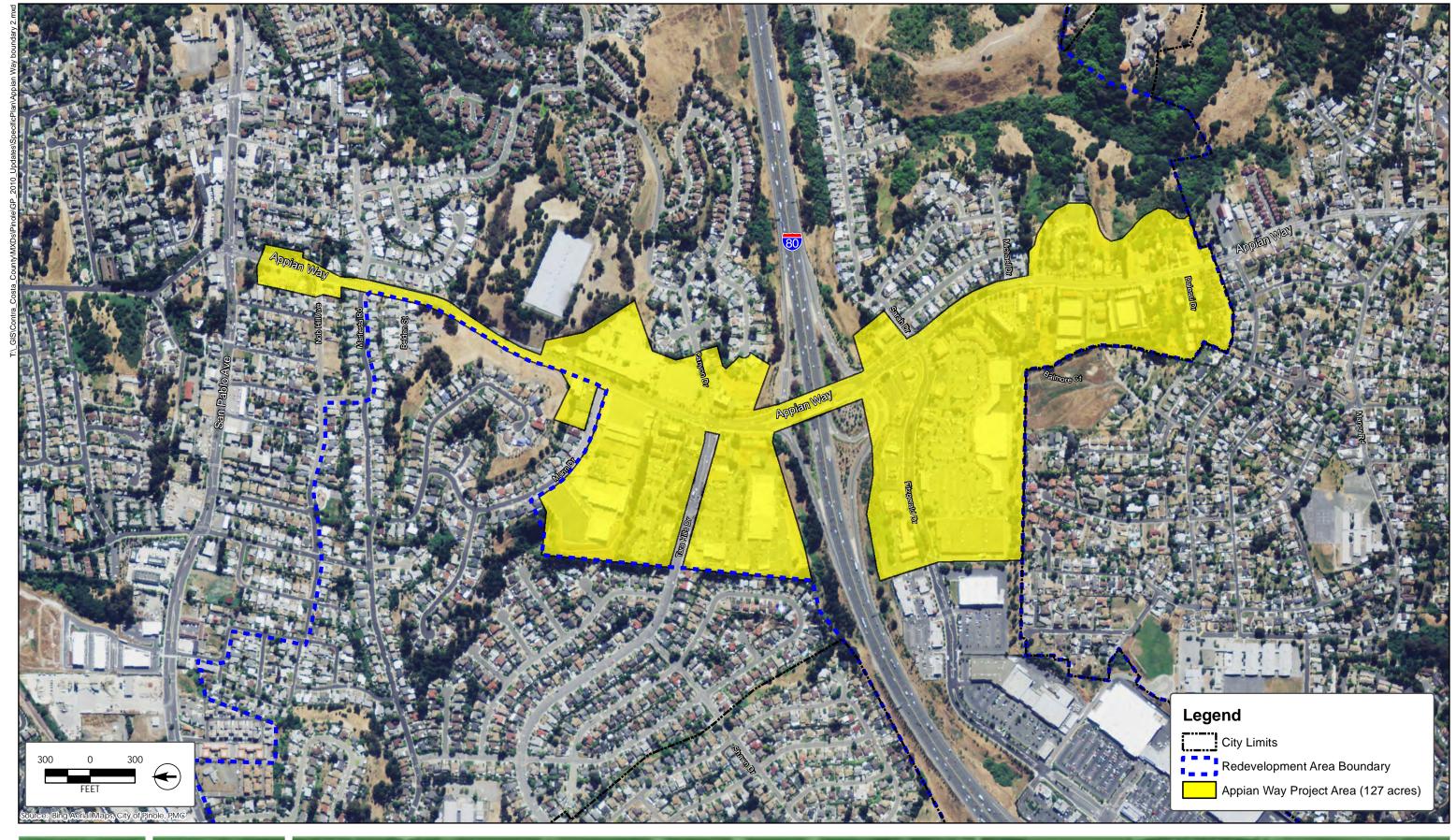












# CHAPTER 1 – INTRODUCTION AND BACKGROUND

#### 1.2 PLAN ORGANIZATION AND USE

Though the project area contains three distinct corridors, each of which has a unique character, they are combined into one Specific Plan to ensure coordinated land use planning where they intersect, efficiently communicate common standards and provisions, and ensure consistency in planning for these three areas. Portions of this document include provisions (e.g., vision, land plans, sub-area frameworks) that are unique to one of the corridors. Except as explicitly stated, all other provisions in this document apply to all three corridors. The document is organized into ten chapters as described below:

**Executive Summary:** This chapter is a general overview of the Specific Plan's content and intent.

- **1.0 Introduction & Background:** Chapter 1.0 discusses the purpose, intent, and organization of this document. It describes the project area boundaries and outlines this plan's relationship to the City's General Plan and other documents relevant to the Specific Plan project areas.
- **2.0 Vision:** This chapter describes the community's vision for the three project area(s) San Pablo Avenue, Pinole Valley Road and Appian Way and the guiding urban design principles that were created through the public participation process.
- **3.0 Existing Conditions:** Chapter 3.0 summarizes relevant conditions existing within the study areas on topics such as existing circulation issues, urban design conditions, land uses, historical resources, and economic conditions. Additionally, this chapter establishes sub-area plans for each of the three Specific Plan Areas.
- **4.0 Economic Development Strategy:** Chapter 4.0 summarizes economic and demographic trends in Pinole and identifies development standards and economic development tools for the Specific Plan Areas, Old Town, and opportunity sites.
- **5.0 Circulation:** Chapter 5.0 describes and provides solutions for mobility and safety such as truck traffic, movement of vehicles, traffic calming, intersection treatments, pedestrian circulation, bicycle circulation, and public transit viability.
- **6.0 Land Use Standards:** This chapter establishes land use plans, allowed use regulations, and site development standards (e.g., height, setback, and parking) for each of the three Specific Plan Areas.

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- **7.0 Private Realm Standards and Design Guidelines:** This chapter provides direction for private property such as guidance for site planning and design, parking lot design and screening, building design and architectural character, building massing and articulation, lighting, screening utilitarian aspects of buildings, landscape and hardscape, and signage.
- **8.0 Public Realm Standards and Design Guidelines**: This section provides direction for public improvements for the entire plan area. Topics include: Gateways and Corridor identity, parking lot design, and streetscape treatments such as medians, street trees, lighting, street furnishings, crosswalks, public art, and public right-of-way landscaping.
- **9.0 Infrastructure and Public Facilities**: This chapter summarizes the conditions of the infrastructure and public facilities in the Specific Plan Area. Subjects addressed include: Water supply, sewer and wastewater, solid waste, drainage, police and fire protection, schools, and parks.
- **10.0 Implementation**: This chapter explains Specific Plan administration, recommends strategic public improvements, and identifies key catalyst projects and programs needed to make the Specific Plan vision a reality. Priorities, phasing, potential funding mechanisms, persons responsible for implementation items, and administration of the Specific Plan document are all defined by this chapter. Also discussed are investment incentives, retail recruitment strategies, and business organization programs for creating and maintaining a vibrant Pinole Valley Road retail and service environment.

**Appendix:** The appendix provides a timeline of project events, a summary of raw feedback from public workshops, an overview of data provided by Bay Area Economics consultants, and a glossary of terms related to the Specific Plan.

The Specific Plan for San Pablo Avenue, Pinole Valley Road, and Appian Way should be used by readers to gain a clear understanding of the community vision for development along the three corridors. This document will be used by property owners, governing bodies, and City staff as a set of regulations containing area-specific land use regulations, development standards, and design guidelines. The Specific Plan establishes the framework for future capital improvement projects. These actions will improve the quality of the City's environment, as well as the clarity and effectiveness of the design review process.

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# 1.3 RELATIONSHIP TO OTHER DOCUMENTS AND GENERAL PLAN COMPLIANCE

The Specific Plan for San Pablo Avenue, Pinole Valley Road, and Appian Way must be compatible with the Pinole General Plan and the Pinole Redevelopment Plan and will be implemented in conjunction with the Pinole Municipal Code. The Specific Plan provides information needed to guide development within the planning area. Future development proposals within the Specific Plan Area must be consistent with the relevant provisions in this document.

#### THE CITY OF PINOLE GENERAL PLAN

The Specific Plan for San Pablo Avenue, Pinole Valley Road and Appian Way is consistent with the City's General Plan adopted in 2010. The Pinole General Plan includes the vision, goals, and policies and implementation actions relevant to the three Specific Plan Areas addressed in this document. The General Plan provisions are listed below:

#### **General Plan Vision**

The Overall Vision is to "Build a City" that is a vibrant destination place in the region and intentionally seeks change in land use for profound improvement in the City. It also expresses a vision for the future where "sustainable development" and land use practices provide for the needs of existing residents and businesses while preserving choices for future generations.

The Land Use Vision is for a series of walkable neighborhoods, that facilitates interactions between residents, employees, and visitors. The City will provide a balanced mix of housing, jobs, commercial activities, and services, as well as cultural and civic amenities. Pinole will foster an integrated mix of uses that are connected by lively streets and contiguous open space areas.

The Community Character Vision and goal is to preserve, enhance, and strengthen community identity. The City will preserve its small-town feel, define itself as a memorable destination with a range of entertainment and recreation opportunities, and promote connections between neighborhood and commercial districts. Pinole will maintain and celebrate its rich cultural heritage and diversity through cultural art and preservation of locally significant sites, buildings, and history.

### CHAPTER 1 - INTRODUCTION AND BACKGROUND

#### General Plan Goals, Policies, and Actions

**COMMUNITY CHARACTER** Maintain Pinole's unique qualities and sense of place to preserve

**GOAL CC.1** the established historic and small town character of the City.

**COMMUNITY CHARACTER** GOAL CC.2 Emphasize and enhance the visual and physical connection between the City's natural environment and the community's quality of life.

COMMUNITY CHARACTER Support the development of local-oriented services in Old Town

**GOAL CC.3** Pinole and encourage and support the local economy.

In addition to the above-listed items, the Overall Vision and Land Plan herein implement smart growth principles for land use, circulation, and transit concepts for the three planning areas - San Pablo Avenue, Pinole Valley Road, and Appian Way – in the Land Use Element. The General Plan directs the creation of a Specific Plan for the San Pablo Avenue, Pinole Valley Road, and Appian Way corridors.

If the Specific Plan conflicts with any of the requirements of the following related documents, the Specific Plan provisions will take precedence.

#### GENERAL PLAN ENVIRONMENTAL COMPLIANCE

Environmental review for this Specific Plan document has been prepared and certified as part of the Environmental Impact Report prepared for the Pinole General Plan. All projections of this Specific Plan respect the thresholds set forth in the General Plan.

#### THE CITY OF PINOLE ZONING ORDINANCE (PMC TITLE 17)

This Specific Plan document customizes many of the standards and regulations found in the City Zoning Ordinance to help achieve the vision for the three Specific Plan Areas – San Pablo Avenue, Pinole Valley Road, and Appian Way. In any instance where the Specific Plan conflicts with the requirements of the City of Pinole's Zoning Ordinance, the Specific Plan provisions will take precedence (e.g., allowed uses, setbacks). Where the Specific Plan is silent on a topic, the Pinole Municipal Code requirements remain in force (e.g., allowable sign area).

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#### CITY OF PINOLE RESIDENTIAL DESIGN GUIDELINES

The Citywide Residential Design Guidelines supplement the Zoning Code development standards with more detailed provisions that direct the look and feel of residential development. The guidelines and standards govern the site and architectural character of residential development.

2.0 VISION

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### 2.1 COMMUNITY OUTREACH PROCESS

Public participation has shaped the Specific Plan for the three project corridors. The following describes community participation and input.

#### **WALKING TOUR**

The consultant team and City staff conducted a walking tour to:

- Identify key land use issues;
- Identify underutilized sites and vacant properties that provided opportunities for revitalizing land use and improving the character of the three corridors; and
- Discuss ways to improve the three project corridors.

The opportunity and constraints discussion is a snapshot of the conditions and initial analysis of potential improvements (See Chapter 3: Existing Conditions) that resulted from this site walk.

### PUBLIC WORKSHOP #1 - NOVEMBER 29, 2007

The first community workshop was attended by approximately 20 community members who identified values, visions, challenges, and opportunities that would be used to steer land use consideration for the Specific Plan Areas. These included:

- Desire for more entertainment and dining options,
- A preference for a blend of single-family housing and mixed-use development.
- Interest in strengthening San Pablo Avenue as "the City's cultural, civic, and historic core;
- Enhancing Pinole Valley Road as the "gateway" into Pinole;
- Desire for an improved pedestrian experience in commercial areas with more landscaping and façade improvements along individual properties; and
- Interest in strong and consistent public improvements (e.g. street lighting and street trees).





### PRESENTATION TO PLANNING COMMISSION/STEERING COMMITTEE AND PUBLIC - MARCH 10, 2008

After reviewing background information, opportunities and constraints analysis, initial vision statements, and urban design goals, the Planning Commission/Steering Committee and public provided input that was incorporated into the alternatives.



#### PUBLIC WORKSHOP #2 - JUNE 5, 2008

Economic analysis and two vision alternatives for each Specific Plan corridor were presented to participants who also participated in land use exercises related to community goals. While the concepts and visions were strongly supported, participants emphasized the need for activities targeting youth and families.

### WORKING SESSION WITH CITY STAFF, PLANNING COMMISSION/STEERING COMMITTEE, AND PUBLIC – JUNE 25, 2008

The Planning Commission/Steering Committee and members of the public reviewed community feedback from the June 5

workshop and discussed/refined alternatives and selected a preferred land use alternative for each study corridor. The preferred land use maps for the three Specific Plan Areas are shown in sections 6.5, 6.6, and 6.7.

#### SPECIFIC PLAN INITIATION - NOVEMBER 29, 2007

Nineteen people attended a public workshop to initiate the Specific Plan preparation. At the workshop, the community was able to:

- Confirm the desired project boundaries;
- Describe their vision for the project area;
- Identify desirable and undesirable land uses; and
- Discuss opportunities and challenges for the Specific Plan to address.

# PRESENTATIONS TO PLANNING COMMISSION/STEERING COMMITTEE, AND PUBLIC – JULY 14 AND AUGUST 11, 2008

Refined land use alternatives were presented to the Planning Commission/Steering Committee in response to comments on the following topics:

- Potential parking solutions
- Specific Plan Area development procedures and administration
- Confirmation of the preferred land use alternatives

# Presentation to the Planning Commission/Steering Committee, and Public – December 11, 2008

Between 40 and 50 people attended this community meeting to discuss the major demographic and economic trends for Pinole as identified by BAE. This meeting also provided a forum for discussing potential changes in land use and circulation that could advantageously position the City to respond to changing market conditions. Specifically, this meeting provided an opportunity for the community to consider increasing residential densities and narrowing San Pablo Avenue in Old Town. Based on Steering Committee input, the Specific Plan includes sites where development densities of 40 – 50 units per acre may be appropriate, and includes plans to narrow San Pablo Avenue to two lanes.

# PRESENTATIONS TO PLANNING COMMISSION/STEERING COMMITTEE, AND PUBLIC – JANUARY 26, THROUGH MAY 11, 2009

After Circulating the Public Draft Specific Plan, the Steering Committee conducted a series of meetings to solicit and provide input and direction on the Specific Plan. At these meetings, the Steering Committee had an opportunity to refine Specific Plan chapters in response to community issues and provide direction on land use, circulation and development standards as follows:

- January 26, 2009 Specific Plan overview
- February 23, 2009 Review Chapters 1 4
- March 9, 2009 Review Chapters 5 & 6
- March 23, 2009 Review Chapters 7 11

- April 27, 2009 Review stakeholder (e.g. EDHAC, TAPS, DPW) input
- May 11, 2009 Review land use and parking standards

### 2.2 VISION

The community identified a separate vision for each of the three Specific Plan Areas, and the Specific Plan proposes urban design principles, plans, standards, and guidelines to implement these visions.

#### VISION FOR SAN PABLO AVENUE

- Old Town has a strong sense of place as the City's cultural, civic, and historic core that could transition from a small-scale network of specialty retail and services, to a mixed-use district with diverse residential and employment opportunities, to local and regional service uses.
- High-quality streetscape improvements and clearly defined walkways could be used to identify Old Town as a special destination rather than a bypass to the next jurisdiction.
- San Pablo Avenue provides vital economic development opportunities that can support diverse uses that serve the needs of local residents and visitors.
- San Pablo Avenue provides opportunities to enhance streets, sidewalks, walking paths, and bicycle lanes that connect with the San Pablo Bay, public transportation, surrounding neighborhoods, and other area parks, landmarks, and attractions.
- Streets should be made attractive by utilizing landscaping and street furniture.
- San Pablo Avenue can support several pedestrian "nodes" where visitors can park once and walk to several destinations.
- San Pablo Avenue should include gateway entry statements into both the City and Old Town.
- Development standards should support pedestrian-scale buildings and promote coordinated street furnishings and enhanced landscaping.
- Streetscape improvements should incorporate gateway features that let travelers know that they are entering Pinole and Old Town.

### VISION FOR PINOLE VALLEY ROAD

- Maintain the "Charming" scale and character of existing residential neighborhoods;
- Landscaping and streetscape improvements should be used to create a sense of security for pedestrians.
- A wide range of uses should be encouraged and accommodated along Pinole Valley Road to
  ensure that needed goods and services are available and to provide opportunity for new and
  unique businesses.
- The circulation system should encourage pedestrian activity and efficiently move automobile traffic. Pedestrian and bicycle access to Old Town should be added north and south of Interstate 80 along Pinole Valley Road with links to the Pinole Valley Creek trail.
- Pinole Valley Road will serve as a gateway into Pinole with widened sidewalks and landscaped plazas that can accommodate various outdoor activities.
- Streetscape improvements should convey a clear community identity to people visiting from outside Pinole.
- Pinole Valley Road should include gateway entry statements into both the City and Old Town.

### VISION FOR APPIAN WAY

- Appian Way is a thriving retail and service hub for the community and region with new open space and gathering places and well-integrated circulation for pedestrians, bicyclists, and vehicles.
- Appian Way will provide opportunity for new and unique businesses and catalyst projects.
- The Appian Way corridor will continue to function as the primary north/south automobile route in the City while also providing more opportunities for pedestrians and cyclists.
- Improvements will be made along the corridor to enhance pedestrian and cycling opportunities and safety along the busy corridor. The existing 1-80 Class III Bike Route along Appian Way from San Pablo Avenue to the south city limits will undergo upgrades to create designated bike lanes and improve cyclist safety. Additionally, pedestrian safety will be improved along Appian Way from the junior high school, across the I-80 interchange, and within and around the Appian Way shopping centers.
- Commercial parking lots will provide convenient and attractive locations to leave the car while walking to several destinations connected by well-landscaped sidewalks and pedestrian corridors.
- Appian Way will serve as a regional gateway into Pinole at the I-80 interchange and at the city limits boundary with El Sobrante. These areas would benefit from improved landscaping and streetscaping including street trees, landscaped medians, street furniture, unique gateway monuments, innovative directional signage, and attractive street lighting.

### 2.3 URBAN DESIGN PRINCIPLES

Input from the public outreach process identified the urban design principles discussed below for all Specific Plan areas as well as for the individual Specific Plan areas.

#### URBAN DESIGN PRINCIPLES APPLICABLE TO ALL SPECIFIC PLAN AREAS

### 1. Land Use and Economic Development

- Support the development of local-oriented services and encourage and support the local economy.
- Encourage a mix of uses that complement and support one another and attract local consumers and regional shoppers.
- Reduce the potential for conflict between residential and non-residential land uses.
- Develop standards and guidelines should support the vision of the Specific Plan and attract investment.
- Encourage and provide incentives for target uses such as movie theaters, sit-down restaurants, and civic theaters.

### San Pablo Avenue

- Encourage a mix of uses that complement and support one another and attract both local consumers and regional shoppers.
- Support diverse land use by targeting mixed-use development that can attract additional residential, live-work, office, and employment uses along with retail, restaurant, and entertainment uses.
- Preserve the historic character and resources of Old Town.

### Pinole Valley Road

- Identify opportunity sites near Interstate 80 and maximize the potential for those areas to develop/redevelop over time.
- Encourage business diversity to attract office workers during the day and provide a destination for individuals and families at night and on the weekend.

- Develop standards and guidelines that support the vision of the Pinole Valley Road Specific Plan and attract investment.
- Support redevelopment of existing retail centers by allowing higher densities and considering reductions in on-site parking requirements.

### Appian Way

- Support a mix of uses that complement and support one another and attract local consumers and regional shoppers.
- Specifically encourage the redevelopment of the Doctors Medical Center complex.
- Improve and renovate the outdated Appian 80 Shopping Center so that it can attract and keep desirable tenants, and also improve the layout and aesthetics of the Appian/Fitzgerald Shopping Center.
- Encourage the redevelopment of the residential properties fronting Appian Way south of I-80 into commercial sites.
- Create active use open space parkland on the hill including nature trails and pathways.
- Concentrate regional-serving commercial development along the I-80 corridor.

#### 2. Circulation

- Enhance pedestrian circulation and improve safety at appropriate pedestrian crossings to improve the walkability of the corridors.
- Enhance auto circulation and reduce congestion at appropriate locations and crossings.
- Improve public transportation connections to BART and the WestCAT J line and maximize safety through shelter, lighting, signage, and visibility.
- Enhance connections between neighborhoods, the Bay, and other recreational opportunities.

### San Pablo Avenue

 Strengthen bicycle connections along the San Pablo Avenue corridor between Old Town and Appian Way.

- Strengthen pedestrian connections between Tennent Avenue by widening and making existing sidewalks continuous within the corridor, adding bike routes and paths along the corridor.
- Narrow San Pablo Avenue in Old Town to improve walkability.

### Pinole Valley Road

- Strengthen pedestrian connections between Pinole Valley Shopping Center and the Valley (south of Interstate 80) by widening and making existing sidewalks continuous within the corridor, adding bike routes and paths along the corridor.
- Improve public transportation connections between the Valley (south of I-80) and the Pinole Valley Shopping Center and maximize safety through shelter, lighting, signage, and visibility.
- Improve and create visual and physical linkages to Pinole Valley Creek via bike paths, pedestrian walkways, and amenities and by orienting commercial uses toward the creek and to the edge of Pinole Valley Road.

### **Appian Way**

- Strengthen bicycle connections along the Appian Way corridor between Old Town and the Appian Way shopping centers.
- Strengthen pedestrian connections between the junior high school and the Appian Way shopping centers.

#### 3. Parking

- Establish on-site parking requirements for non-residential uses and discourage off-site parking that may impact nearby residential areas.
- To the maximum extent feasible, provide parking that is available, convenient, and unobtrusive.
- Ensure adequate pedestrian and vehicular connections between parking lots in commercial areas to encourage shared parking.

### San Pablo Avenue

Encourage on-street parking and support additional parking by building a parking garage.

### Pinole Valley Road

• Encourage additional retail development at the shopping center by reducing the number of existing excess parking spaces and enhancing pedestrian connection.

### 4. Focal Point, Aesthetics, Landscaping, Lighting, and Signage

- Streetscape and landscape improvements should include attractive gateways and signage, green spaces, public art, and distinctive décor that contribute to a clean, safe, and entertaining small town atmosphere.
- Establish consistency in street lighting, streetscape furnishings and fixtures, and landscape improvements.
- Encourage business reinvestment to (1) increase the density and optimize development potential of the corridors, (2) improve landscaping throughout the site, and (3) update signage programs.

3.0	Existir	ng Conditions			
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3.2	Area History and Historic Resources				
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### 3.1 INTRODUCTION

This chapter describes the built environment within the three Specific Plan corridors, including historic resources, circulation and mobility, land use and zoning, and economic conditions.

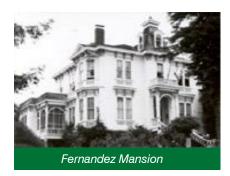
### 3.2 AREA HISTORY AND HISTORIC RESOURCES

The City of Pinole has a rich history with many historic resources and architecturally unique buildings.

The City of Pinole was



incorporated in 1903. By 1915, the City included numerous commercial businesses as well as the Pinole Opera House, the Pinole Theater, and the Bank of Pinole. Between 1918 and 1958, Pinole remained quiet and quaint even during World War II.



Since the 1970s, the City has steadily grown in population and in physical development. Most of the regional and local shopping centers in Pinole today have been developed since 1970.

The City developed rapidly with the construction of I-80 in 1958. Pinole has developed as a suburban "bedroom community" for the San Francisco and Oakland workforce.



Homes along Tennent Avenue in Old Town Pinole

In spite of the population growth and development over the years, Old Town Pinole still retains its historic and architectural character.



#### **ARCHITECTURAL STYLES**

Old Town Pinole and areas along San Pablo Avenue reflect the architectural styles that were prevalent at the time of development. Within Old Town Pinole, these include neoclassical commercial architecture and residential architecture in the style of Queen Anne Cottages (1880-–1905), Hip Roof Cottages (1870–1910), and Bungalows (1915–1930). Other residential styles also include Classical Revivals, Farmhouse/Ranch Style homes, Prairie Style Bungalows, and Second Empire French.

#### How Historic Information Will Be Used

While the Specific Plan does not require strict application of

traditional historic architectural styles, it will seek to honor and retain historic influences where

possible. The degree to which historic resources influence design will vary by project, but protection of existing noteworthy structures and respect for the City's heritage is a theme that will guide new development.

# 3.3 TOPOGRAPHY AND ENVIRONMENTAL CONSIDERATIONS

#### **TOPOGRAPHY AND CIRCULATION**



Physical development in the City of Pinole is substantially influenced by topographic conditions that include a fairly steep shoreline, distinct valleys that are separated by small ridge lines, and a comparatively level alluvial plane at the mouth of Pinole Creek. Originally, development occurred primarily in Pinole's valleys and commercial activity was most prevalent in Old Town, adjacent to San Pablo Bay and Pinole Creek. Beginning in the late 1950s, residential and commercial development expanded into surrounding uplands.

In addition to topographic constraints, the City's physical form is largely influenced by Interstate 80 and San Pablo Avenue, and to a lesser degree by Appian Way and Pinole Valley Road. Built in 1958, Interstate 80 is the most heavily travelled route through the City. Most of the City's regional health services and retail shopping centers are adjacent to I-80. Interstate 80 also acts to divide Pinole by separating the northern half of the City from the southern half.

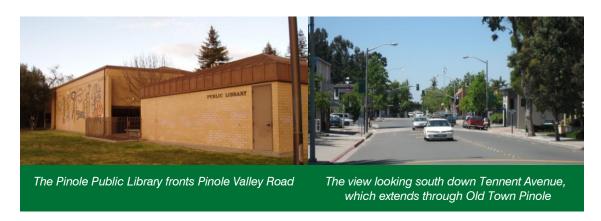
#### **SAN PABLO AVENUE CORRIDOR**

San Pablo Avenue is located north of Interstate 80 and extends west-east through the City of Pinole. San Pablo is lined with a diverse mixture of land uses that include single-family and multifamily residential units; a wide variety of commercial uses including office parks, small businesses, and a local shopping mall; light industrial uses such as Sugar City; and historic homes and buildings.



#### PINOLE VALLEY ROAD CORRIDOR

Pinole Valley Road and Tennent Avenue are major arterials located on the eastern end of the City. Pinole Valley Road connects Pinole Valley to the northern portion of the City. Pinole Valley Road also includes a highway interchange. Most notably, Pinole Valley Road and Tennent Avenue host a number of the major community services and facilities including Pinole Valley High School, the public library, and the new Kaiser Medical Office Building. Tennent Avenue extends from Pinole Valley Road to San Pablo Avenue, through Old Town Pinole.





#### **APPIAN WAY CORRIDOR**

Appian Way is a major arterial connecting two routes of regional significance, Interstate 80 and San Pablo Avenue. This corridor contains retail, medical office, public school, and retail land uses. The presently vacant Doctors Medical property dominates the corridor where it intersects with Interstate 80.

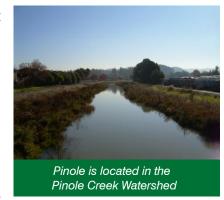
#### **ENVIRONMENTAL CONSIDERATIONS**

This chapter discusses key environmental considerations for the development of the specific plan: open space and sustainability and green building.

### **Open Space**

The City of Pinole has substantial open space resources that are under the stewardship of the City, homeowners associations, the West Contra Costa Unified School District, the Contra Costa County Flood Control District, and private landowners. Open space highlights relevant to the Specific Plan Area are listed below:

 Watersheds. The Pinole Creek watershed covers approximately 15 square miles of west Contra Costa



County, extending from the headwaters in the Briones Hills to the outfall at San Pablo Bay north of Point Pinole. Approximately 30% of the watershed is owned and managed by the East Bay Municipal Utilities District (EBMUD). Though these holdings are located outside of the City of Pinole, their use and management are crucial to the City's water quality, resource

enhancement and flood protection objectives. Recent discussions have included the possible use of the EBMUD property for possible mitigation land banking and flood water diversion basins.

- Open Space. East Bay Regional Park District (EBRPD) manages 95,000 acres of open space
  and preserves within Contra Costa and Alameda counties providing regional amenities to
  Pinole residents, including direct access to the San Francisco Bay Trail. Additionally, the City
  provides recreational facilities including parks and trails, one regional park, four community
  parks, five neighborhood parks, and four unimproved open space public areas.
- Pinole Creek Restoration. The Pinole Creek Restoration Project plans to restore high-quality riparian habitat, provide additional flood protection, and improve water quality in Pinole Creek. This project will also enhance recreation by connecting Old Town Pinole to the Bay Trail by means of a multipurpose path and will provide a launch for non-motorized boats at the mouth of the creek as part of the Bay Water Trail backbone.
- Pinole General Plan. The Natural Resources and Open Space Element of the City's General Plan establishes policies and programs related to natural resources.

### Sustainability and Green Building

Concerns about energy consumption, air and water quality, climate change, and related issues have caused the City of Pinole to actively promote development and construction practices that are sustainable and to use green building techniques. Pinole is striving to achieve tangible reductions in greenhouse gas emissions and create a more sustainable community to help combat global climate change.

### 3.4 EXISTING CIRCULATION

Pinole's circulation system includes roadways, transit services, bicycle and pedestrian facilities, and other transportation-related facilities such as parking and freight service.

The City's street circulation system locates major arterials so that they do not pass through, but provide the boundaries for, residential neighborhoods. Interior streets are designed to include culde-sacs, loop streets, and neighborhood collectors to discourage through traffic. Given the topography of Pinole, the City's street system does not represent a traditional grid pattern.

The General Plan and this Specific Plan focus on increasing the opportunities for multiple travel modes including public transit and pedestrian and cycling options. Furthermore, the Specific Plan suggests ways to calm traffic with the objective of creating a friendlier pedestrian environment.

#### **KEY CITY ROADWAYS**

The major arterial corridors included within this Specific Plan are described below and are depicted in **Figure 3.1**.

#### **PROJECT AREA CORRIDORS**

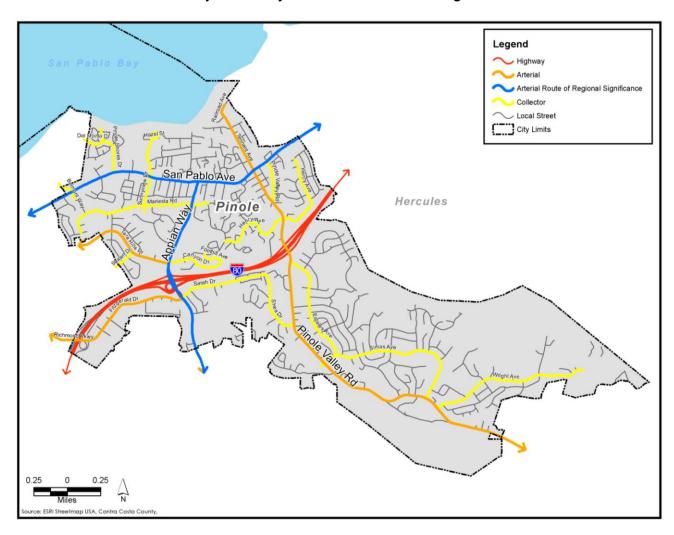
San Pablo Avenue is a 4-lane, mostly divided arterial running east-west along the northern portion of Pinole and providing connections to the unincorporated community of Tara Hills to the west and City of Hercules to the east. San Pablo Avenue runs from the City of Oakland to the unincorporated community of Crockett and is a route of regional significance. On-street parking is generally allowed along San Pablo Avenue in Pinole.

Appian Way is a 4-lane, north-south arterial extending south from San Pablo Avenue and narrowing to two travel lanes before running through the unincorporated community of El Sobrante. On-street parking is allowed on some segments of the roadway.

Pinole Valley Road is a 2-lane local collector from San Pablo Avenue to Henry Avenue where it becomes a 4-lane, north-south arterial from Henry Avenue to Simas Avenue. South of Simas Avenue, Pinole Valley Road narrows to two lanes until it becomes Alhambra Valley Road at the city limit. On-street parking is allowed on some segments of the roadway.

**Tennent Avenue** is a 2-lane, north-south arterial from its southern terminus at Pinole Valley Road to San Pablo Avenue. From San Pablo Avenue to its northern terminus at Pinole Bayfront Park, Tennent Avenue serves as a collector. On-street parking is generally allowed.

Figure 3.1 Major Roadways in Pinole and Surrounding Area



#### **OTHER SUPPORT ARTERIALS**

**Fitzgerald Drive** is a 4-lane, east-west divided arterial connecting Richmond Parkway and Appian Way on the southern side of I-80. On-street parking is prohibited.

Tara Hills Drive is a circuitous arterial connecting Appian Way to unincorporated communities on both sides of San Pablo Avenue. Tara Hills Drive is a 4-lane roadway extending from Appian Way to Flannery Road, where it narrows to two lanes to its terminus in the unincorporated community of Bayview-Montalvin.is On-street parking is generally allowed on the 4-lane section and prohibited on the 2-lane section.

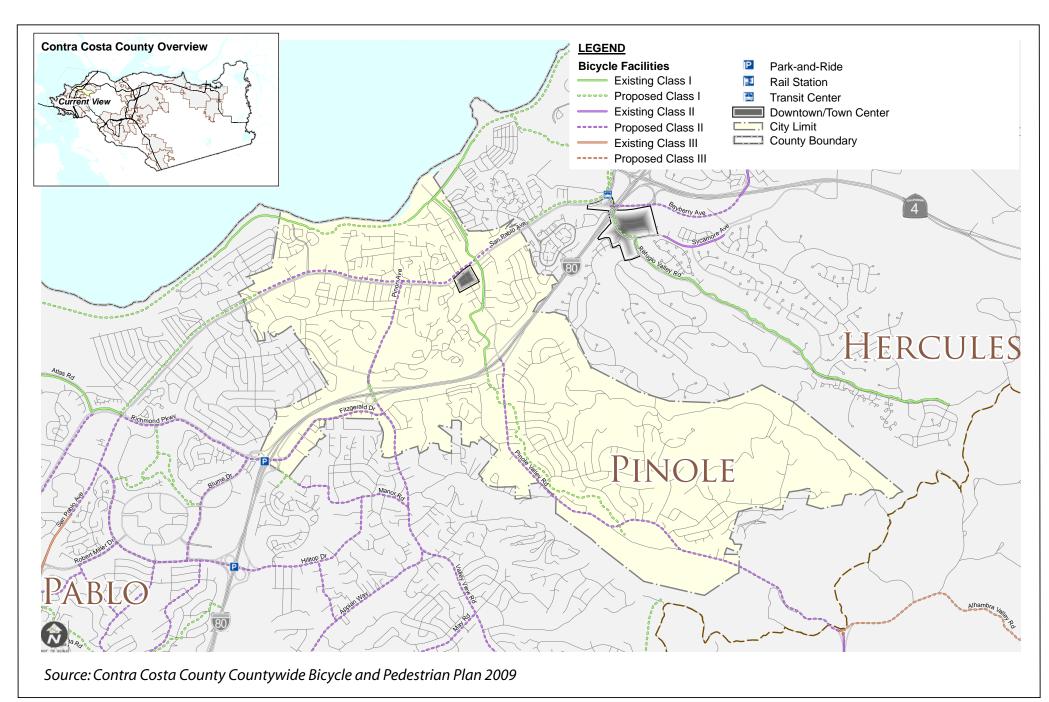
#### **BICYCLING FACILITIES AND PROGRAMS**

Topographic constraints preclude the use of most local streets as parallel, lower-traffic-volume, slower-speed alternatives to arterial streets. Consequently, bicycle routes must be located primarily on arterial streets. There are three categories of bicycle facilities. Class I facilities are bicycle paths that are physically separated from travel lanes by landscaping or grade changes. Class II facilities are identified with signs and roadway striping that denote use by bicyclists adjacent to automobile travel lanes. Class III facilities make use of signs to identify bicycle routes where cyclists and auto users share a travel lane.

Bicyclists using roadways and paved lanes and paths have access to facilities identified in **Table 3.1** and shown on **Figure 3.2**.

Table 3.1 Existing Bikeway Facilities in Pinole

Bicycle Lane/Path Segment	Type of Facility	Length (Miles)
Pinole Creek Trail from Henry Avenue to Railroad Avenue – Bay Trail	Class I Multi-Use Path	1.15
Bayfront Park – Bay Trail loop near Tennent Avenue and Railroad Avenue	Class I Multi-Use Path	0.35
Class I Multi-Use Path Subtotal		1.5
Class II Bike Lane Subtotal		0.0
Appian Way (I-80 Bikeway) from San Pablo Avenue to Dalessi Drive (city limit)	Class III Bike Route	0.95
Fitzgerald Drive from I-80 to Appian Way	Class III Bike Route	0.95
San Pablo Avenue from western to eastern city limits	Class III Bike Route	1.90
Class III Bike Route Subtotal		3.8
Total: 5.30 Miles		



The Class I Multi-Use Path along Pinole Creek provides connections to Collins Elementary School, Pinole Valley Lanes bowling alley, the Central Business District, Fernandez Park, residential areas, and the Bay Trail. While a bit narrow to accommodate both bicyclists and pedestrians, it provides a safe recreational and commuter path. There are disconnection issues, lack of signage, and roadway crossing aids at Henry Avenue, Tennent Avenue, San Pablo Avenue, and Railroad Avenue.

The Class I Multi-Use Path at Bayfront Park, which is part of the Bay Trail, provides a recreational cycling opportunity but is not yet connected to other segments of the Bay Trail.

San Pablo Avenue is the east-west arterial and Pinole Valley Road is the north-south arterial with the gentlest grades but only San Pablo Avenue is designated as a Class III bike route. High vehicle speeds and volumes on these arterials may discourage inexperienced bicyclists from using the roadways. Fitzgerald Drive and Appian Way are designated Class III bike routes with several access points to major retail centers. High vehicle speeds and volumes, as well as a large number of turning movements along Fitzgerald Drive and Appian Way, may discourage inexperienced bicyclists. Hilly terrain along Appian Way may serve to discourage bicycling for all but the fittest and most experienced bicyclists.

#### **PEDESTRIAN SYSTEMS**

The City of Pinole contains sidewalks with raised curb and gutter on all arterials and collectors. There is a discontinuous sidewalk on the north side of San Pablo Avenue in two locations, immediately west of Appian Way, and at "Shale Hill" just west of Old Town. In several locations, sidewalks are too narrow for comfortable circulation and are partially obstructed by utility poles, newspaper racks or other similar facilities. Most signalized intersections have marked crosswalks on all legs and pedestrian signal heads and actuation. Intersections close to I-80 prohibit pedestrian crossing across some legs. Only one intersection has audible crossing signals (San Pablo Avenue at Sunnyview Drive) and none have audible actuation. Pedestrian curb ramps are located at most intersections except at some T-intersections, such as the northwest corner of Pinole Valley Road at Ramona. However, some ramps are steep and are not optimally positioned to lead into the crosswalk.

The City's multi-use paths along Pinole Creek and in Bayfront Park at the Bay Trail provide recreational and transportation opportunities to walkers, with Pinole Creek Trail providing connections to several activity centers. Though these trails are well used, signage and width issues

<sup>&</sup>lt;sup>1</sup> The Bay Trail is a project of the Association of Bay Area Governments (ABAG) and is a planned recreational corridor that, when complete, will encircle San Francisco and San Pablo Bays with a continuous 400-mile network of bicycling and hiking trails.

create problems for pedestrians and bicyclists. Additionally, pathway connections from Tennent Avenue and Railroad Avenue to the Bay Trail are unpaved and unsigned. Neighborhood access to the Pinole Creek Trail is provided at Prune Street where there is a creek crossing, but access is discouraged at Plum Street.

The commercial area located on San Pablo Avenue from Quinan Street to Pinole Valley Road, contains pedestrian-oriented land uses, pedestrian-scaled lighting, and crossing aids at all intersections such as marked, high-contrast crosswalks, intersection bulb-outs, directional curb ramps, and pedestrian signal heads. The sidewalk on the north side of San Pablo Avenue is narrow near Tennent Avenue where newspaper stands, planters, portable signs, and tables serve as impediments to pedestrian circulation.

### **Transit Systems**

Transit systems provide a motorized alternative to private vehicles. They serve citizens who cannot drive or chose not to drive, including senior citizens, residents with limited mobility, people under the age of 16, residents with no driving licenses or suspended driving licenses, and citizens opting to live a less car-dependent lifestyle. Transit stops providing regional connectivity for the Specific Plan Area exist along Appian Way, San Pablo Avenue, and Pinole Valley Road. Many of these stops lack benches, shelter from the elements, and solid waste containers, amenities that are important for encouraging transit use.

#### **Bus Service**

Pinole is served by two bus transit agencies, the Western Contra Costa Transit Authority (WestCAT) and the Alameda Contra Costa Transit Authority (AC Transit). For Pinole, WestCAT presently operates fixed-route services, an express bus service, the JPX, to El Cerrito BART, and an express bus service, the Lynx, to San Francisco from the Hercules Transit Center. WestCAT also operates fixed-route services in southern Pinole and operates dial-a-ride paratransit services for seniors and the disabled in southern Pinole.

WestCAT operates seven fixed-route services that serve Pinole. **Table 3.2** below summarizes the fixed route services and **Figure 3.3** displays their service map in Pinole. All fixed route busses are equipped with front-loading racks that can hold up to two bicycles. WestCAT's portable route map contains information on bus stop locations, routes, and fixed route bus schedules.

AC Transit operates 2 fixed-route services in southern Pinole, Route 70 (Appian) and Route 376 (North Richmond Night), which are pictured in **Figure 3.3**. AC Transit also operates dial-a-ride

paratransit services for seniors and the disabled in southern Pinole. Busses are equipped with front-loading racks that can hold up to two bicycles.

Table 3.2
WestCat Bus Service in Pinole

Route	Cities Served	Roads
16 Pinole Valley	Richmond Pinole	Richmond Parkway Fitzgerald Drive Pinole Valley Road San Pablo Avenue
17 Bay View	Richmond Pinole	Richmond Parkway Fitzgerald Drive Appian Way
18 Tara Hills	Pinole Tara Hills Richmond	Richmond Parkway Hilltop Mall Tara Hills San Pablo Avenue
19 Hilltop/Hercules	Richmond Pinole Hercules	Richmond Parkway Hilltop Mall Fitzgerald Drive Pinole Valley Road Hercules Transit Center
30Z Martine	El Cerrito Richmond Pinole Hercules Martinez	El Cerrito del Norte BART Richmond Parkway Fitzgerald Drive Hercules Transit Center Martinez Amtrak
C3 Contra Costa College	San Pablo Pinole Hercules	Contra Costa College San Pablo Avenue Hercules Transit Center (Fall and Spring Semesters only)
J BART	El Cerrito Richmond Pinole Hercules	El Cerrito del Norte BART Richmond Parkway Hilltop Mall San Pablo Avenue Hercules Transit Center

Source: WestCAT 2007

Legend Bus Stops City Limits **Bus Lines** JPX (Commute) JPX (Midday) urce: ESRI Streetmap USA, Contra Costa County

Figure 3.3
WestCat and AC Transit Bus Service in Pinole

### **Passenger Rail Service**

The San Francisco Bay Area Rapid Transit District (BART) provides heavy-rail, regional transit service to Alameda, San Francisco, Contra Costa, and San Mateo counties. There is currently no BART service in Pinole. However, WestCAT operates the J bus route which connects to the BART station at El Cerrito del Norte. Amtrak operates passenger rail service for three routes that traverse western Contra Costa County. While there is no station in Pinole, Union Pacific Railroad facilities pass through the northern portion of the City. The closest passenger rail station is an unmanned Amtrak station at the Richmond BART station and there are plans to build a ferry and Amtrak station in neighboring Hercules to the east.

### 3.5 EXISTING ZONING AND LAND USE

Originally adopted in 1971, the current Zoning Ordinance for Pinole has been incrementally amended since that date. The most recent substantive amendments have included changes to the Zoning District map to rezone properties along San Pablo Avenue to Planned Development and the addition of Residential Design Guidelines and tree preservation requirements. The Zoning Ordinance is the primary tool for implementing the policies of the General Plan and includes physical development standards and procedures for changing the status of land use.

As previously stated, the Specific Plan for San Pablo Avenue, Pinole Valley Road, and Appian Way will replace the zoning regulations with special/unique districts, allowed use provisions, and development standards for land within the project areas. The Citywide Zoning Code applies to the Specific Plan Area only where this Specific Plan specifically refers to the Citywide Code or does not address a particular topic (e.g., allowed sign area).

### 3.6 EXISTING FISCAL CONDITIONS

#### **CITY BUDGET AND REVENUES**

The City of Pinole is facing reductions in projects and programs in order to balance the City Operations Budget with projected reductions in revenue. The City Operations Budget includes a five-year Capital Improvement Program that includes improvements within the Specific Plan Boundaries.

#### REDEVELOPMENT AND ECONOMIC DEVELOPMENT

Since 1972, the Pinole Redevelopment Agency (Agency) affirms that it has brought in an additional \$40 million in revenues to the City through the use of tax increment financing (TIF). TIF is a

redevelopment tool to use future gains in taxes to finance improvements that benefit the Redevelopment Project Areas.

The Redevelopment Agency assists local developers and businesses through a new City program called the Business Relocation and Assistance Program (BRIP).

### BRIP has the following goals:

- Assist new business relocate new jobs to the Project Area
- Assist existing business expand with new jobs and relocate to the Project Area
- Assist property owners obtain the highest and best use for vacant property
- Attract private sector investment in the Project Area
- Increase the employment base of the Project Area

#### **REDEVELOPMENT PROJECTS**

Redevelopment projects in the Specific Plan Area include, but are not limited to, Fernandez Corner, a multi-story mixed-use project on San Pablo Avenue; the development of underutilized properties along the West San Pablo Avenue corridor at the corner of Sunnyview and San Pablo Avenue; the remodeling of Memorial Hall; Pinole Shores Business Park, a light industrial park on San Pablo Avenue; the commercial project located at 2301 San Pablo Avenue; the Pear Street Bistro located at 2395 San Pablo Avenue; the Pinole Valley Shopping Center; the Kaiser Medical Office Building; and several other infrastructure and design improvement projects that help to facilitate economic development in the City.

### **OLD TOWN PINOLE REVITALIZATION**

At the end of 2006, the Pinole Redevelopment Agency undertook a revitalization study of the economic development potential of Old Town Pinole. The study determined that there is strong support for Old Town redevelopment in the City. Residents in Pinole would like to see the following changes in the Old Town area: more shops, restaurants, and activities; a safe environment; a grocery store; and development of vacant lots.

#### REDEVELOPMENT PROGRAMS

The Agency provides several programs to encourage economic and business development in the City, particularly in the Specific Plan Area. The Agency's Commercial Rehabilitation Program provides loans and grants to tenants and property owners for the improvement of commercial structures within Old Town Pinole and along the San Pablo Avenue corridor. Funding categories include architectural design services, business consultations, seismic upgrade grants, loans for security, signage, awnings, accessibility improvements, and general rehabilitation.

### 3.7 OPPORTUNITIES AND CONSTRAINTS

The opportunities and constraints of each Specific Plan corridor have been analyzed to identify factors that could contribute to or hinder attainment of goals and objectives. Each corridor's strengths and weaknesses were assessed with consideration of physical design and economic potential. Where possible, the plan builds upon the corridor's strengths and overcomes or minimizes its weaknesses. The following discussion provides an overview of the major opportunities and constraints findings for the San Pablo Avenue, Pinole Valley Road, and Appian Way corridors.

# OPPORTUNITIES AND CONSTRAINTS ANALYSIS FOR SAN PABLO AVENUE Summary of Opportunities and Constraints for San Pablo Avenue

### **Opportunities**

The following list includes the physical opportunities that should be recognized and built upon in the San Pablo Specific Plan Area (see **Figure 3.4A & 3.4B** Opportunities and Constraints Maps for Land Use and Circulation).

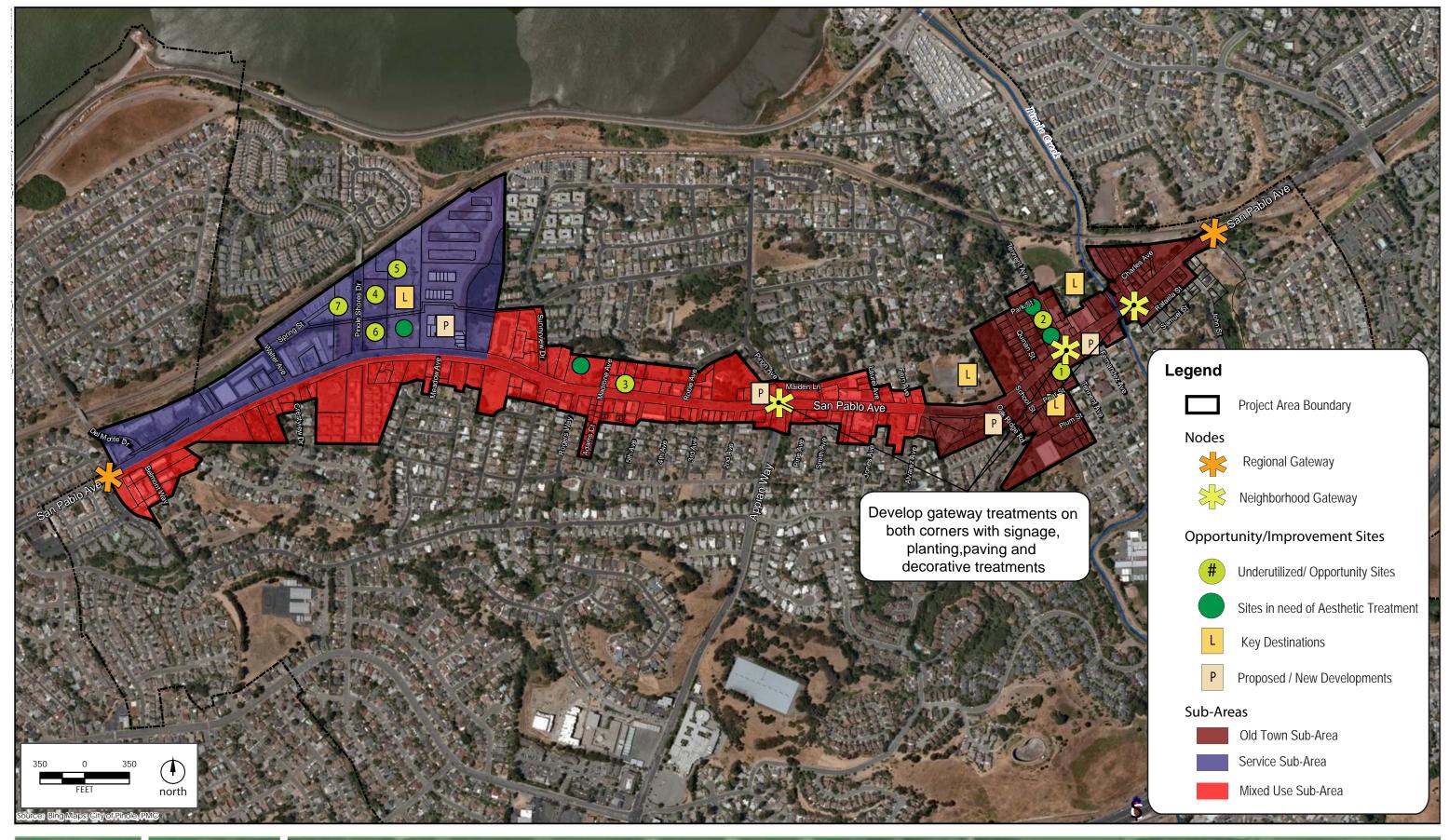
- Many potential retail street development projects and sites, including existing retail anchors
- Quaint Old Town, pedestrian-friendly area from Johns Street to Oak Ridge
- Gateway to Pinole and Old Town to and from the surrounding region
- Number of community and regional destinations
- Pinole Creek provides good opportunity for pedestrian linkages and bike paths.
- Complementary blend of uses (residential, services, restaurants, and employment)

- Natural topography allows for district creation
- Number of recent, planned, or potential improvements
- Number of opportunities for infill and/or redevelopment
- Redevelopment area boundary encompasses plan area

### **Constraints**

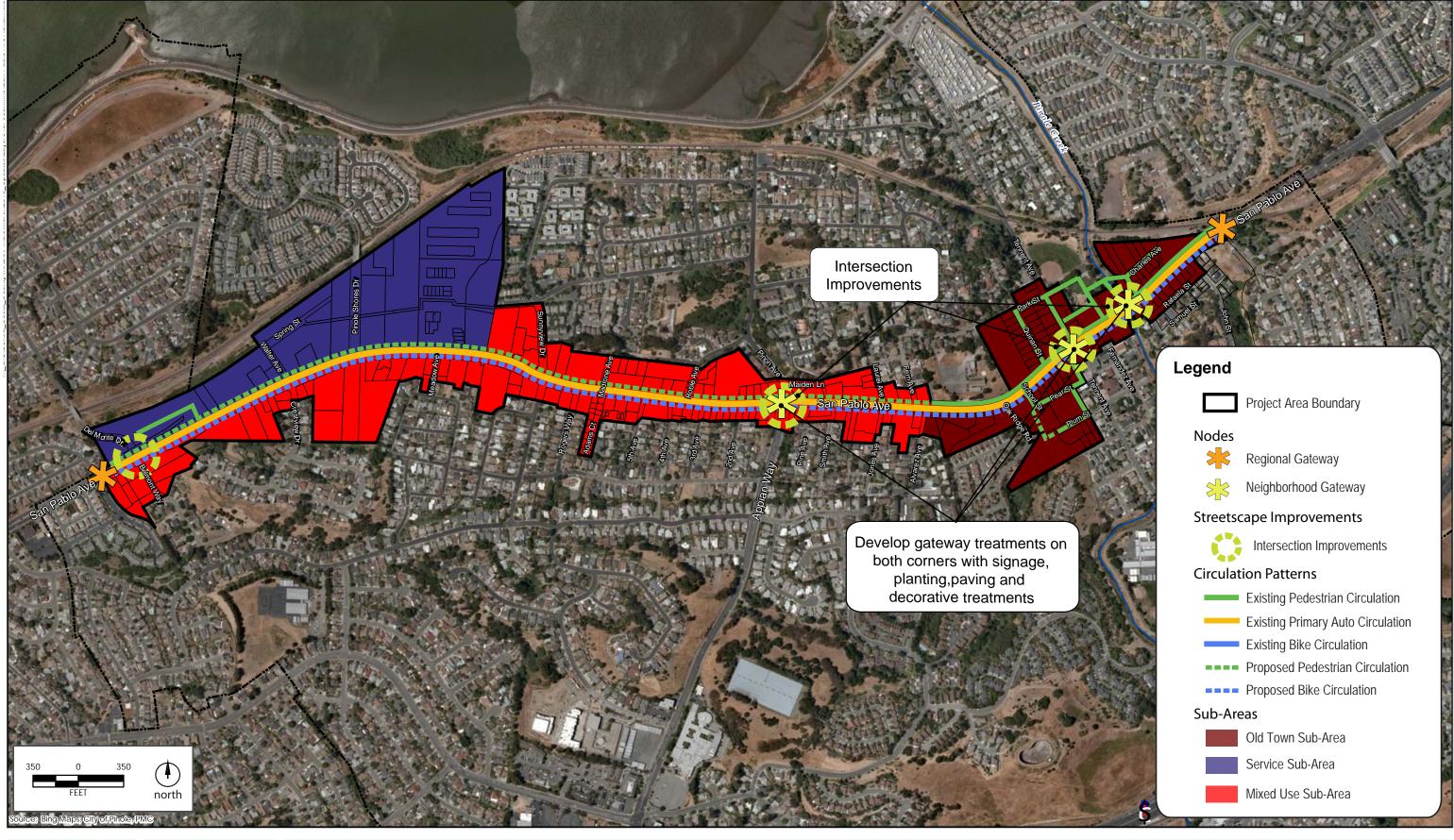
Physical constraints or obstacles that need to be overcome in the San Pablo Specific Plan Area include:

- Wide streets that dedicate a high proportion of the right-of-way to automobile traffic
- Narrow sidewalks located near heavy traffic inhibit pedestrian travel
- High traffic speeds that conflict with pedestrian activity
- Lack of cohesive theme or identity for avenue
- Tired-looking developments on west end
- Need for stronger connections between residential and commercial uses
- Lack of convenient public transit options and connections, and inadequate transit amenities (e.g. bus shelters, benches, trash receptacles, etc.)













#### **Sub-Areas**

Land use, neighborhood character, development patterns, and topography create distinct subareas within the corridor. Three key sub-areas were identified for the San Pablo Avenue Specific Plan Area including (see **Figure 3.4C**):

#### Old Town Sub-Area

- With a small town and historic character of its own, Old Town should focus on continuing and enhancing its architectural scale, character, and style.
- Old Town should continue hosting cultural events and support specialty and locally owned businesses that serve as the current draw.
- More effort is needed to have existing businesses coordinate events and activities to attract
  more visitors including development of an organized merchants association to represent the
  businesses and provide for an organizational structure.

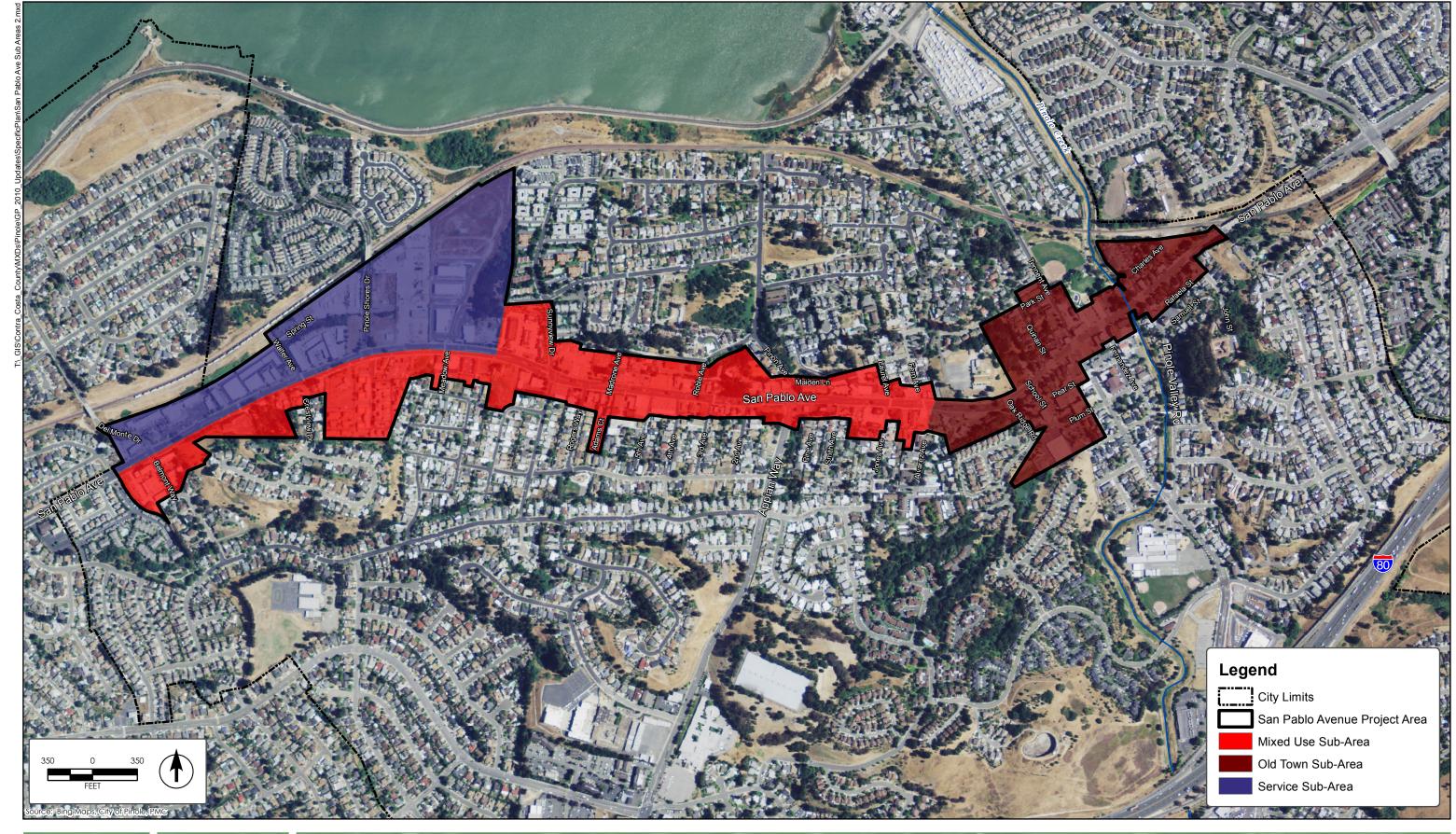
#### Mixed-Use Sub-Area

- The Mixed-Use sub-area should encourage a balance of employment and housing opportunities, with an emphasis on a variety of housing densities.
- This area should provide a higher-density housing stock to contribute potential customers and employees to a vibrant and active Old Town environment, for employers proximate to the project area.

#### Service Sub-Area

- As a gateway to the Specific Plan Area, the service district is well placed for capturing revenue from neighboring communities. Higher intensity service centers like home improvement stores, office uses and light industry have found this area suits their economic demographic needs and larger lot sizes and location criteria (closer to the highway).
- This area may continue as a local service center, providing necessary services to residents of Pinole and neighboring communities.
- It is important that the scale and massing of new services that locate in this area fit Pinole's small town character.

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# OPPORTUNITIES AND CONSTRAINTS ANALYSIS FOR PINOLE VALLEY ROAD Summary of Opportunities and Constraints for Pinole Valley Road

#### **Opportunities**

The following list includes the physical opportunities that should be recognized and built upon in the Pinole Valley Road Specific Plan (see **Figure 3.5A & 3.5B** Opportunities and Constraints Maps for Land Use and Circulation).

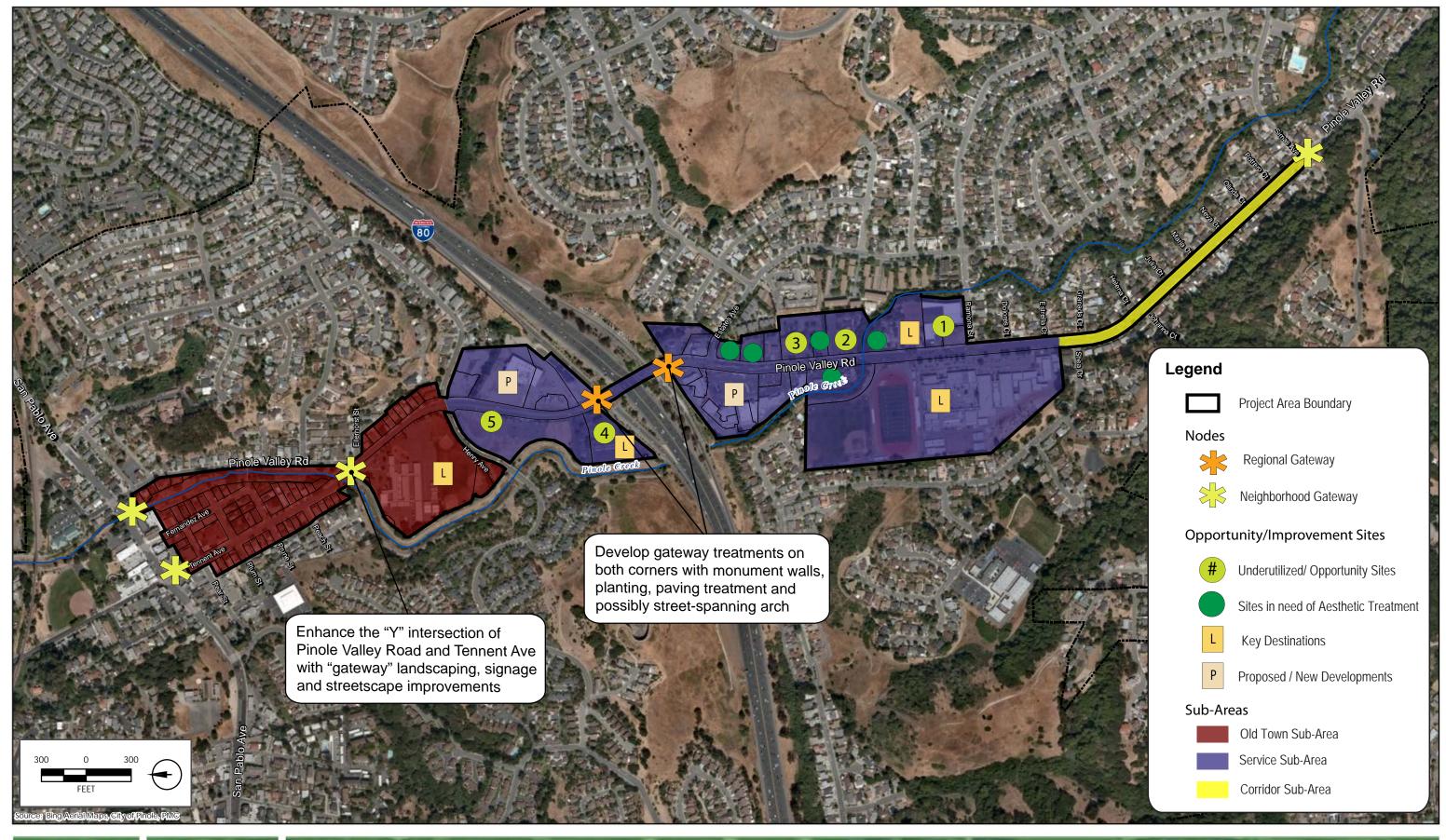
- Many potential retail street development projects and sites, including existing retail anchors
- Attractive new investment properties/ opportunity sites, entertainment and recreational facilities, quality streetscapes, and street improvements
- Gateway to Pinole from the Interstate and surrounding region
- Successfully revitalized Pinole Valley Shopping Center
- Pinole Creek provides good opportunity for pedestrian linkages and bike paths
- Kaiser building and offices provide a stable local workforce
- Landscape and streetscape improvements will improve access and gateway appearance to Old Town and to Pinole Valley Shopping Center
- Quaint Old Town, pedestrian-friendly and treasured area from Ellerhorst Street (along Tennent Avenue/Pinole Valley Road split) to San Pablo Avenue

#### Constraints

Physical constraints or obstacles that need to be overcome in the Pinole Valley Road Specific Plan Area include:

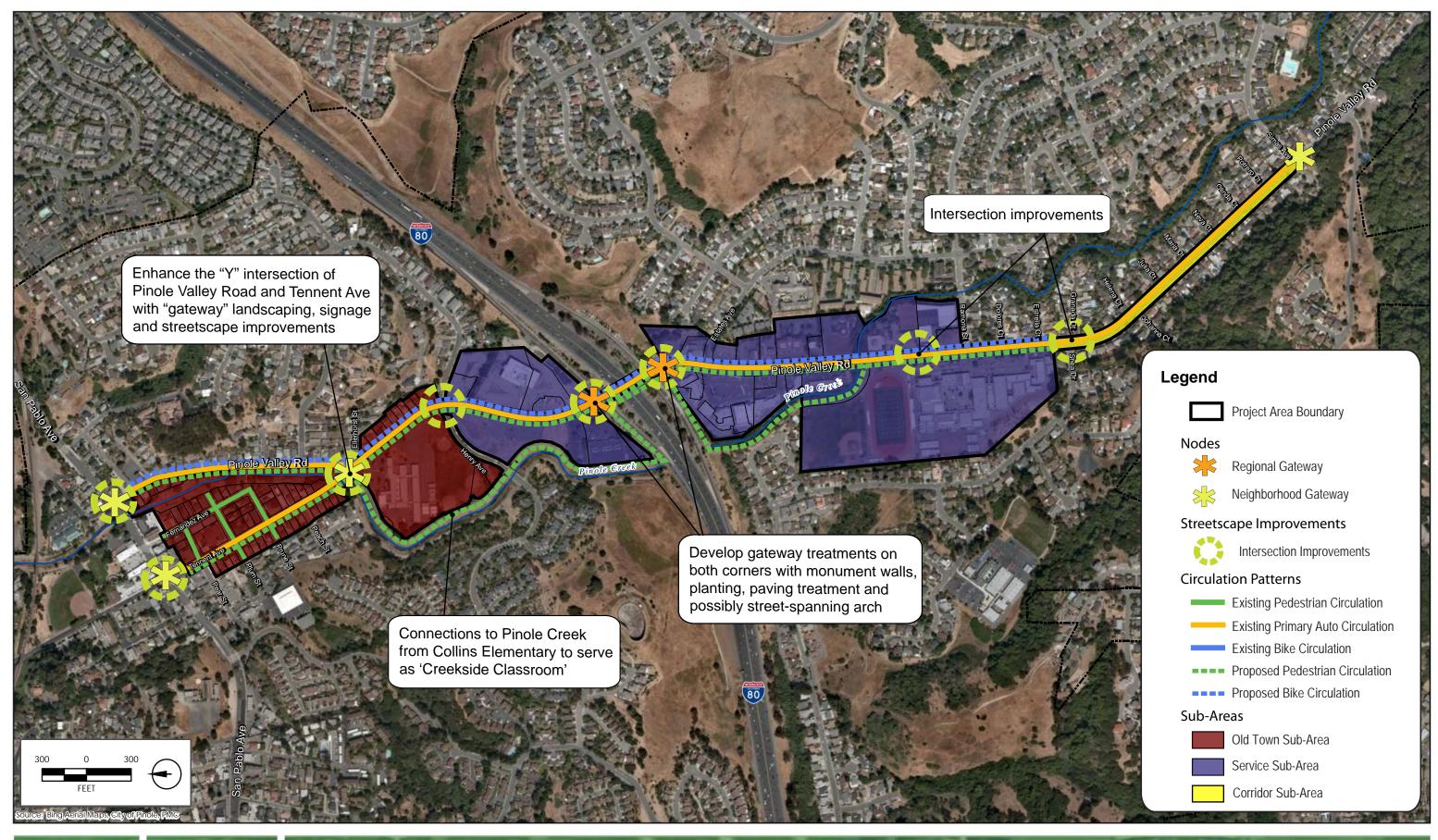
- Wide streets that dedicate a high proportion of the right-of-way to automobile traffic
- Traffic that travels at high speed when volumes are low, but that becomes congested during peak periods of use
- Pedestrian and auto circulation conflicts around Collins Elementary and Pinole Valley High Schools

- Interstate 80 is a physical and psychological barrier to pedestrian and bicycle circulation
- Indistinct retail streets and retail district
- Outdated automobile oriented commercial development
- Inadequate pedestrian and bicycle links between residential and commercial uses
- Lack of convenient public transit options and connections













#### **Sub-Areas**

Land use, neighborhood character, development patterns, and topography create distinct subareas within the corridor. Three key sub-areas were identified for the Pinole Valley Road Specific Plan Area including (see **Figure 3.5C**):

#### Old Town Sub-Area

- With a small town and historic character of its own, Old Town should focus on maintaining and enhancing its quaint residential, commercial and mixed-use nature, architectural scale, character, and style and serve as a connection to Old Town Pinole.
- More effort is needed to have existing businesses coordinate events and activities to attract
  more visitors including development of an organized merchants association to represent the
  businesses and provide for an organizational structure.

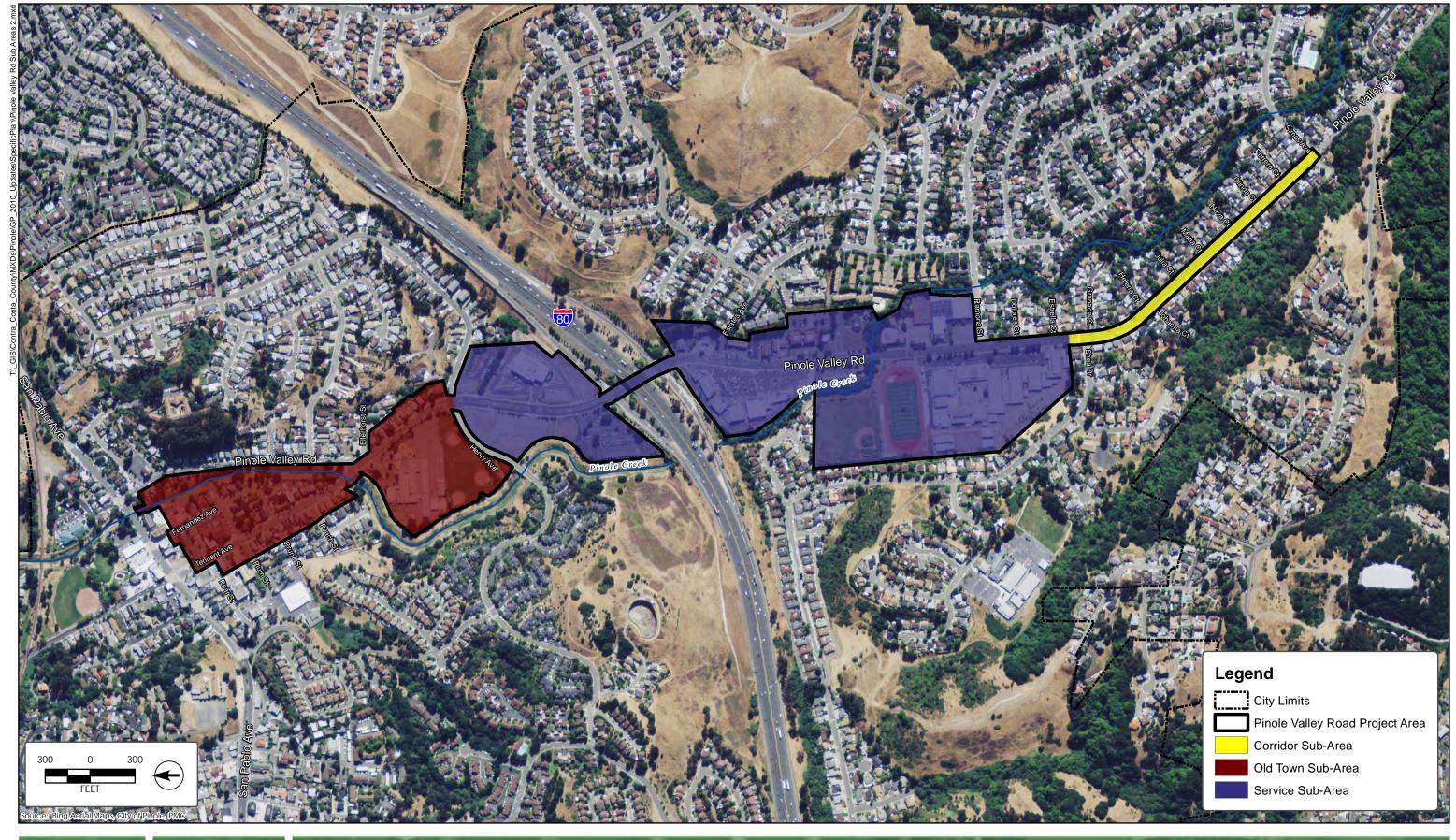
#### Service Sub-Area

- This area serves as the gateway through the Specific Plan Area and has significant potential for capturing revenue from neighboring communities.
- While primarily service-oriented, this area also has live-work potential and should therefore
  encourage a balance of employment and housing opportunity, with an emphasis on a variety
  of housing densities.
- The scale and massing of existing and new services should not be impersonal, rather they should be compatible with Pinole's small town character.

#### Corridor Sub-Area

- The corridors are critical to circulation and provide opportunities to enhance the scenic and civic values of the community.
- "Gateway" improvements pedestrian and bicycle facilities and linkages should be provided along Pinole Valley Road with convenient connections to the surrounding residential, commercial, recreational, and institutional uses.

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## OPPORTUNITIES AND CONSTRAINTS ANALYSIS FOR APPIAN WAY Summary of Opportunities and Constraints for Appian Way

#### **Opportunities**

The following list includes the physical opportunities that should be recognized and built upon in the Appian Way Specific Plan Area (see **Figure 3.6A & 3.6B** Opportunities and Constraints Maps for Land Use and Circulation):

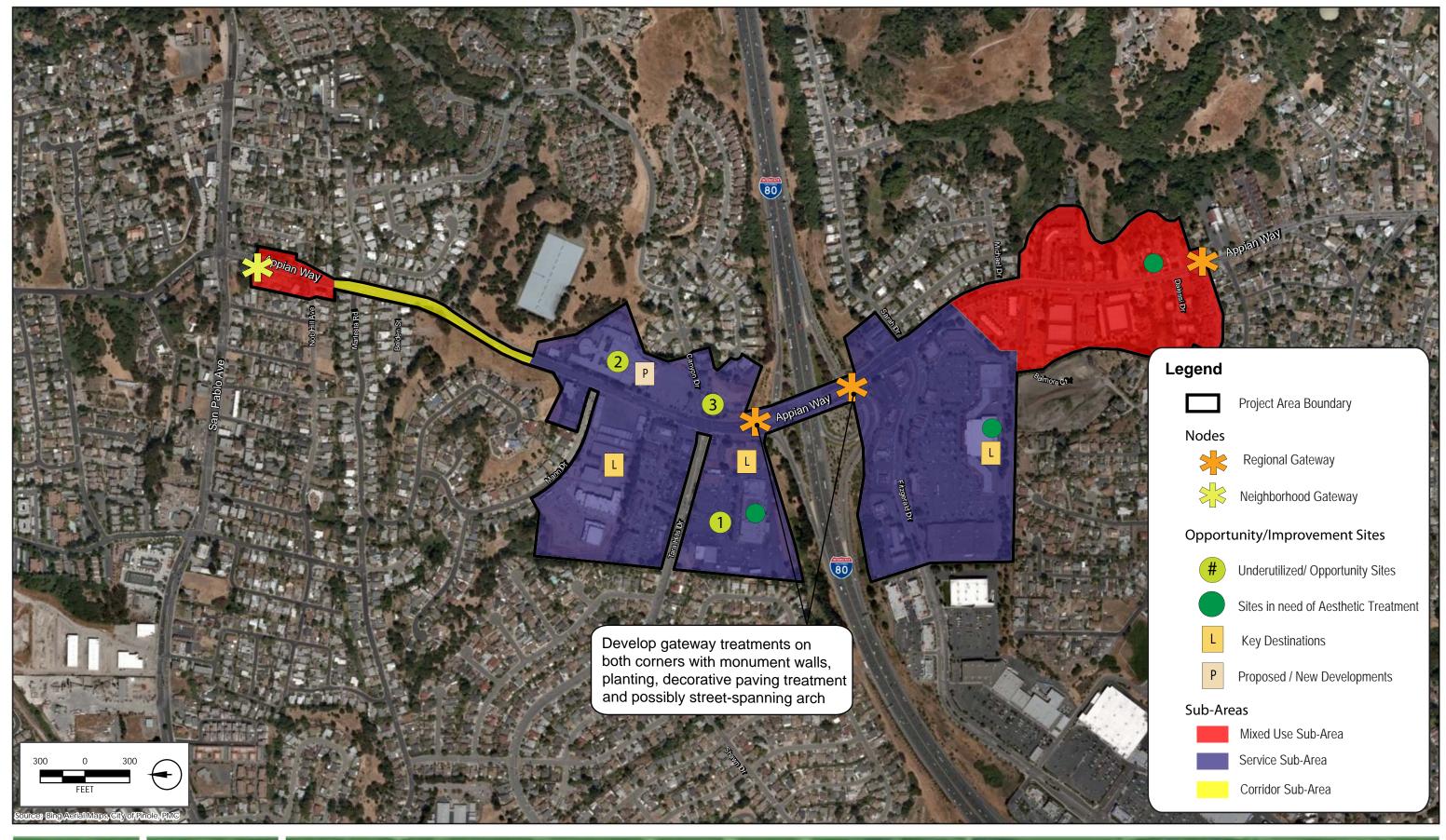
- Gateway to Pinole to and from the surrounding region (I-80 and El Sobrante)
- Landscape, streetscape, and traffic calming measures will improve flow of traffic (including appropriate corridor speeds)
- Location of shopping centers close to I-80 creates potential to draw in regional shoppers with the right mix of tenants and façade and landscaping improvements
- Residential properties fronting Appian Way south of I-80 have redevelopment potential as local-serving retail and office uses
- Pedestrian and bicycle access should be developed on Appian Way to improve access to the "hill" located above the Doctor's Hospital site. The "hill" can also be developed with trails to take advantage of open space within the city limits
- Doctors Medical hospital could be redeveloped as a hotel, large hardware store, or the site of a car dealership to generate tax revenue for the City, while the associated medical offices could be redeveloped for new medical uses or a restaurant
- Redeveloping parcels can dedicate land to creating new street connections

#### Constraints

Physical constraints or obstacles that need to be overcome in the Appian Way Specific Plan Area include:

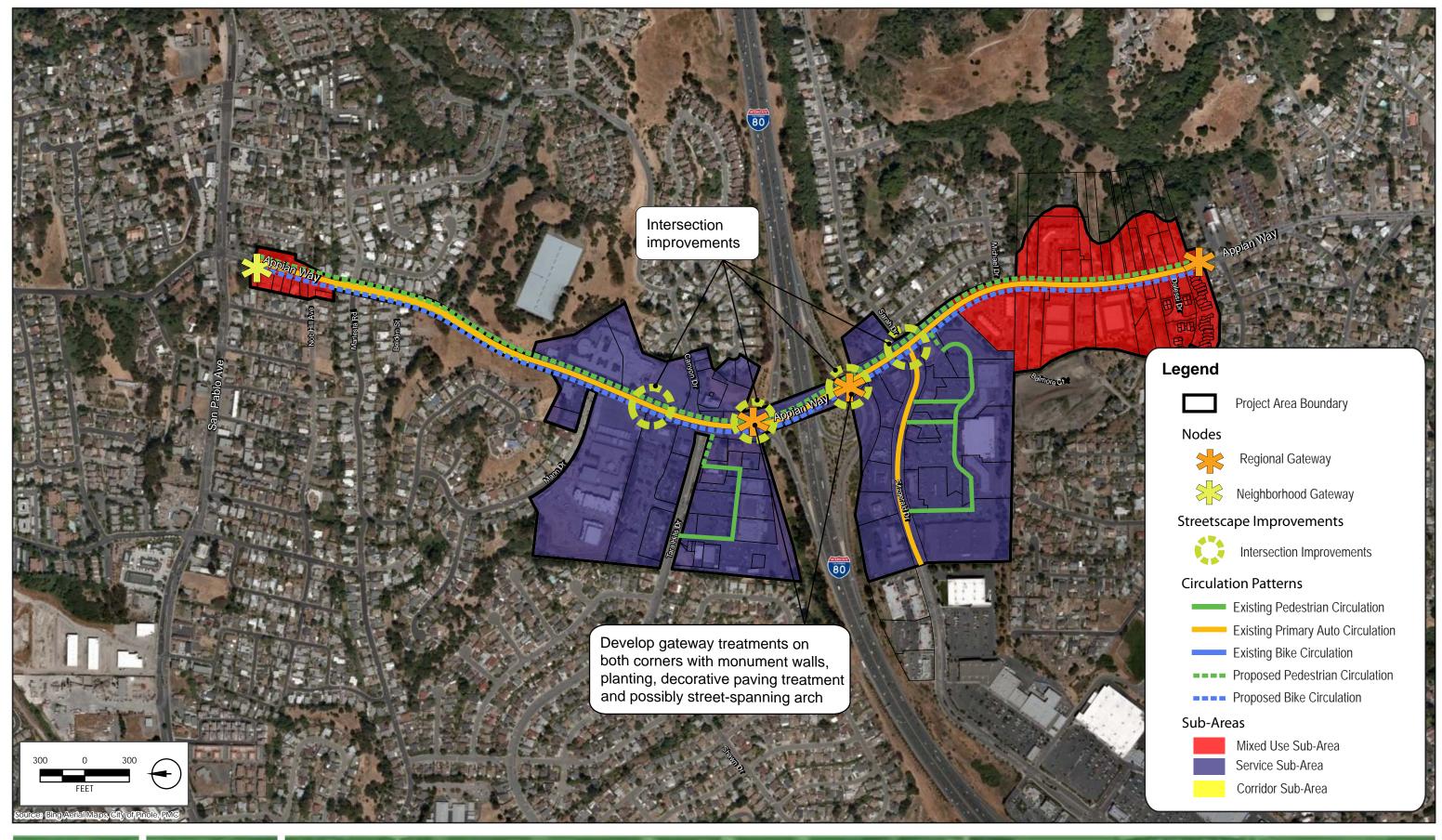
- Automobile speeding on portions of Appian Way
- Traffic congestion occurs near the junior high around 8:30 a.m. and 2:30 p.m. each weekday
- Highway and safety issues created by highway overpass

- Tired-looking strip-mall shopping centers
- Challenging topography
- Disconnect between commercial and residential uses
- Lack of quality public transit options and connections
- Lack of bike lanes
- Appian Way is not pedestrian-friendly
- Most economically/socially depressed area of Pinole; needs to re-establish its identity as an integral part of the Pinole community
- I-80 bisects the corridor













#### **Sub-Areas**

Land use, neighborhood character, development patterns, and topography create distinct subareas within the corridor. Three key sub-areas were identified for the Appian Way Specific Plan Area including (see **Figure 3.6C**):

#### Corridor Sub-Area

- The corridor sub-area is characterized by a high-volume local traffic corridor that is minimally loaded with land uses and punctuated by open space with residential neighborhoods branching off of it.
- As a corridor that serves Pinole and existing development within the unincorporated area of the County, land use and development on this corridor needs to be coordinated between the City and County to ensure land use compatibility and the installation of appropriate circulation improvements.

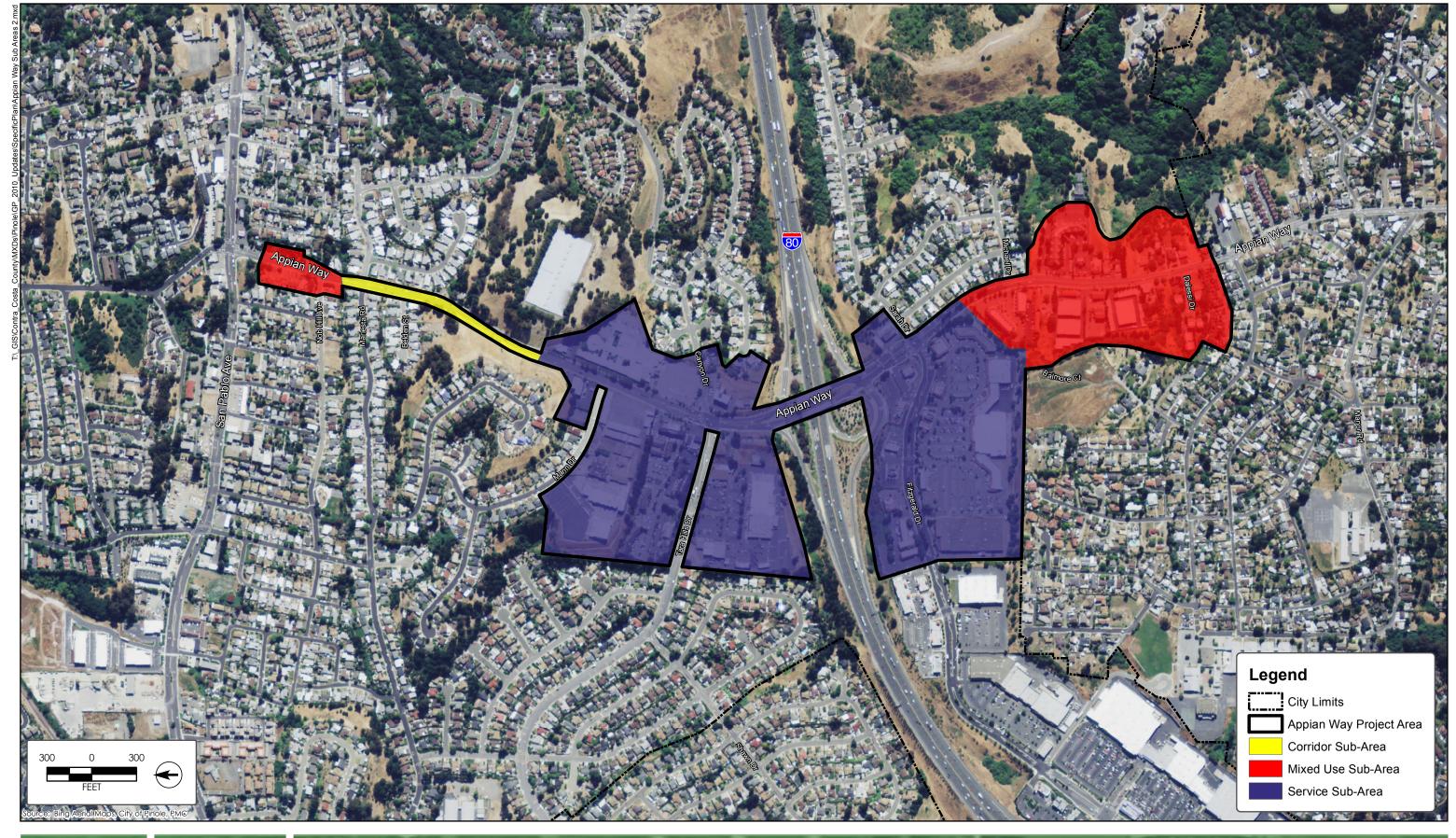
#### Service Sub-Area

- The service sub-area should capitalize on its existing reputation as a regional shopping center by upgrading existing development and attracting increasingly desirable commercial tenants.
- This area would greatly benefit from vehicular and pedestrian circulation improvements in and around the shopping centers.
- This area includes the Doctors Hospital site, the largest commercial use within the sub-area.

#### Mixed-Use Sub-Area

- The mixed-use sub-area should encourage mixed-use development, with an emphasis on local-serving businesses.
- This area would strongly benefit from streetscaping, public art, and signage that will firmly establish its unique identity within the broader Pinole community.

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Figure 4.20

The City of Pinole seeks to attract new development, and retain existing businesses, that serve the community and contributes to the fiscal and economic health of Pinole. In pursuit of this objective, the City of Pinole has prepared this Economic Development Strategy to take advantage of flexible land use standards for the Specific Plan Area and to identify opportunity sites for catalytic development. Central to accomplishing these objectives, the City of Pinole seeks to identify new opportunities for the reuse of or revitalization in the following areas:

- Doctors Hospital;
- Downtown/Old Town by:
  - Supporting mixed-use development at the intersection of San Pablo and Tennant avenues;
     and
  - Modifying Old Town circulation to enhance the pedestrian environment;
- Appian 80 Shopping Center, possibly including residential development;
- Preserve Sugar City; and
- Provide greater development certainty and streamline the development review for, opportunity sites as identified in this Specific Plan.

The following discussion provides information about how these and other economic development objectives can be attained.

#### 4.1 INTRODUCTION

Economic development involves efforts to improve the economic well-being and quality of life of a community by creating and/or retaining jobs and supporting or growing incomes and the tax base. In practice, economic development requires a multifaceted approach to community health and vitality that includes the following.

• <u>Business Climate</u>: A good business climate is one that supports business growth and development.

- Workforce and Education: A highly skilled workforce must be available to support the regional economy and possess the knowledge desired by potential employers.
- <u>Land and Infrastructure</u>: Communities must have an available supply of development-ready land that has zoning, infrastructure, and services to accommodate the needs of business and industry.
- <u>Transportation and Infrastructure</u>: To stay competitive, cities must have a modern and efficient transportation system, providing connections and physical infrastructure for all modes of transportation.
- Quality of Life: Community assets (e.g., natural resources, community facilities, architectural character) must be enhanced to make a community a unique, attractive place to live, work, and play.
- Marketing: A City needs a strategic, dynamic, and consistent marketing message in order to attract private investment.

#### **ECONOMIC DEVELOPMENT GOALS**

The Pinole Three Corridors Specific Plan focuses on the land use, transportation, and infrastructure needs for the City's primary transportation corridors by providing land use direction for an approximately 20-year implementation period. To have value, the Specific Plan must establish realistic land use goals, objectives and standards that reflect and respond

#### Guiding Principles for Economic Development

- Market forces are the dominant drivers of a regional economy.
- Public-sector economic development efforts must focus on factors internal to the workings of the regional economy and under the influence of public policy at any given level of government, be that local, regional, state, or federal.
- Sensible economic development policy must build upon the strengths of the regional economy.
- Economic development should yield real net impacts on growth or, in the short term, the potential for growth.
- The public sector should pursue economic development policies that result in broad benefits for residents and businesses, especially benefits that will continue to have a positive impact even if specific businesses close or move.
- Public-sector economic development efforts should pay attention to the needs of lagging or distressed areas and of groups at the lowest rungs of the economic ladder.
- Public policy should recognize the regional nature of economic development and advance strategies that address challenges and opportunities throughout the regional economy.
- Economic development efforts should address the development potential of places, as well as the needs of people in that place.

to economic conditions. In combination with the land use and design aspects of the Specific Plan, the Economic Development Strategy seeks to implement the following Goals.

- Support commercial and residential developments that can function as the catalyst for economic revitalization and further the City's goals and objectives;
- Enhance Old Town Pinole as a vibrant, pedestrian-oriented commercial destination with a strong civic identity;
- Encourage Transit Oriented Development (TOD) within the Priority Development Areas (PDA) on San Pablo Avenue, Pinole Valley Road, and Appian Way;
- Promote ecological, social, and business sustainability in Pinole by maintaining ecological functions and productivity into the future while meeting the present and future needs of Pinole without compromising the ability of future generations to meet their needs;
- Support economic development that will bring more housing, retail, and employment to the community; and
- Identify opportunity sites and priority projects for the City.

To be a successful economic development tool, the Specific Plan must provide necessary flexibility to respond to economic conditions while ensuring that new development satisfies the community's jobs, housing, character, circulation, and economic development objectives.

#### 4.2 DEMOGRAPHIC TRENDS AND MARKET CONDITIONS

In order to evaluate economic and demographic conditions and trends that will influence economic viability, the City retained the services of Bay Area Economics (BAE) to provide an overview of existing conditions and to identify land uses that would be supported by anticipated trends. The following are the major conclusions from the BAE Economic Background and Opportunities Report.

#### **DEMOGRAPHICS AND EMPLOYMENT**

 Pinole has seen little change in population in recent years and is expected to experience slow population growth in coming decades.

- Job growth is expected to outpace household growth in coming years, leading to an improved balance between commercial and residential uses.
- Future job growth in Pinole is expected to be concentrated in service sectors, consistent with a need for additional development of office and retail space.

#### **REAL ESTATE MARKET**

- Economic conditions in the for-sale residential market are expected to hamper development of new units in the near term.
- Long-term prospects for the for-sale residential market remain strong in Pinole.
- Prospects for rental residential development remain modest in Pinole.
- Current development activity and long-term job forecasts point to opportunities for measured growth in Pinole's office supply in coming years.
- Competitive pressures suggest a need to focus on the redevelopment potential for aging retail centers.
- Current industrial/flex space development in Pinole provides an adequate supply in the shortterm.
- Additional planned development of industrial/flex space is likely to be sufficient to meet future demand based on long-term employment forecasts.

#### 4.3 ECONOMIC DEVELOPMENT STRATEGY

The Specific Plan establishes a land use framework, design and development standards, and implementation actions that will guide development along the San Pablo Avenue, Pinole Valley Road, and Appian Way corridors for the next 20 to 30 years. The economic development objectives are:

- To retain and support existing businesses;
- To bring more and higher-density housing and commercial uses to the Priority Development Areas;

- To make Old Town a more vibrant commercial center;
- To create attractive, accessible streets and sidewalks; and
- To preserve existing industrial uses and attract "green" industry.

This Economic Development Strategy supports these goals by identifying the economic role played by PDAs and various sub-areas within the City, as well as highlighting particular opportunity sites for new development.

Throughout the Specific Plan corridors, but particularly on identified opportunity sites, the City wants to promote projects that:

- Are economically viable (e.g., generate benefits that are adequate to offset costs);
- Have the potential to develop in the near term;
- Have the potential to stimulate other development;
- Provide diverse housing opportunities;
- Strengthen Old Town as a vibrant commercial center;
- Enhance both regional and livable community objectives of supporting transit, pedestrian, and bicycle travel to destinations within the City and connecting to regional alternative transportation options; and
- Take advantage of preferred commercial locations that are near, and have convenient access
  to, the highway, are located on arterial roads that experience high traffic volumes, and that
  benefit from other factors that will support successful commercial (both office and retail)
  development.

The City intends to facilitate projects that meet the above-identified objectives by establishing a land use framework that can direct future development proposals and simplify the land use approval process.

#### **ECONOMIC DEVELOPMENT HIERARCHY**

The City of Pinole recognizes the need to focus available economic development tools in the locations where they will create the greatest returns. For this reason, the Specific Plan focuses on the three primary circulation corridors within the City. Along these corridors, there are a number of opportunity sites that, due to their size, visibility, and/or nature of their use, have particular value as economic development catalysts. There are also opportunity areas that are not site-specific but that are important to the City's future. These opportunity areas include the Priority Development Areas and Old Town Pinole. Accordingly, this analysis is organized to identify implementation tools for Pinole and the Specific Plan corridors as follows:

- <u>Citywide</u>: Market forces will continue to drive the regional economy. Pinole seeks to influence public policy at the local level in a way that builds on the strengths of the region while optimizing benefits for residents and businesses in Pinole.
- Specific Plan Area: This planning sub-area creates opportunities for greater clarity related to land use and development to improve the City's readiness and responsiveness to priority project proposals.
- <u>Priority Development Areas</u>: Priority Development Areas are locations where a community is committed to developing more housing, services, and civic amenities to meet the needs of residents in a pedestrian-friendly environment served by transit. Financial assistance is available for PDA projects that further these objectives.
- Old Town Pinole: By virtue of its historic significance in Pinole, a rich architectural character, and its location at the junction of two primary travel corridors through Pinole, Old Town is a significant community resource that has the potential to be a vibrant, pedestrian-oriented destination.
- Opportunity Sites: Opportunity sites within the three corridors are locations where the City would like to see priority projects develop in the near term.

#### **CITYWIDE STRATEGIES**

Most of the region's economic health is influenced by factors the City of Pinole has little power to influence. Consequently, the City should continue its efforts to remain flexible in order to efficiently participate in the regional economy. The Specific Plan focuses on addressing land use and

infrastructure needs of the City, but addressing the following additional factors can improve local economic health.

- Business Climate: The Pinole Redevelopment Agency has actively acquired properties, facilitated redevelopment, and provided necessary infrastructure to support business. While highly beneficial to business, this activity has also resulted in artificially inflated land values and altered market conditions, making strong private sector leadership difficult, particularly for smaller businesses that have less ability to respond to regulatory burdens and fees. The City should strive to:
  - Support a business climate that is understandable and predictable;
  - Account for small business needs in the formulation of City policies;
  - Maintain the City's focus as customer-driven government; and
  - Support the infrastructure improvements necessary to serve the needs of the business community.
- Quality of Life: Quality of life is determined by a variety of components, some essential, others
  important. These often intangible assets should be factored into the City's land use and
  economic decision making.

### Table 4.1 Quality of Life Factors

- Jobs
- Economic Stability
- High Quality, Affordable Housing
- Mobility (between work, home, school, etc.)
- Dependable Public Services
- Healthy Air and Water Quality
- High-Quality Schools

- Parks and Open Space
- Access to Nature
  - San Pablo Bay
  - Pinole Creek
  - Point Pinole
- Cultural, Recreational Amenities

Marketing: With a gentle climate, an accessible waterfront, thriving youth and senior centers, a historic Old Town, active recreational facilities, and a thriving business community located in close proximity to San Francisco, Pinole is an attractive place to live, work, and play but lacks an identity in the region. Pinole would benefit from a dynamic marketing message that identifies the City's amenities for business attraction.

## SPECIFIC PLAN AREA STRATEGIES CEQA

The Specific Plan provides the City with an opportunity to comprehensively evaluate the environmental impacts of changed land use

#### **Priority Projects**

- Are economically viable;
- Have the potential to develop in the near term;
- Have the potential to stimulate other development;
- Provide diverse housing opportunities;
- Strengthen Old Town as a vibrant commercial center;
- Take advantage of high traffic areas; and
- Enhance the Priority Development Area objectives of supporting transit, pedestrian, and bicycle travel.

policies and programs at a level of detail that will ensure that future development, provided it is consistent with the Specific Plan, will not result in unanticipated negative environmental impacts. To ensure that this environmental protection remains in place, the City will review future projects that implement, and are consistent with, the Specific Plan, and where possible, exempt them from further environmental review or conduct environmental review to supplement the Specific Plan Environmental Impact Report (EIR).

#### Streamlined Administration

To encourage public benefit projects and to ensure successful development of opportunity sites, the City will develop a facilitated review process for priority projects. The facilitated review is intended to provide development certainty for proposals that are deemed desirable to the City and that further the objectives of this Specific Plan.

#### **Economic Development Incentives**

The City of Pinole already has attributes that are strategically important to business: character, climate, and convenience, easy access to Interstate 80 and the Richmond Parkway, and proximity to employment centers in the East Bay as well as in San Francisco and Marin. Valuable natural resources (e.g., San Pablo Bay, open space, Pinole Creek, and trails) and a well-run recreation

program contribute to an amenity package that needs to be better communicated to the business community. The City would also benefit from cultivating local businesses and providing development assistance to green industry and mixed-use developers.

#### **Old Town Strategies**

The activities and amenities of Old Town are essential to supporting a transit-oriented lifestyle. Diverse land uses place office, retail, civic, residential, cultural, and recreational uses within walking distance of each other and transit services. To be successful, residential and commercial densities will have to generate adequate daytime and evening populations to support commerce and transit. Public spaces within Old Town also need to be reconsidered in order to change San Pablo Avenue through Old Town from a through-traffic bypass to a main street and to maximize pedestrian activity to revitalize commercial activity. Important to revitalizing the Old Town are anchor activities or uses that draw people to the area and mesh with the unique character of the downtown.

Currently, civic uses are the economic engine in Old Town, with City Hall, the Public Safety Building, the Post Office, Youth Center and Senior Center, Fernandez Park, and the Town Hall Community Theater all located within walking distance of one another. Planned future development within Old Town includes a retail project at the corner of Tennent and San Pablo avenues. Other development opportunities include housing along Tennent Avenue across from the Youth Center. Going forward, small-scale residential, office, and retail mixed-use projects will continue to complement existing development in Old Town. Potential commercial tenants in the Old Town area include sit-down restaurants, specialty retailers (e.g., used bookstores, locksmiths, pet shops), and personal and professional services, including salons and tax preparation services. Redefining the major downtown intersection of San Pablo Avenue and Tennent Avenue with commercial buildings at each corner will provide a visual anchor and sense of place currently lacking in the City's core.

Any effort to revitalize Old Town must be accompanied by a parking plan and strategy that adequately accommodates new parking demand while not over-parking the downtown. Opportunities for shared parking and recognition that pedestrian travel between well-located parking facilities and the commercial destination is what makes for a walkable downtown are important to downtown's resurgence. In addition to modifying circulation and establishing a parking strategy, the City also needs to:

- Make the economic vitality of Old Town a priority by attracting anchor uses and activities to the downtown and growing clusters of anchor uses in the downtown such as cultural and civic facilities, specialty restaurants, and retail;
- Support the economic health of Old Town by encouraging more, and higher-density, housing within walking distance of Old Town;
- Enhance multi-modal, pedestrian-friendly access to Old Town by providing bicycle lanes, wider sidewalks with amenities, bus stops and shelters; and
- Establish the identity of Old Town as a vital area for commerce through wayfinding graphics, public art, and thematic streetscape elements.

#### A. Circulation

To implement the Specific Plan vision for Old Town, San Pablo Avenue will have to be recast as a main street rather than a bypass, with a distinct, vibrant, pedestrian-oriented destination for commerce, civic activity, and recreation. To accomplish this, San Pablo Avenue needs to be redesigned to discourage bypass traffic, slow automobiles, and promote pedestrian activity on the street. This could be accomplished by reducing vehicle travel lanes to one in each direction while providing space for bicycle circulation, widened sidewalks, and public amenities.

#### B. Parking

In order to provide development opportunity and adequate parking in Old Town, the Redevelopment Agency has assembled property for shared parking and the City has promulgated standards that rely on this parking rather than compelling each business to satisfy parking demand on-site. These two actions maintain the traditional downtown commercial character and pedestrian nature of this business district. The City periodically evaluates parking conditions to ensure that adequate parking remains available, and these studies continue to demonstrate that there is adequate parking for Old Town. Redevelopment of Old Town under the Specific Plan has the potential to increase parking demand beyond the present parking supply. To balance pedestrian-friendly design with future parking needs, the parking management strategy for Old Town:

- Defines parking as being adequate if parking demand does not exceed 85 percent of the available capacity within a two-block radius of a project;
- Will implement parking controls (e.g., time limits, metered parking, remotely located employee parking) before seeking to expand parking capacity;

Figure 4.1
San Pablo/ Tennent Avenue - Before Improvements



Figure 4.2
San Pablo/ Tennent Avenue - After Improvements



- Will continue to monitor parking supply and demand on a regular basis; and
- Will consider expanding available shared public parking (e.g., a parking garage) in order to relieve individual projects of the obligation to provide parking on-site as determined by a parking management study.

#### 4.4 OPPORTUNITY SITES

Opportunity sites will, with appropriate development, substantially further the City's objectives related to economic vitality and diverse housing opportunities. The following discussion identifies the area, land use, and desirable future development prototypes for the opportunity sites along the San Pablo Avenue corridor. The opportunity sites were selected with the following criteria in mind:

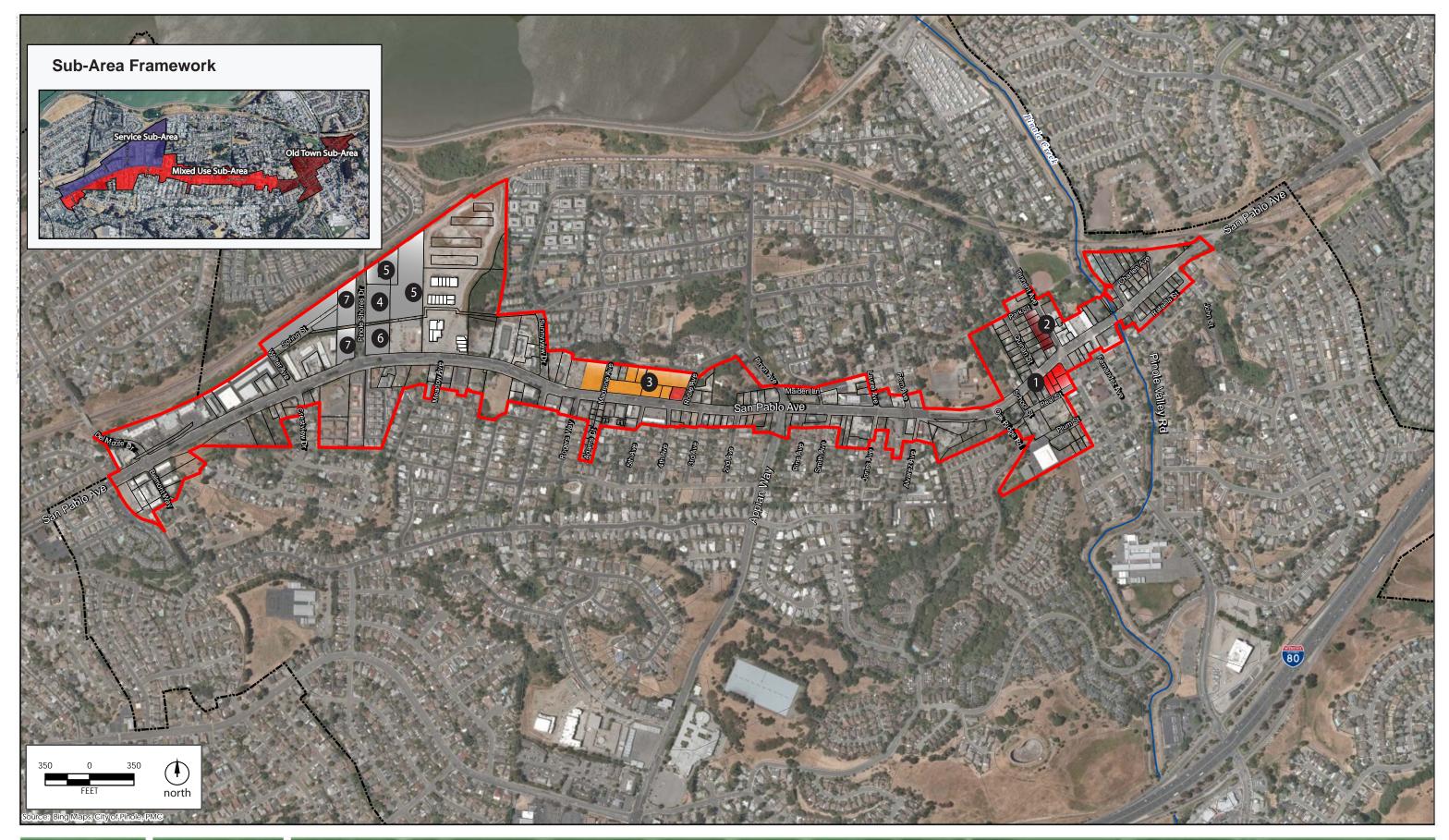
- Ability to catalyze other development. Due to their location and/or size, development on these
  opportunity sites would set the tone for land uses in the area and help spark additional activity
  along the corridor.
- "Anchor" properties. Development of these opportunity sites was seen as crucial to establishing the corridor's character.
- Readiness to be developed. Sites that were either vacant or underutilized, and/or that require
  minimal land assembly are considered prime candidates for economic development. These
  "low-hanging fruit" would require less effort by a developer to build out the property.

The inclusion of opportunity sites within the Three Corridors Specific Plan is intended to encourage, but not require land use change. Developed properties with pre-existing uses identified as opportunity sites may continue. Existing and future property owners of opportunity sites will decide when and if change occurs in the future.

#### SAN PABLO AVENUE CORRIDOR

The primary objectives for the San Pablo Avenue corridor are to attract higher-density residential development to key locations that support local-serving and specialty business, to grow community and civic uses to anchor Old Town, and to support industrial use at the south end of the avenue that can serve the local and regional demand for green industries.

Figure 4.3 identifies seven sites along the San Pablo Avenue corridor that provide an opportunity for development to occur in a manner that would further City objectives. Figures 4.4 – 4.10 provide detailed information about these opportunity sites and the type of development the City would encourage.

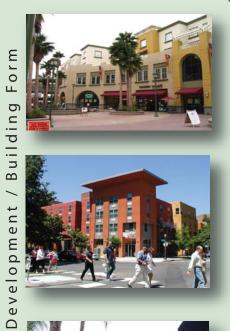






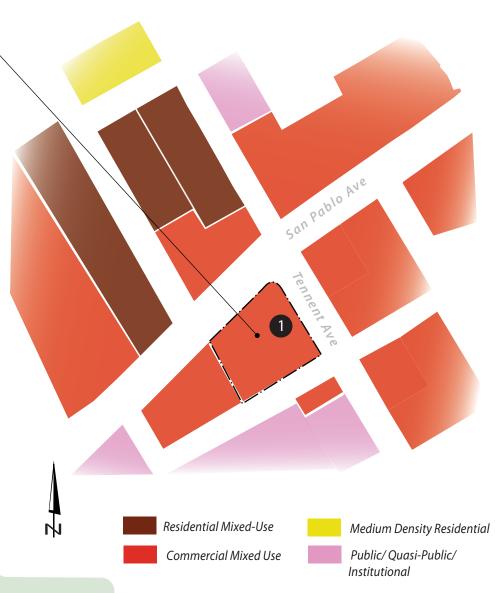
### Fig 4.4 San Pablo Avenue - Old Town District: Opportunity Site 1

### **Opportunity Site 1**











Existing Conditions

Site Information		
Size:	0.79 Acres	
Zoning:	CMU	
Height Limit:	35'	
Minimum Frontage:	80%	
Development Examples		
Commercial:	5,000- 20,000 Sq. Ft.	
Residential:	18 Units	
FAR	1.5 - 1.75	
Desired Uses: Residential, Restaurant,		

**Bookstore** 

### Fig 4.5 San Pablo Avenue - Old Town District: Opportunity Site 2

### **Opportunity Site 2**

Development / Building Form









Existing C	onditions
------------	-----------

Site Information			
Size:	0.69 Acres		
Zoning:	RMU		
Height Limit:	35′		
Minimum Frontage:	80%		
Development Examples			
Residential:	10 Units		
FAR	NA		
Desired Uses: Residential Use			

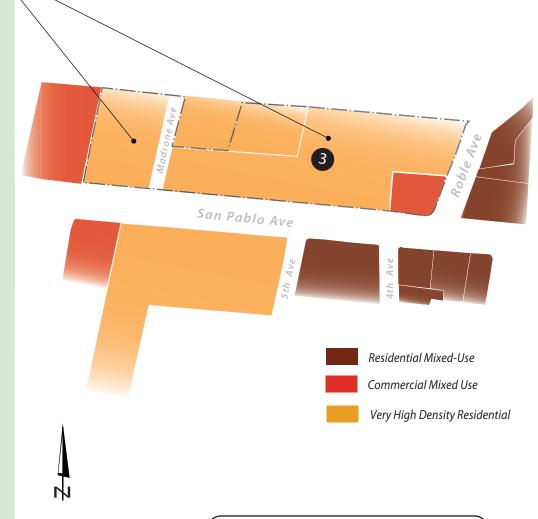
### Fig 4.6 San Pablo Avenue - Mixed Use District: **Opportunity Site 3**

### **Opportunity Site 3**

Development / Building Form





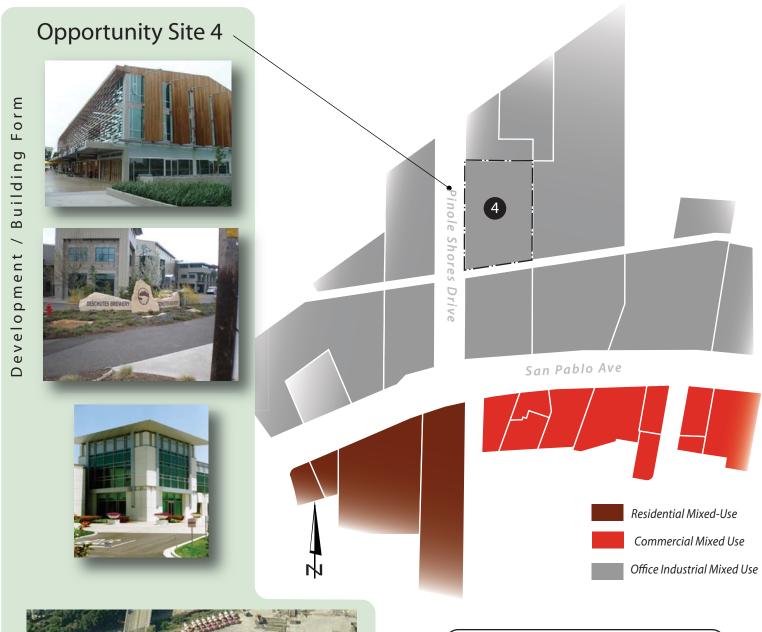




**Existing Conditions** 

Site Information			
Size:	3.72 Acres		
Zoning:	VHDR & CMU		
Height Limit:	50′		
Minimum Frontage:	60%		
Development Examples			
Residential:	128 Units		
Commercial:	3,000 Sq. Ft.		
FAR	N/A		
Desired Uses: Residential Use			

## Fig 4.7 San Pablo Avenue - Service District: Opportunity Site 4



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Existing Conditions

Site Information			
Size:	1.28 Acres		
Zoning:	OIMU		
Height Limit:	50′		
Minimum Frontage:	30%		
Development Examples			
Industrial:	9,000 SF		
FAR	25%		
Desired Uses: Green Industry			

## Fig 4.8 San Pablo Avenue - Service District: Opportunity Site 5

# Opportunity Site 5 Form Development / Building Pinole Shores Drive San Pablo Ave Residential Mixed-Use Commercial Mixed Use Office Industrial Mixed Use



**Existing Conditions** 

Site Information		
Size:	4.5 Acres	
Zoning:	OIMU	
Height Limit:	50′	
Minimum Frontage:	30%	
Development Examples		
Industrial:	26,000 Sq. Ft.	
FAR	25%	
Desired Uses: Green Industry		

## Fig 4.9 San Pablo Avenue - Service District: Opportunity Site 6



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Site Information						
Size:	1.04 Acres					
Zoning:	OIMU					
Height Limit:	50′					
Minimum Frontage:	30%					
Development Examples						
Industrial:	11,000 Sq. Ft.					
FAR	25%					
Desired Uses: Office / Flex						

## Fig 4.10 San Pablo Avenue - Service District: Opportunity Site 7





**Existing Conditions** 

Site Information						
Size:	3.99 Acres					
Zoning:	OIMU					
Height Limit:	50′					
Minimum Frontage:	30%					
Development Examples						
Commercial:	26,000 Sq. Ft.					
FAR	15%					
Desired Uses: Green Industry, Retail						

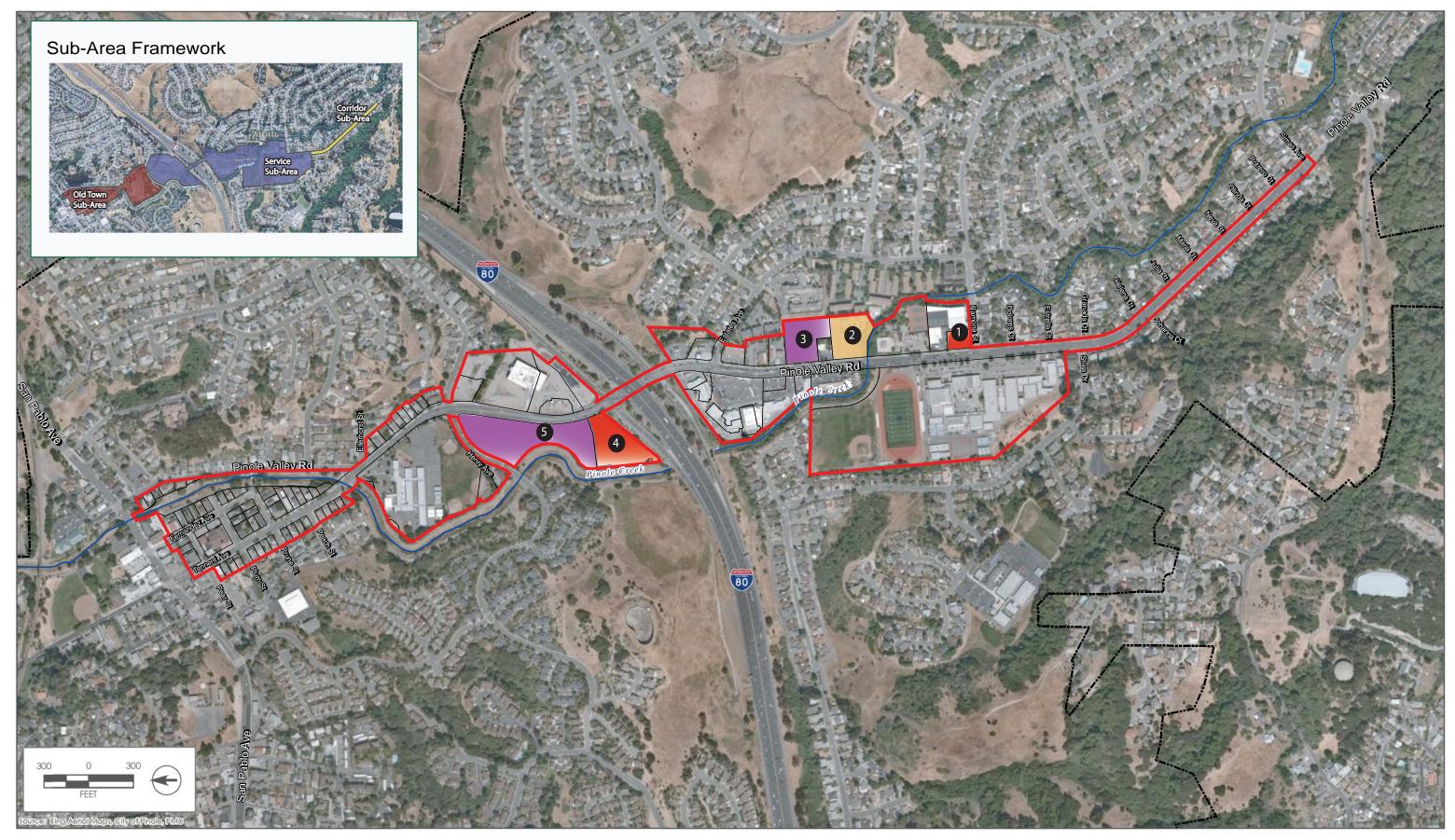
#### PINOLE VALLEY ROAD CORRIDOR

This corridor contains educational, medical office, retail, recreation/cultural, and residential uses. It provides convenient access to and from I-80 and contains Pinole Creek.

The primary objectives for the Pinole Valley Road corridor are to capitalize on revitalized retail centers south of Interstate 80, encourage new medical office use north of Interstate 80, and revitalize Pinole Creek as a natural amenity with a trail system connecting the commercial uses adjacent to Interstate 80 via downtown to San Pablo Bay and the Bay Trail.

**Figure 4.11** identifies five sites along the Pinole Valley Road corridor that provide an opportunity for development to occur in a manner that would further City objectives. **Figures 4.12 – 4.16** provide detailed information about these opportunity sites and the type of development the City would encourage.

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## Fig 4.12 Pinole Valley Road - Service District: Opportunity Site 1

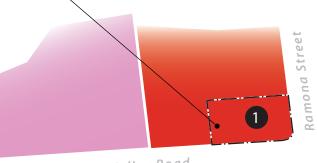
### **Opportunity Site 1**





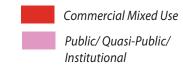
Development / Building Form





Pinole Valley Road





75' 0' 75'

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Site Information						
Size:	0.45 Acres					
Zoning:	CMU					
Height Limit:	50′					
Minimum Frontage:	30%					
Development Examples						
Commercial:	3,000 Sq. Ft.					
FAR	30%					

**Desired Uses:** Convenience Retail, Mixed Use with Housing

## Fig 4.13 Pinole Valley Road - Service District: Opportunity Site 2

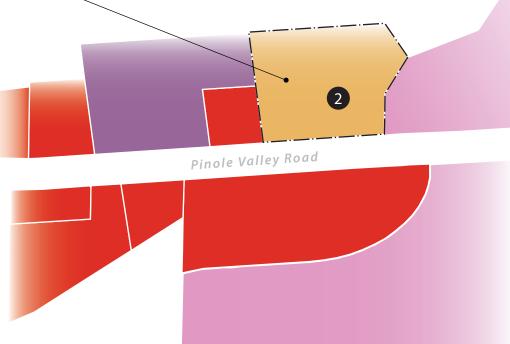
### **Opportunity Site 2**





Development / Building





Commercial Mixed Use

Office Professional Mixed Use



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Site Information							
Size:	1.77 Acres						
Zoning:	HDR						
Height Limit:	50′						
Minimum Frontage:	30%						
Development l	Examples						
Residential:	53 units						
FAR	30%						
Desired Uses: Residential Uses							

High Density Residential

Public/ Quasi-Public/

Institutional

## Fig 4.14 Pinole Valley Road - Service District: Opportunity Site 3

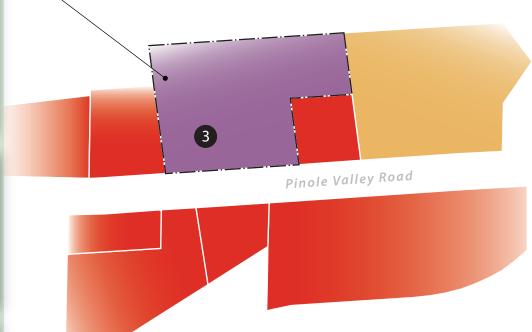
### Opportunity Site 3





Develoment / Building Form







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Site Information							
Size:	1.75 Acres						
Zoning:	OPMU						
Height Limit:	50′						
Minimum Frontage:	30%						
Development I	Examples						
Commercial:	68,750 Sq. Ft.						
FAR	50%						
Desired Uses: Office Mixed Use							

Commercial Mixed Use

High Density Residential

Office Professional Mixed Use

## Fig 4.15 Pinole Valley Road - Service District: Opportunity Site 4

### Opportunity Site 4

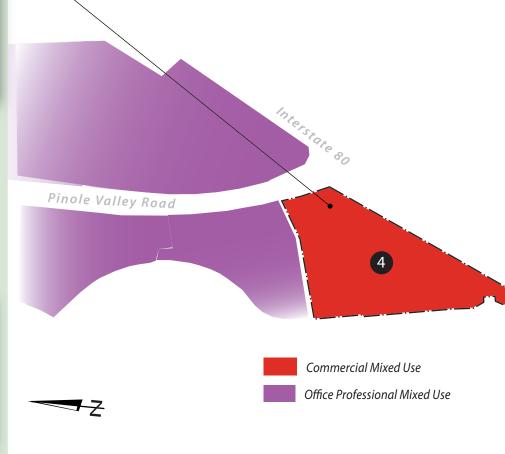


Form

Development / Building









	l e e e e e e e e e e e e e e e e e e e					
Size:	2.24 Acres					
Zoning:	CMU					
Height Limit:	50′					
Minimum Frontage:	30%					
Development Examples						
Commercial:	30,050 Sq. Ft.					
FAR	30%					
<b>Desired Uses:</b> Medical Offices, Restaurant						

**Site Information** 

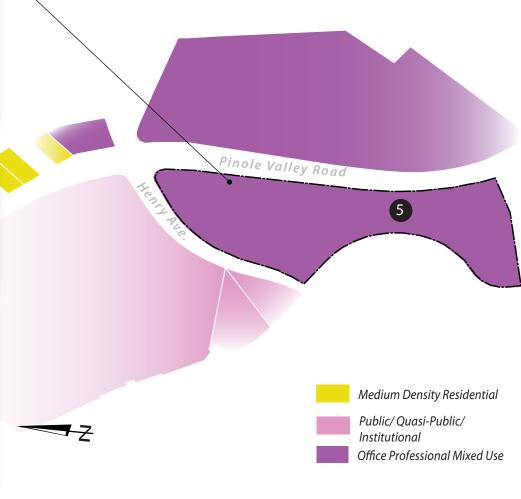
### Fig 4.16 Pinole Valley Road - Service District: Opportunity Site 5

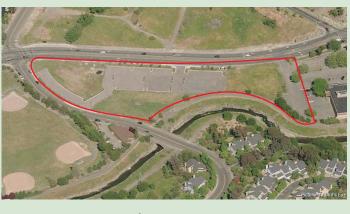
### Opportunity Site 5











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Site Information								
Size:	4.51 Acres							
Zoning:	OPMU							
Height Limit:	50′							
Minimum Frontage:	30%							
Development I	Examples							
Commercial:	70,000 Sq. Ft.							
FAR	40%							
<b>Desired Uses:</b> Medical Offices								

# **ECONOMIC DEVELOPMENT STRATEGY**

### **APPIAN WAY**

This corridor contains educational, medical office, retail, and residential uses that have convenient access to and from I-80.

The primary objectives for the Appian Way corridor are to support development that provides jobs, primarily by reusing the Doctors Medical Center site, and to revitalize retail at Appian 80 and to the south of Interstate 80.

Figure 4.17 identifies three sites along the Appian Way corridor that provide an opportunity for development to occur in a manner that would further City objectives. Figures 4.18 – 4.20 provide detailed information about these opportunity sites and the type of development the City would encourage.

# ECONOMIC DEVELOPMENT STRATEGY

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# Opportunity Sites: Potential Development Diagram

# Fig 4.18 Appian Way - Service District: Opportunity Site 1

# **Opportunity Site 1**

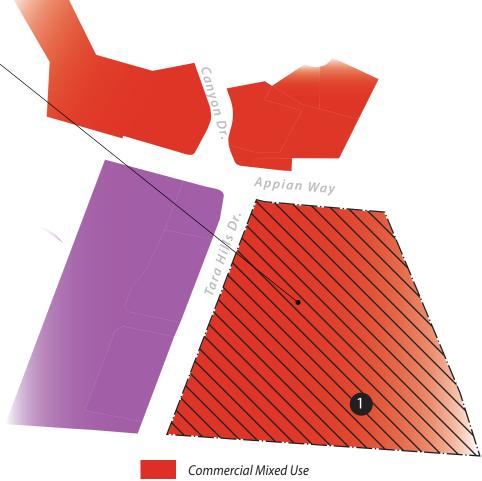


Form

Development / Building







Office Professional Mixed Use

High Density Residential Overlay (A minimum of 2.5 acres within the HDR Overlay area shall be reserved for residential development)

150 0' 150

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Site Information				
Size:	13.0 Acres			
Zoning:	CMU & HDR overlay			
Height Limit:	75′			
Minimum Frontage:	60%			
Development Examples				
Residential:	75 units			
Commercial:	140,000 Sq. Ft.			
FAR	40%			
Desired Heart Mixed Hear Desidential				

**Desired Uses:** Mixed Use Residential and Retail

# Opportunity Sites: Potential Development Diagram

# Fig 4.19 Appian Way - Service District: Opportunity Site 2

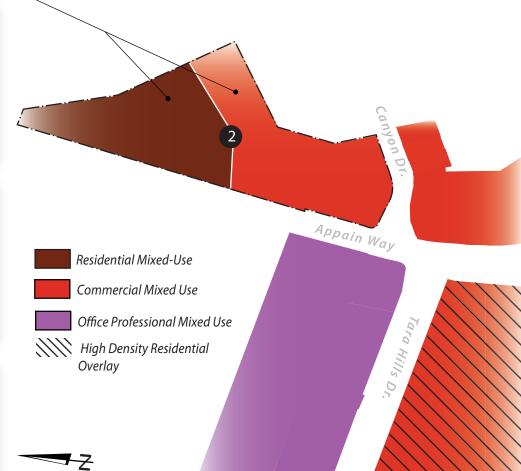
# Opportunity Site 2





Development / Buildling Form







Existing Conditior	۱ S	
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Site Information			
Size:	7.42 Acres		
Zoning:	CMU + RMU		
Height Limit:	75′		
Minimum Frontage:	60%		
Development Examples			
Commercial:	75,000 Sq. Ft.		
Residential:	225 Units		
FAR	100%		
<b>Desired Uses:</b> Hospital, Out-Patient, or Other Medical Facility, Lodging,			

Mixed Use

# Opportunity Sites: Potential Development Diagram

# Fig 4.20 Appian Way - Service District: Opportunity Site 3



# **ECONOMIC DEVELOPMENT STRATEGY**

### 4.5 DEVELOPMENT POTENTIAL

Population and employment growth in Pinole will likely be slower than the County and the region due to the largely built-out nature of the City. Based on ABAG population, housing, and employment projections, economic trends and conditions in the region, Bay Area Economics estimated that there will be demand for the following development within the next 15 years:

Office: 75,000 – 100,000 square feet
Retail: 125,000 square feet
Industrial: Existing Supply is Adequate
Lodging: Undetermined

The Specific Plan increases development potential within the three corridors. Predicting how much additional commercial square footage and how many additional residential units would develop under the Specific Plan in the next 20 to 30 years is difficult because the land use is deliberately flexible to accommodate a broad range of market conditions. The percentage of commercial and residential use within the mixed-use zones could vary broadly, while lot coverage will be higher in locations where off-site parking facilities (e.g., a parking garage in Old Town) obviate the need for parking on-site and lower for some commercial uses (e.g., industry) that are not located entirely within a structure.

Appendix A describes the assumptions that were used to estimate the development that could occur within the San Pablo Avenue, Pinole Valley Road, and Appian Way corridors. In projecting development potential for the planning horizon of 2030, it was assumed that demand for commercial and industrial development would limit buildout more than land use constraints, but that no such limit on demand would apply to residential development. Because the proposed land use designations would accommodate more commercial development than is expected to occur in during the General Plan cycle, these development projections limit growth in the commercial and office sectors to 150,000 square feet above the existing land use capacity by the year 2030. While actual development in Pinole will be influenced by a number of market and regulatory factors, the Specific Plan is expected to increase the potential for development beyond that allowed by the 1995 General Plan as follows:

 Residential:
 1,076 Units

 Retail:
 150,000 s.f.

 Office:
 150,000 s.f.

 Industrial:
 0 s.f.

# 5.0 CIRCULATION

5.0	Circula	ıtion	
5.1		ew	
5.2		tion Goals and Policies	
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### 5.1 OVERVIEW

Mobility is essential to support the vision and goals for the planning area. This chapter presents improvement opportunities for the Appian Way, Pinole Valley Road, and the San Pablo Avenue Corridor thoroughfares. In addition to suggested improvements along the three corridors, the Specific Plan will examine improvement opportunities in Old Town Pinole and elsewhere to address truck traffic issues, traffic calming, bicycle facilities, parking, and transit issues and opportunities.

The Specific Plan strive to create an environment that is functional and comfortable for all forms of transportation including, pedestrians, bicyclists, vehicles, and transit riders. Furthermore, the Specific Plan will independently address Old Town Pinole to create a pedestrian-friendly destination, to cultivate a lively downtown environment with an engaged, active, human oriented streetscape where the automobile is not the primary emphasis for people who live, work and shop in Old Town Pinole.

### 5.2 CIRCULATION GOALS AND POLICIES

The specific plan for City of Pinole –San Pablo Avenue, Pinole Valley Road and Appian Way Corridors, seeks to accomplish the following objectives relating to circulation:

- To facilitate the transition of the San Pablo, Pinole Valley Road and Appian Way corridors so
  that they complement the land uses and development pattern planned for the corridors
  through implementation of this specific plan;
- To maintain and improve vehicular traffic circulation within the Specific Plan Areas to safely and efficiently move both local and through traffic to its destination;
- Accommodate future demand for circulation by all modes of transportation;
- To implement traffic calming techniques in specific areas to improve traffic and pedestrian safety; and,
- To create attractive urban streetscapes with design and amenities that are visually compatible
  with and enhance planned private development pursuant to this specific plan, and that support
  pedestrian use and outdoor activities.

To accomplish these objectives, the Specific Plan is proposing following policies:

CIRCULATION POLICY 1 The City will implement a comprehensive plan for a coordinated street circulation system that will provide for the safe and efficient movement of people and goods within and through the Specific Plan Areas.

CIRCULATION POLICY 2 All future roadway and intersection improvements will consider pedestrian and traffic safety first and foremost. Modifications to the standards, regulations, and/or guidelines contained herein are permitted in those instances where safety is at issue.

**CIRCULATION POLICY 3** The City will implement traffic calming measures, as designated in this Specific Plan, to facilitate the creation of a pedestrian friendly environment throughout the Specific Plan Area in general, and in specified pedestrian-oriented retail, mixed-use and residential development areas along San Pablo Avenue.

**CIRCULATION POLICY 4** The City will continue to oversee circulation system improvements within the Specific Plan Areas that is capable of adequately accommodating a reasonable increase in future traffic demands.

CIRCULATION POLICY 5 The City will discourage through traffic and truck traffic for those roadway segments that are not designed to handle such traffic.

CIRCULATION POLICY 6 The City will employ measures that will discourage through traffic on local streets.

### CIRCULATION POLICY 7

The City will ensure that there are clear rights-of-way for safe passage of pedestrians and bicyclists using San Pablo Avenue, Pinole Valley Road and Appian Way.

CIRCULATION POLICY 8 The City will provide access for people who are disabled or impaired at all pedestrian crosswalks, and will include audible pedestrian crossing signal devices, and other appropriate safety measures at signalized pedestrian crosswalks where the City Engineer deems them appropriate.

**CIRCULATION POLICY 9** Any future roadway and intersection improvements undertaken by the City shall be in conformance to, and consistent with, this specific plan.

CIRCULATION POLICY 10 The City will continue to analyze traffic congestion and evaluate strategies to improve the efficiency of the local transportation and circulation system.

### 5.3 VEHICULAR CIRCULATION

San Pablo Avenue is the main arterial roadway in the City of Pinole and serves as a thoroughfare for the entire East Bay running from downtown Oakland north through the City of Pinole and terminating in the unincorporated community of Crockett. Within the City limits of Pinole, San Pablo Avenue is a four lane arterial running east-west along the northern portion of Pinole providing a primary circulation route that provides linkages between the City and the regional circulation system. San Pablo Avenue runs though industrial, commercial and residential areas and directly through the heart of Pinole in the Old Town District.

The Pinole Valley Road Corridor is bound by San Pablo Avenue on the north and Simas Avenue to the south. Pinole Valley Road is a 4 lane arterial with on-street parking permitted on some segments of the roadway.

The Appian Way Corridor is bounded by San Pablo Avenue on the north and Dalessai Drive on the south. Appian Way is a 4-lane arterial with on street parking permitted on some roadway segments. Over the years Appian Way has become a major local shopping hub, a destination for medical services, and is site of Pinole Middle School.

### **EXISTING TRAFFIC CONDITIONS**

### Level of Service Standards

The operating conditions experienced by motorists are described as "levels of service" (LOS). LOS provides a qualitative measure of circulation efficiency that is influenced by a number of factors, including speed and travel time, traffic interruptions, freedom to maneuver, driving comfort, and convenience. Levels of service are designated "A" through "F" from best to worst, which cover the entire range of traffic operations that might occur. Levels of service "A" through "E" generally represent traffic volumes at less than roadway capacity, while LOS "F" represents over capacity and/or forced flow conditions.

Level of Service	Description
А	Level of service A represents free flow. Excellent level of comfort, convenience and freedom to maneuver.
В	Level of service B is in the range of stable flow, but the presence of other road users in the traffic stream causes noticeable reductions of comfort, convenience, and maneuvering freedom.
С	Level of service C is in the range of stable flow, but the operation of individual users is significantly affected by others in the traffic stream.
D	Level of service D represents high-density, but stable flow. Users experience severe restriction in speed and freedom to maneuver, with poor levels of comfort and convenience.
E	Level of service E represents operating conditions at or near the capacity level. All speeds are reduced to a low, but relatively uniform value. Freedom to maneuver is difficult, with users experiencing frustration and poor comfort and convenience. Unstable operations are frequent, where small increases or minor perturbations to the traffic flow can cause breakdown conditions.
F	Level of Service F is used to define forced or breakdown conditions. This condition exists wherever the amount of traffic approaching a point exceeds the amount that can traverse a point. Roadways store long queues behind such locations, with traffic advancing in stop-and-go "waves".

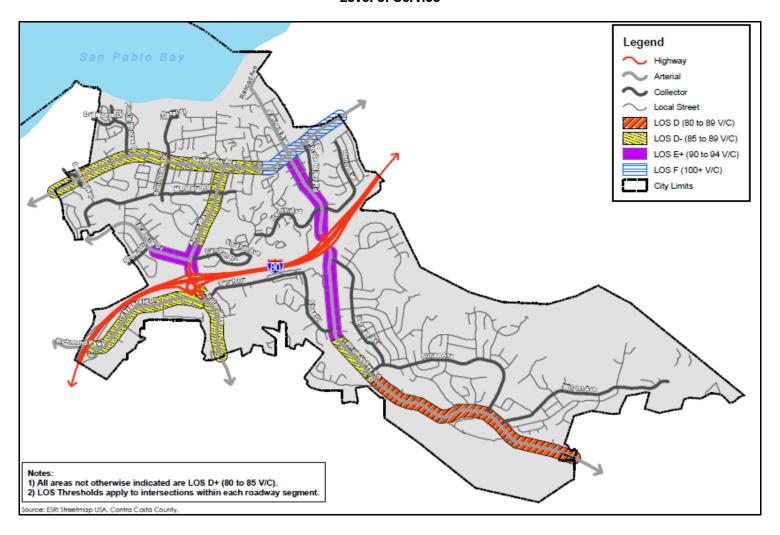
### **Volume to Capacity Ratios**

Another measure of circulation efficiency is Volume to Capacity Ratios. This is a quantitative standard that compares the number of vehicles that pass through an intersection or road segment to the capacity of roadway improvements.

### **City Thresholds**

To encourage balanced transportation, the City of Pinole has LOS and Volume to Capacity Ratios (V/C Ratios) as the minimum acceptable standards along its local, collector, and arterial roadways.

Figure 5.1 Level of Service



### **Roadway Segments**

Roadway levels of service were evaluated at 11 key roadway segments. The corresponding levels of service are shown in Table 5.1 below. Under existing conditions, all roadways are operating within City standards, with the exception of Appian Way south of Tara Hills Drive. At this location, the daily volumes result in LOS E conditions.

Table 5.1

Roadway Level of Service – Existing Conditions

Otro et Norre	Leading	Existing Conditions		
Street Name	Location	LOS	V/C	Daily Volume
Regional Arterials				
San Pablo Ave	West of Del Monte Dr/Belmont Wy	С	0.50	17,100
San Pablo Ave	West of Appian Wy	С	0.60	20,600
San Pablo Ave	East of Pinole Valley Rd	D	0.85	20,900
Appian Way	South of Tara Hills Dr/Canyon Dr	E	0.99	34,300
Appian Way South of Michael Dr		D	0.84	27,500
Arterials				
Pinole Valley Rd	North of Henry Ave	D	0.91	14,100
Pinole Valley Rd	South of Estate Ave	С	0.58	19,000
Pinole Valley Rd	ey Rd South of Wright Ave		0.21	3,200
Henry Ave	East of Ridgecrest Rd	С	0.13	1,700
Fitzgerald Dr	d Dr West of Appian Wy		0.52	18,100
Shea Dr West of Pinole Valley Rd		С	0.27	3,500

Source: Dowling Associates, Inc. 2009

### Intersections

Intersection levels of service were evaluated at 20 key intersections in the Specific Plan planning areas for the AM and PM peak hours. The corresponding levels of service at the intersections are shown in **Table 5.2**.

Under the existing conditions, all study intersections operate at LOS C or better during the AM and PM peak hours. The capacities of these key intersections are more than adequate to meet the current peak hour traffic demands.

### Interchanges

Operations at the I-80 interchanges of Appian Way and Pinole Valley Road were observed for 15-minute periods during a weekday morning commute time. There was one observed instance at southbound Appian Way where the queue from the traffic signal caused vehicle backups for right-turning vehicles from Tara Hills Drive. No spillovers onto city roadways were observed.

Table 5.2 Existing Intersection Level of Service

#	Intersection	Time Period	LOS	V/C Ratio
	Del Mente Drive et Can Deble Avenue	AM	Α	0.498
1	Del Monte Drive at San Pablo Avenue	PM	Α	0.404
2	Pinole Shores Drive at San Pablo Avenue	AM	Α	0.441
2	Pinole Shores Drive at San Pablo Avenue	PM	Α	0.340
3	Supplyious Drive et Sen Bable Avenue	AM	Α	0.388
3	Sunnyview Drive at San Pablo Avenue	PM	Α	0.385
4	A : W O . D . I . A	AM	Α	0.433
4	Appian Way at San Pablo Avenue	PM	Α	0.593
5	Tennent Avenue at San Pablo Avenue	AM	Α	0.551
5	ennent Avenue at San Pablo Avenue	PM	Α	0.427
6	inole Valley Road at San Pablo Avenue	AM	Α	0.569
0		PM	Α	0.552
- B: I	nole Valley Road at Tennent Avenue	AM	Α	0.416
7	Findle valley hoad at Terment Avenue	PM	Α	0.324
6 B: I )	Pinole Valley Road at Henry Avenue	AM	Α	0.407
8	Findle valley hoad at herity Avenue	PM	Α	0.390
9	Pinela Vallay Bood at I 90 weethound rampa	AM	Α	0.543
9	Pinole Valley Road at I-80 westbound ramps	PM	Α	0.515
10	Pinela Vallay Bood at I 90 agethaund rampa	AM	В	0.687
10 F	Pinole Valley Road at I-80 eastbound ramps	PM	С	0.709
4.4	Dinale Valley Deed at Fetates Avenue	AM	Α	0.482
11	Pinole Valley Road at Estates Avenue	PM	Α	0.452
10		AM	Α	0.303
12	Pinole Valley Road at Ramona Street	PM	Α	0.241

#	Intersection	Time Period	LOS	V/C Ratio
13	American Wassesh Marris Deliver	AM	Α	0.512
13	Appian Way at Mann Drive	PM	Α	0.498
14	Appier Wey et Tere Hills Drive Conven Prive	AM	В	0.673
14	Appian Way at Tara Hills Drive-Canyon Drive	PM	Α	0.555
15	Appier Wey et I 90 weeth and rempe	AM	В	0.667
15	oian Way at I-80 westbound ramps	PM	А	0.598
10	Appian Way at I-80 eastbound ramps	AM	А	0.394
16		PM	В	0.696
4.7	Appian Way at Fitzgerald Drive-Sara Drive	AM	А	0.500
17		PM	А	0.541
10	Oak Ridge Lane at San Pablo Avenue	AM	А	0.414
18		PM	А	0.351
10	Fernandez Avenue at San Pablo Avenue	AM	А	0.412
19		PM	А	0.336
00		AM	А	0.437
20	John Street at San Pablo Avenue	PM	А	0.376

Source: Dowling Associates, Inc. 2009

### **ROADWAY IMPROVEMENTS**

The circulation system roadways typology for San Pablo Avenue, Pinole Valley Road and Appian Way is described below. **Figure 5.1** shows the overall circulation system and **Figures 5.2 through 5.7** show the proposed roadway designs for San Pablo Avenue, Pinole Valley Road and Appian Way. The enlarged sections that follow illustrate the details of each cross section.

### **Major and Minor Arterials**

Major and minor arterial streets within the Specific Plan serve as the backbone for transportation and connect users to major destinations within the Specific Plan Area.

### Major Arterial

Major Arterials are intended to serve medium to high speeds of traffic. Major arterials are divided thoroughfares in urban and suburban environments designed to carry primarily medium to high speed traffic (30 – 45 mph), long distance traffic and serve large tracts of separated land uses. Major Arterials may be high ridership transit corridors. Major Arterials are primary goods movement

and emergency response routes. Major Arterials include San Pablo Avenue, Fitzgerald Drive, Tara Hills Drive, Appian Way, and Pinole Valley Road.

### Minor Arterial

Minor Arterials are intended be walkable medium speed thoroughfares (35 mph or less) in urban and suburban environments designed to carry both through traffic and local traffic, pedestrians and bicyclists. Tennent Avenue between San Pablo Avenue and Pinole Valley Road is considered a Minor Arterial.

- San Pablo Avenue is a 4-lane, mostly divided arterial running east-west along the northern
  portion of Pinole and providing connections to the unincorporated community of Tara Hills to
  the south and City of Hercules to the north. San Pablo Avenue generally runs north-south from
  the City of Oakland to the unincorporated community of Crockett. On-street parking is
  generally allowed along the segment in Pinole.
- <u>Fitzgerald Drive</u> is a 4-lane, east-west, divided arterial connecting Richmond Parkway and Appian Way on the southern side of I-80. On-street parking is prohibited.
- <u>Tara Hills Drive</u> is a circuitous arterial connecting Appian Way to unincorporated communities
  on both sides of San Pablo Avenue. It's a 4-lane roadway from Appian Way to Flannery Road,
  where it narrows to 2-lanes to its terminus in the unincorporated community of BayviewMontalvin. On-street parking is generally allowed on the 4-lane section and prohibited on the
  2-lane section.
- Appian Way is a 4-lane, north-south arterial beginning at San Pablo Avenue and running through the unincorporated community of El Sobrante. On-street parking is allowed on some segments of the roadway.
- <u>Pinole Valley Road</u> is a 4-lane, north-south arterial from Henry Avenue through Pinole that becomes Alhambra Valley Road at the city limits. From San Pablo Avenue to Henry Avenue, Pinole Valley Road is a 2-lane collector. On-street parking is allowed on some segments of the roadway.
- <u>Tennent Avenue</u> is a 2-lane, north-south arterial from its southern terminus at Pinole Valley Road to San Pablo Avenue. From San Pablo Avenue to its northern terminus at Pinole Bayfront Park, Tennent Avenue serves as a collector. On-street parking is generally allowed.

Arterials consists of either two or four travel lanes, bike lanes in some locations, landscaped center median islands where appropriate, and sidewalks separated by a parkway strip or adjacent to the back of curb, consistent with the character of the street and adjacent development. Arterials may also include on-street parking at the discretion of the Public Works Director.

### Improvements along San Pablo Avenue

San Pablo Avenue improvements involving the reconfiguration of the San Pablo Avenue right-of-way within the Old Town area, including reducing the number of through lanes, is conceptual and will require closer review and further public input prior to installing design improvements. Future steps in the implementation of conceptual changes include obtaining funding to study various design alternatives in detail, inclusion in the City's Capital Improvement Program, preparation of final design and construction documents, developing a phasing program for right-of-way improvements, and obtaining funding for construction of improvements.

The rationale in the transformation of San Pablo Avenue from four through lanes to two through lanes and a third turn lane is to return this Old Town section of San Pablo Avenue into a robust, vital and economically sound environment. Currently this section of San Pablo Avenue features four traffic lanes. These four-lane roadways significantly discourage mobility and access of pedestrians, bicyclists, and transit users who are discouraged by the lengthily crossing distances of these wide street and the accompanying higher traffic speeds.

As a two lane roadway, San Pablo Avenue would aid in resolving a number of conflicts now found in this Old Town roadway section. Currently traffic speeds through Old Town can average anywhere from 35 – 45 mph creating a deterrent for non-motorized mobility. In addition these high traffic speeds also limit the visibility of Old Town to passing motorist and detract from a sound and vibrant Old Town environment. This conversion will seek to slow traffic while keeping traffic flow adequate. By keeping the full number of lanes at intersection (buy using turn pockets), four-lane to two-lane conversions often keep the capacity of the original four lane roadway.

### Collectors

Collectors consist of either two or four travel lanes, in some cases bike lanes, and in some cases landscaped center median islands, and sidewalks in some cases buffered by on-street parking. Collectors are generally shorter in length than arterials, providing access to abutting land. Collectors serve as primary pedestrian and bicycle routes and may serve local transit routes.

Collectors do not exceed 4 lanes and access to land is a primary function. Good movement is typically limited to local routes and deliveries.

The following roadways with the Specific Plan planning area are identified as Collectors:

- Allview Avenue
- Canyon Drive
- Del Monte Drive
- Estates Avenue
- Galbreth Road
- Henry Avenue
- Manor Road
- Marlesta Road

- Pinole Shores Drive
- Pinole Valley Road (between Tennent and San Pablo Avenue)
- Sarah Drive
- Shamrock Drive
- Shea Drive
- Simas Avenue
- Sunnyview Drive

Collectors consist of either two or four travel lanes, bike lanes, landscaped center median islands, and sidewalks buffered from travel lanes by on-street parking.

### **Local Streets**

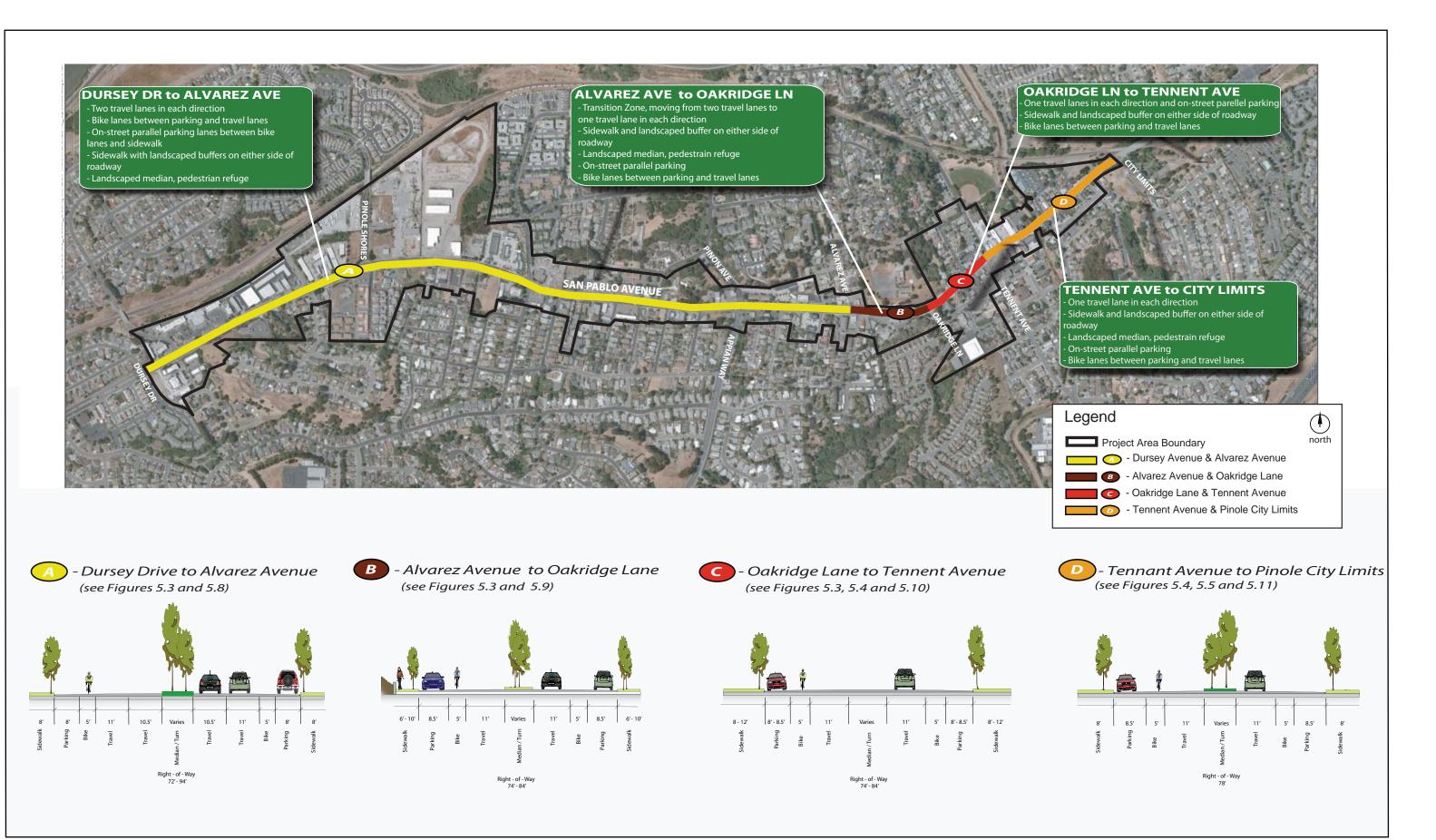
All other roadways in Pinole are classified as Local Streets. Local streets are intended to be low to medium speed thoroughfares primarily serving abutting property. Local streets are intended to connect residential neighborhoods with each other, connect neighborhood with commercial and other districts, and to connect local streets to collector and arterials. All local streets should be constructed in combination with future revitalization efforts.

Local streets will consist of both two or four travel lanes and on-street parking. Adjacent land uses will dictate the type of sidewalk, whether attached or detached.

### **Emergency Service Considerations**

In areas in Pinole where Topography, Geography or Right-of-Way limitations adversely affect public safety response times, medians maybe modified to ensure proper methods to reduce emergency response times. These methods include but are not limited to, utilizing/ implementing

rolled curbs on medians, utilizing turf block on medians and sidewalks where City Staff, including the Pinole Fire Department, deem appropriate. The recommended right - of - way improvements contained within this chapter may be modified to incorporate emergency services as recommended by the Pinole Fire Department. Traffic safety pre-emption devices and/or signage include OptiCom Systems, "Keep Clear" signage or other methods deemed appropriate by City Staff including the Pinole Fire Department.

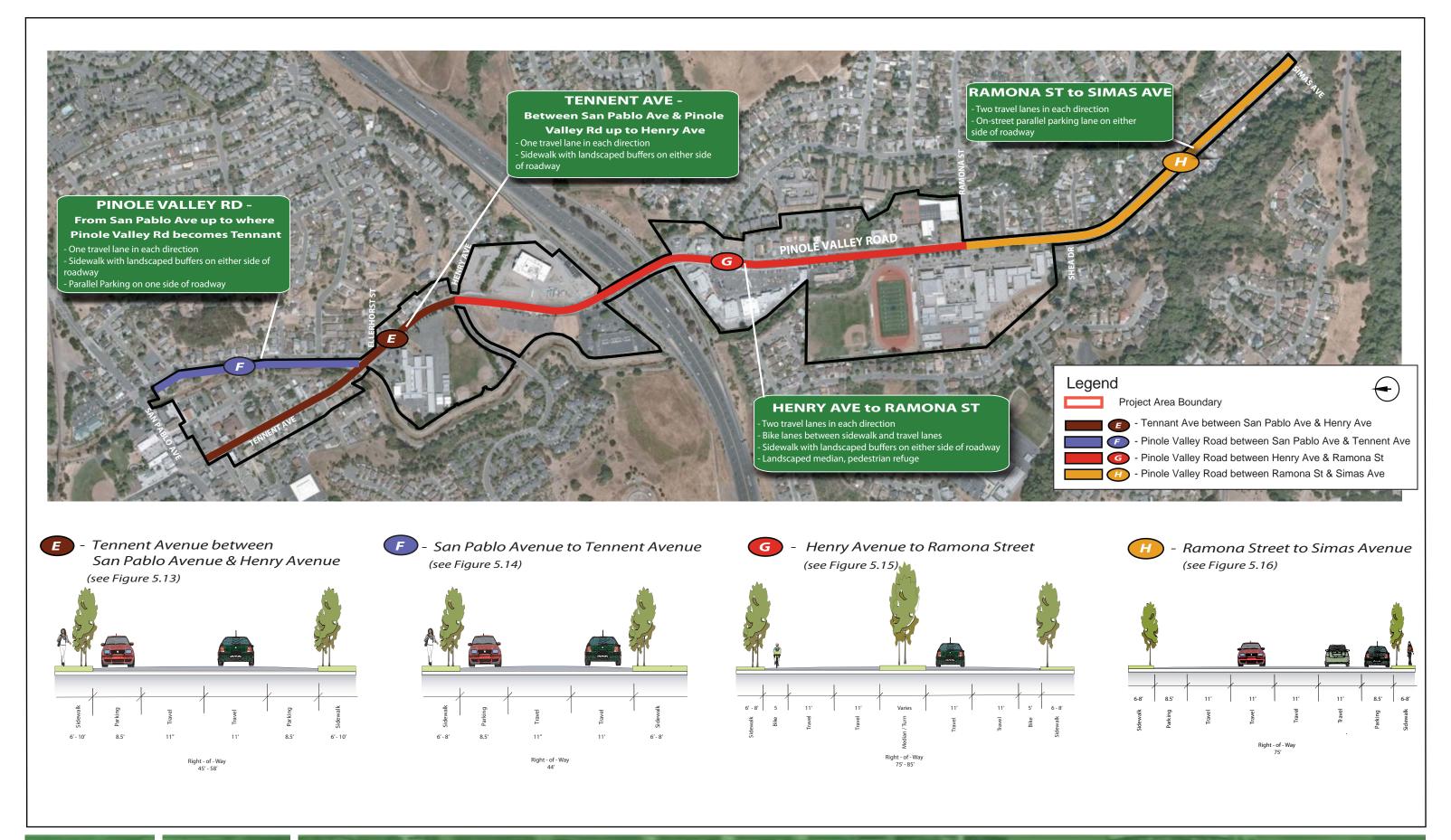


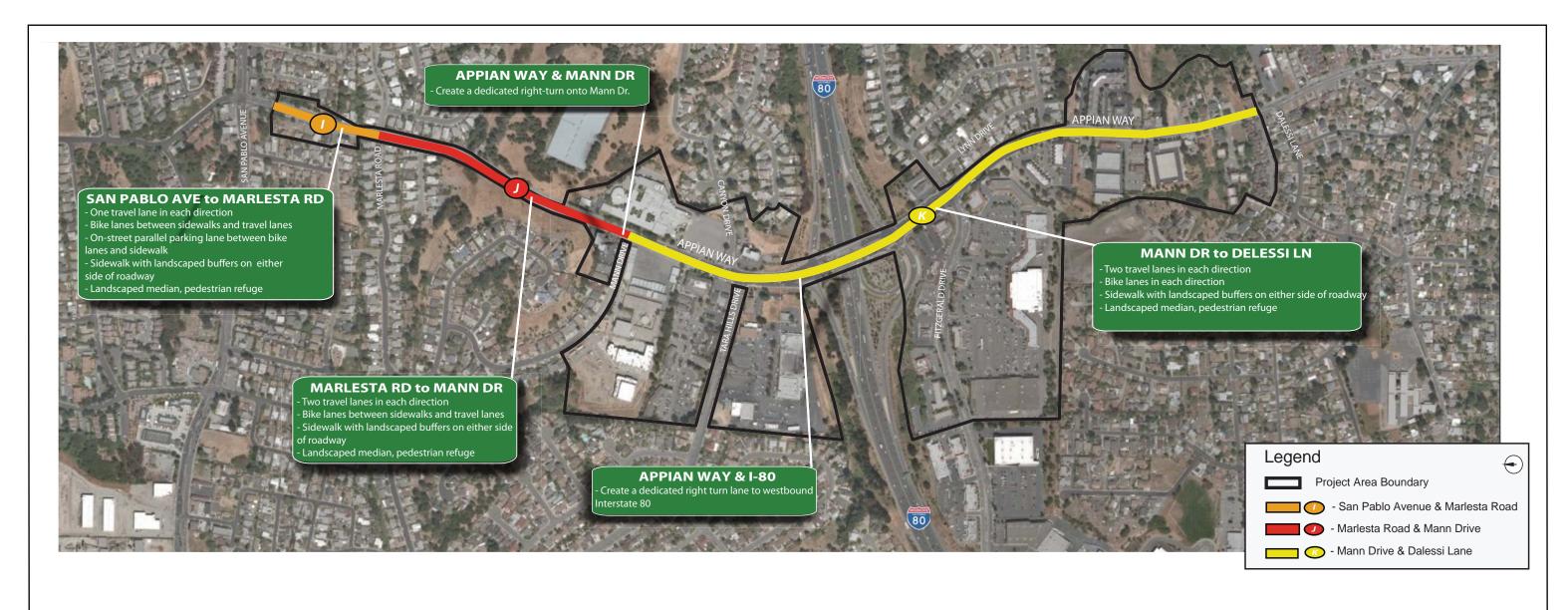






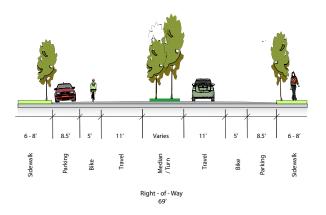






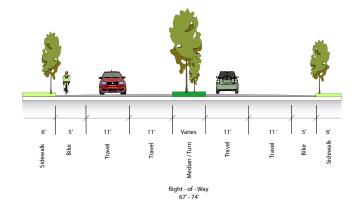


- San Pablo Avenue to Marlesta Road (see Figure 5.17)





- Marlesta Road to Mann Drive (see Figure 5.18)





- Mann Drive to Dalessi Lane (see Figure 5.19)

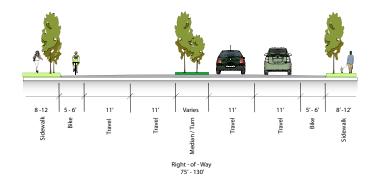






Figure 5.8
San Pablo Avenue Concept – between Dursey Drive and Alvarez Avenue

## **Proposed Street Statistics**

72'-94'

Traffic Flow	Two Ways
Lane Width	10.5' (inner travel lanes)
	11' (outer travel lanes)
Number of Lanes	2 Each Direction, with additional turn lanes
Median/ Turn	Yes
Median Width	Varies
Parking	Yes
Parking Width	8'
Sidewalk Width	8'
Bike Lane Width	5'

This section of San Pablo Road is one of the most traveled sections of roadway in Pinole. The improvements are designed to allow safe and efficient passage of all forms of transportation. A large median is meant to beautify the corridor and calm traffic. Bike lanes and on-street parking are permitted. Sidewalks are to be adequately landscaped in order to help buffer pedestrians from vehicular traffic.

6' - 10' 5′ 6' - 10' 8.5 11' Varies 11′ 5' 8.5' Sidewalk Travel Travel /Turn Sidewalk Parking Parking Bike Bike 8' - 8.5' 5′ 8 - 12' 8'-8.5' 8'-12' 11' Varies 11' Sidewalk Parking Sidewalk Travel Travel Parking Bike Right - of - Way 74' - 84'

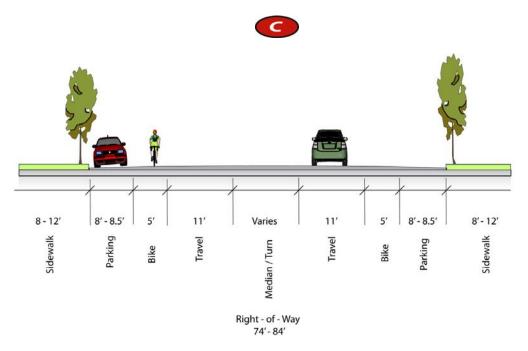
Figure 5.9
San Pablo Avenue Concept – between Alvarez Avenue and Oakridge Lane

## **Proposed Street Statistics**

Traffic Flow	Two Ways
Lane Width	11'
Number of Lanes	1 Each Direction
Median/ Turn	Yes
Median Width	Varies
Parking	Yes
Parking Width	8' - 8.5'
Sidewalk Width	6' – 10' & 8' – 12'
Bike Lane Width	5'

This section of San Pablo Avenue is a transition point from Old Town to a more freely flowing San Pablo Avenue. On-Street Parking is permitted. Sidewalks are to be adequately landscaped in order to help buffer pedestrians from vehicular traffic.

Figure 5.10
San Pablo Avenue Concept – between Oakridge Lane and Tennent Avenue

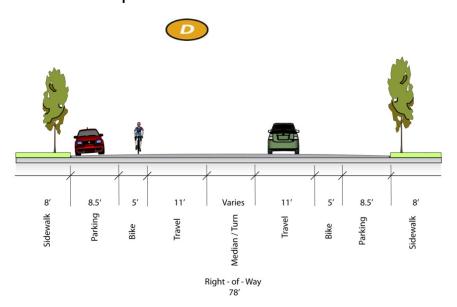


## **Proposed Street Statistics**

Traffic Flow	Two Ways
Lane Width	11'
Number of Lanes	1 Each Direction
Median/ Turn	Yes
Median Width	Varies
Parking	Yes
Parking Width	8' - 8.5'
Sidewalk Width	8 – 12"
Bike Lane Width	5'

The character of this section of San Pablo Avenue is meant to preserve the historic character of Old Town and provide on-street parking for business patrons. This segment will include "choke points" to slow traffic and accommodate parallel parking that is permitted on both side of the roadway in this section of San Pablo Avenue.

Figure 5.11
San Pablo Avenue Concept – between Tennent Avenue and John Street

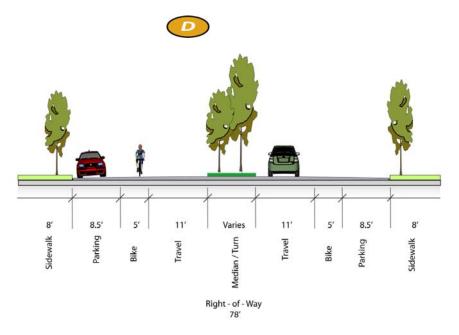


## **Proposed Street Statistics**

Traffic Flow	Two Ways
Lane Width	11'
Number of Lanes	1 Each Direction
Median/ Turn	Yes
Median Width	Varies
Parking	Yes
Parking Width	8.5'
Sidewalk Width	8'
Bike Lane Width	5'

The character of this section of San Pablo Avenue is meant to preserve the historic character of Old Town. Parallel parking is permitted on both sides of the roadway in this section of San Pablo Avenue.

Figure 5.12
San Pablo Avenue Concept – between John Street and Pinole City Limits



## **Proposed Street Statistics**

Traffic Flow	Two Ways
Lane Width	11'
Number of Lanes	1 Each Direction
Median/ Turn	Yes
Median Width	Varies
Parking	Yes
Parking Width	8.5'
Sidewalk Width	8'
Bike Lane Width	5'

The character of this section of San Pablo Avenue includes a landscaped median to create an entry statement for visitors to Old Town. Parallel parking that is permitted on both side of the roadway in this section of San Pablo Avenue.

| Sight - of - Way 45' - 58'

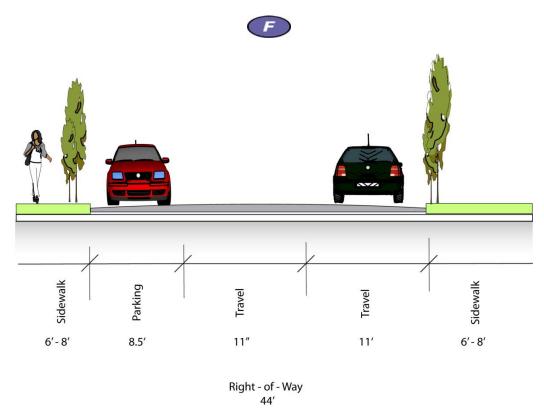
Figure 5.13
Tennent Avenue Concept – between San Pablo Avenue and Henry Avenue

## **Proposed Street Statistics**

Traffic Flow	Two Ways
Lane Width	11'
Number of Lanes	1 Each Direction
Median	No
Median Width	NA
Parking	Yes
Parking Width	8.5'
Sidewalk Width	6' – 10'
Bike Lane Width	NA

This section of Tennent Avenue is designed to preserve the historic character of Old Town. Here the street widths narrow to slow traffic and accommodate the narrow right-of-way. Parallel parking is permitted on both sides of the roadway where feasible in this section of Tennent Avenue. Where Tennent Avenue narrows, parallel parking shall continue on the west side of the roadway.

Figure 5.14
Pinole Valley Road Concept – between San Pablo Avenue and Tennent Avenue



**Proposed Street Statistics** 

Traffic Flow	Two Ways
Lane Width	11'
Number of Lanes	1 Each Direction
Median	No
Median Width	NA
Parking	Yes, on east side of roadway
Parking Width	8.5'
Sidewalk Width	6' – 8'
Bike Lane Width	NA

This section of Pinole Valley Road runs through the heart of Old Town Pinole and will maintain the smaller quaint streets that are currently present here. The continued on-street parking here will separate the pedestrian environment from vehicular traffic.

G 11' 11′ Varies 11' 11' 5' 6 - 8' Travel Travel Sidewalk Travel Sidewalk Median / Turn Travel Bike Right - of - Way 75' - 85'

Figure 5.15
Pinole Valley Road Concept – between Henry Avenue and Ramona Street

## **Proposed Street Statistics**

Traffic Flow	Two Ways
Lane Width	11'
Number of Lanes	2 Each Direction
Median / Turn	Yes
Median Width	Varies
Parking	No
Parking Width	NA
Sidewalk Width	6 – 8"
Bike Lane Width	5'

This section of Pinole Valley Road exemplifies a more institutional character of Pinole. Street parking is not permitted in this section of Pinole Valley Road and turn lanes become more prevalent. Bicycle lanes are located in both directions and the sidewalks are to be landscaped in order to buffer the pedestrian from vehicular traffic.

Bight - of - Way 75'

Figure 5.16
Pinole Valley Road Concept – between Ramona Street and Simas Avenue

## **Proposed Street Statistics**

Traffic Flow	Two Ways
Lane Width	11'
Number of Lanes	2 Each Direction
Median	No
Median Width	NA
Parking	Yes
Parking Width	8.5'
Sidewalk Width	6'
Bike Lane Width	NA

This section of Pinole Valley Road exemplifies a residential character and provides a transition between the rural lands at the southern edge of the City and the more developed community core. Bicycles will begin sharing the road with automobiles, and parallel parking is allowed on both sides of the roadway for this segment of Pinole Valley Road. The sidewalks here are adequately landscaped in order to make pedestrians' feel buffered from vehicular traffic.

Figure 5.17
Appian Way Concept – between San Pablo Avenue and Marlesta Road

## **Proposed Street Statistics**

Traffic Flow	Two Ways
Lane Width	11'
Number of Lanes	1 Each Direction with additional turn lanes
Median/ Turn	Yes
Median Width	Varies
Parking	Yes
Parking Width	8.5'
Sidewalk Width	6' – 8'
Bike Lane Width	5"

This section of Appian Way narrows to intersect with San Pablo Avenue. Bicycle and pedestrian access is accommodated on both sides of the roadway, as is on-street parking.

Sidewalk Bilber of Maximum Median / Travel 11, Assistance 11, Assi

Figure 5.18
Appian Way Concept – between Marlesta Road and Mann Drive

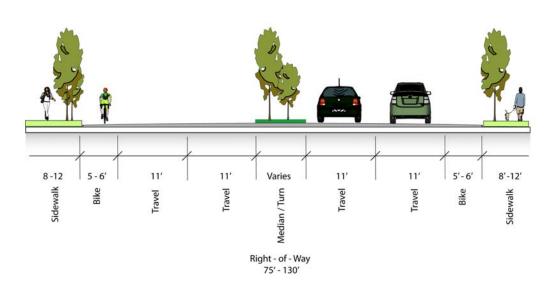
## **Proposed Street Statistics**

Traffic Flow	Two Ways
Lane Width	11'
Number of Lanes	2 Each Direction with additional turn lanes
Median/ Turn	Yes
Median Width	Varies
Parking	No
Parking Width	NA
Sidewalk Width	6'
Bike Lane Width	5'

This section of Appian Way is a transition point that seeks to slow higher speed traffic as it approaches San Pablo Avenue. Sidewalks narrow to accommodate the narrowing Right-of-Way. No on-street parking is permitted.

Figure 5.19
Appian Way Concept – between Mann Drive and Dalessi Lane Intersection





## **Proposed Street Statistics**

Traffic Flow	Two Ways
Lane Width	11'
Number of Lanes	2 Each Direction
Median/ Turn	Yes
Median Width	Varies
Parking	No
Parking Width	NA
Sidewalk Width	8' -12'
Bike Lane	5' - 6'

As a main thorough fare in Pinole, improvements for this section of Appian Way strive to maintain vehicular traffic movement while beautifying this section with a tree lined median. Bike Lanes are proposed to allow additional modes of transportation on both sides of Appian Way. The sidewalk area is to be landscaped in order to help buffer pedestrians from vehicular movement.

## 5.4 PARKING

Generally, parking demands within the Specific Plan Areas will be accommodated primarily by offstreet parking and to a lesser degree on-street. On-street parking also functions to buffer pedestrian zones and acts as a natural traffic-calming feature.

The Old Town sub-district of the Specific Plan differs from all other Specific Plan Areas. Currently Old Town Parking standards are governed by the Old Town Parking Overlay District. The overlay allows for shared parking between public and private projects, making it more feasible to redevelop existing buildings into a mix of uses. Old Town Overlay District standards will be sufficient for some time into the future in creating adequate parking for patrons to reach their destination within 1-2 blocks.

It is envisioned however, that Old Town will become a shopping and entertainment destination with a lively street and a mix of land uses. The Specific Plan is suggesting an intensification of development in the Old Town area, creating an increased demand for off-street parking. There are opportunities within the Old Town Sub-District to provide a parking structure and it is suggested in the implementation section of this plan that the City studies the possibility of building a public parking structure within Old Town Pinole to accommodate more intense development and business activity in this area.

## 5.5 PUBLIC TRANSPORTATION

Transit systems provide a motorized alternative to private vehicles. They serve citizens who cannot drive or chose not to drive, including senior citizens, residents with limited mobility, people under the age of 16, residents with no driver's licenses or suspended driving licenses, people who cannot afford a vehicle, and citizens opting to live a less car-dependent lifestyle. Currently transit service in Pinole is limited to bus service with a limited number of lines and infrequent service outside of peak commute periods. Citizens of Pinole are forced to rely too heavily on the private automobile, thereby limiting circulation options for youths and others who don't drive, and contributing to traffic congestion, and greenhouse gas emissions.

#### **BUS SERVICE**

Pinole is served by two bus transit agencies, the Western Contra Costa Transit Authority (WestCAT) and the Alameda Contra Costa Transit Authority (AC Transit).

WestCAT operates seven fixed-route services that serve Pinole. Additionally, WestCAT operates an express bus service, the JPX, to El Cerrito BART and an express bus service, the Lynx, to San Francisco from the Hercules Transit Center. WestCAT also operates a dial-a-ride paratransit service for seniors and the disabled. All fixed route busses are equipped with front-loading racks that can hold up to two bicycles. WestCAT's portable route map contains information on bus stop locations, routes, and fixed route bus schedules.

AC Transit operates 2 fixed-route services in southern Pinole, Route 70 (Appian) and Route 376 (North Richmond Night). AC Transit also operates dial-a-ride paratransit services for seniors and the disabled in southern Pinole. Busses are equipped with front-loading racks that can hold up to two bicycles.

#### PASSENGER RAIL SERVICE

The San Francisco Bay Area Rapid Transit District (BART) provides heavy-rail, regional transit service to Alameda, San Francisco, Contra Costa, and San Mateo counties. There is currently no BART service in Pinole. However, WestCAT operates the J bus route, which connects to the BART station at El Cerrito del Norte. BART's direct service from this station includes the Richmond/Fremont line, with trains every 15 minutes during the weekday until 7:00 PM and every 20 minutes during evening weekday times and the weekend. This train line runs until midnight everyday, with weekday, Saturday, and Sunday service beginning at 4:15 AM, 6:00 AM, and 8:00 AM, respectively. The Richmond/San Francisco line also runs with trains every 15 minutes during the weekday until 7:00 PM and every 20 minutes on Saturday until 6:00 PM. Connections to the Fremont/San Francisco line, Pittsburgh/Daly City line, and the Dublin-Pleasanton/Millbrae line can be made at various points throughout the system.

Amtrak operates passenger rail service for three routes that traverse western Contra Costa County. While there is no station in Pinole, the closest station is an unmanned Amtrak station at the Richmond BART station and there are plans to build a ferry and Amtrak station in neighboring Hercules. Two of Amtrak's routes that stop in Richmond are intrastate services, the Capitol Corridor serving Sacramento/ San Jose, and the San Joaquin serving Oakland/ Bakersfield. The other route is the Zephyr, an interstate service serving Emeryville/ Chicago.

#### **FERRY SERVICE**

There is currently no ferry service offered in western Contra Costa County. Ferry service to San Francisco is provided by Baylink from Vallejo and by East Bay Ferry from Oakland. Weekday service is provided from about 6:00 AM into the evening hours at 30- to 100-minute intervals and weekend service is provided from 10:00 AM to 6:30 PM at 60- to 120-minute intervals. There are plans to build a ferry station in Richmond and a multi-modal ferry and Amtrak station in neighboring Hercules.

#### TRANSIT IMPROVEMENTS

The City of Pinole should continue to coordinate transit operations with Regional Transit Agencies and the West Contra Costa County Transit Authority to enhance the existing transit service. The following recommendations would enhance the existing transit service within the Specific Plan Area:

- Develop neighborhood bus service connectivity within the Specific Plan Areas
- Identify new transit stops near pulse point destinations
- Develop services that connect residents to local employment centers
- Upgrade amenities at transit stops
- Sheltered structures
- Wayfinding signage
- Real-time boarding schedules
- Pedestrian and bicycle accessible

#### 5.6 BICYCLE FACILITIES

Bicycles are an important component to any mobility plan. Bicycle facilities create recreational opportunities that contribute to the quality of life in Pinole, and provide a viable, cost effective, and environmentally sensitive form of transportation. In the event that there is not physical room to provide bicycle lanes in segments of the corridor, other improvements and facilities can still be installed to support bicycle use.

The concept of 'Shared Lanes' has increased in popularity recently with the new approaches to traffic controls. 'Sharrows,' or 'Shared Lane Bicycle Stencils' have been developed and used in California to allow 13 to 15 foot outside lanes to function as both bikeways and vehicle travel lanes. These markings can reduce vehicle speeds along the roadway and reinforce proper



lane positioning for both vehicles and bicycles, thus increasing overall safety.

Off-street facilities for bicycles (bicycle parking) are also integral to cyclists for accessibility and encouragement. Convenient bicycle parking shall be provided at all major destinations, in Old Town, and at public parks. Bicycle racks shall be placed along the street where appropriate and provided in parking lots at 5 percent of the number of vehicle stalls. Racks in off-street locations shall be visible and well lit to discourage theft or vandalism and be placed to be convenient to the cyclist.

New streets within the Specific Plan Areas should feature Class II bike lanes on roadways that provide direct connection to other major destinations. The bicycling experience can be enhanced through the use of the following treatments:

- Develop a bicycle wayfinding system provides direction to bicyclists in finding connecting Class I or II facilities and key destinations of interest.
- Advance bike loop detectors can actuate traffic signals in situations where a side street has a
  rest on red phase.
- Colored bicycle lanes increase the visibility of cyclists in areas of high potential conflict between motorists and cyclists.
- Bicycle shared use arrow reducing wrong-way riding, riding on the sidewalk, and places the bicyclist outside the door zone.

## 5.7 PEDESTRIAN CIRCULATION

Emphasis should be placed on creating pedestrians facilities that are not only safe and functional, but that create a sense of place within the Specific Plan Areas. Pedestrian amenities should be compatible with adjacent land uses and provide opportunities for public interaction. Pedestrian facilities should be buffered from the adjacent travel lanes by use of parkway strips and street trees.

The following pedestrian treatments are recommended to enhance pedestrian facilities within the Specific Plan Areas.

#### INTERSECTION TREATMENTS

The following intersection treatments are recommended where intersection crossing distances are greater than 60 feet:

- Center median pedestrian refuge Create a two stage crossing for pedestrians by only having
  to cross one-half of the roadway at a time. Center median pedestrian refuges should be a
  minimum of four feet wide but preferably six to eight feet wide.
- Reduce intersection corner radii Minimize the intersection corner radii to 25 feet where heavy
  vehicle turning movements are infrequent. For a larger corner radius designs necessary to
  accommodate high heavy vehicle turning movements, right-turn pedestrian refuge islands
  should be considered.
- Eliminate "free" right turn lanes at all intersections.
- Right-turn pedestrian refuge island Provide a multi-stage crossing for pedestrians and create
  a shorter walking distance where large corner radii are necessary. The schematic to the right
  illustrates a right-turn pedestrian refuge island.

#### **MID-BLOCK CROSSINGS**

Mid-block pedestrian crossings should be considered where block lengths are greater than 800 feet. Midblock crosswalks should be located where sufficient demand exists to justify the installation of a crosswalk, and must be designed to ensure safe access.

The decision to install mid-block crossings will be made by the Public Works Director based on a location specific engineering study. Candidate crossing locations should:

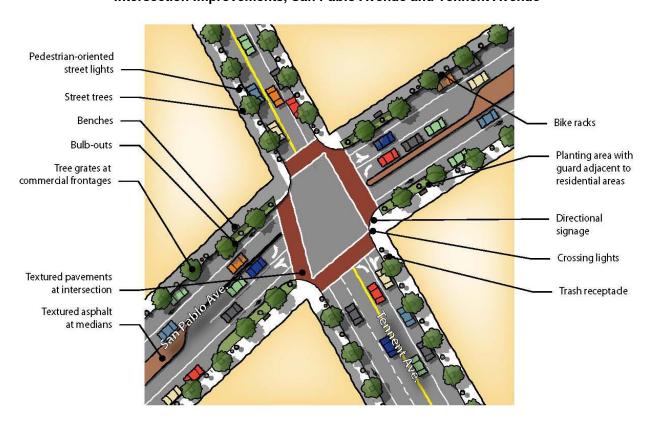
- meet applicable sight distance requirements
- be located an appropriate distance from the nearest crossing
- channelize potential jaywalkers
- incorporate appropriate safety measures in locations that experience high traffic volumes or speeds.

The following treatments are recommended to enhance the visibility of mid-block crossings:

- High visibility crosswalks Use unique striping designs to draw motorists' attention to the crossing location.
- Split pedestrian crossover Provide pedestrian refuge areas that channel pedestrians across a
  roadway in two stages by first crossing one half of the street, entering a center island at one
  end, walking towards the flow of traffic, and exiting at the other end to cross the second half of
  the street.
- Pedestrian activated signals Provide a controlled mid-block pedestrian crossing.

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Figure 5.20 Intersection Improvements, San Pablo Avenue and Tennent Avenue









Intersection Improvements
San Pablo Avenue and Tennent Avenue: Figure: 5.20

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#### TRAFFIC CALMING MEASURES

The potential configurations described below attempt to balance the need to effectively moderate vehicle speeds and improve the pedestrian environment. They also seek to maintain acceptable multi-modal circulation for bicyclists, pedestrians, vehicles, and mass transit.

These traffic calming tools include narrowing of traffic lanes, adding median refuge islands where appropriate, adding Public Realm Design Guidelines corner curb extensions or "bulbouts", adding accent paving at crosswalks, and improving pedestrian crosswalk signals. Chapter VII - Public Realm Design Guidelines of this Specific Plan offers guidelines for implementing these traffic calming tools.

## **Narrowed Travel Lanes**

Narrowing travel lanes is an effective tool to regulate vehicle speeds. Drivers have been found to travel more slowly on streets with lane widths of 10 to 11 feet versus more typical 12-foot lane widths. The effect is largely psychological. Narrower travel lanes require more attention from drivers and are often used in environments where calmer traffic speeds are desired. In addition even narrower streets are effective in downtown environments with a higher degree of potential conflicts, such as pedestrians, frequent movements to and from side streets, and vehicles making parking maneuvers.

Narrower lanes also have the benefit of reducing pedestrian crossing distances and freeing up space for other uses such as parking, bike lanes, medians, and widened sidewalks and the

pedestrian environment. Many portions of the three Specific Plan sub-areas would benefit from a decrease in lane size.

## Refuge Islands

Medians can be used to create pedestrian "refuge islands" that reduce the number of lanes a pedestrian must cross at one time. Refuge islands are extensions of the median that create a



protected crosswalk area in the middle of the street. There are many areas throughout the Specific Plan Areas that could be improved for pedestrians by in building these pedestrian friendly spaces.

## **Bulbouts**

The use of curb extensions or "bulbouts" is suggested at selected intersections Old Town Pinole and other appropriate sections of the Specific Plan Areas. Bulbouts extend the curbs to widen the

sidewalk area at crosswalk locations. This reduces the distance that pedestrian must cross. Drainage issues with bulbouts are an important concern, particularly in topographically undesirable potions of Pinole where there is the potential for excessive storm water build up. So where gutter flow cannot be accommodated around the perimeter of the bulbout, it may be necessary to incorporate features such as removable grates or parkway drains to facilitate water flow.

## **Street Trees**

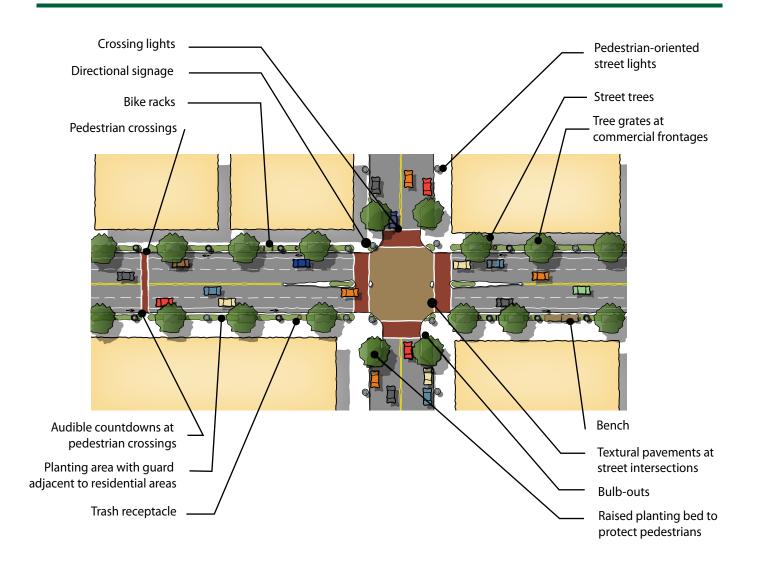
Street trees are a necessary component of great cities throughout the nation and offer an aesthetic alternative to the wide-open speedway feeling of a treeless arterial. Street trees planted at the sidewalk edge, or in medians, have a traffic calming effect as they create a visually enclosed and perceptually narrower street scene while simultaneously creating a pedestrian environment. Additionally they have the added benefit of reducing the effects of urban heat islands.

## **Pedestrian Crosswalk Signals**

Improvements to crosswalk signals such as audible pedestrian crosswalk signals, and manually-triggered crosswalks can increase safety at pedestrian crossings. In-street crossing lights give motorists a visual heightened awareness of pedestrians, which in turn, can slow traffic; audible pedestrian crosswalk signals can improve safety for the visually impaired; and manually-triggered crosswalks can improve pedestrian and vehicular traffic flow. See public realm (Chapter 6) for additional information on pedestrian crossings.



In preparing street improvement opportunities, focus was given to creating a street environment that encourages lower speeds, thereby creating a safer and more enjoyable environment for pedestrians in the Specific Plan Area. However, this is a challenging task given the high traffic volumes in some of the Specific Plan Areas. Where traffic volumes are high and streets maybe to wide to navigate safely, crosswalk signals should be limits to traffic lights.



# Streetscape Elements Palette:

## **Paving Materials**









Sidewalks - Patterned Concrete

Crosswalks - Brick/Paver on Concrete

Intersections - Brick/Paver on Concrete Medians - Level Stamped Asphalt

## Street Furniture









Bike Racks

Trash Cans

## Landscaping





Tree Grates

Median Landscaping

## Signage









Wayfinding Sign

Banner Sign

**Gateway Treatments** 





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## 6.1 INTRODUCTION

The Land Use and Development Standards Chapter contains regulatory tools for guiding development and redevelopment in the Specific Plan areas. These tools will direct land use and development practices to implement the vision for San Pablo Avenue, Pinole Valley Road and Appian Way. This chapter includes the land use plan, permitted uses, and development standards for future development and redevelopment.

For ease of use, this chapter is organized by Specific Plan area – San Pablo Avenue, Pinole Valley Road and Appian Way. To use this chapter, one should first read the general section which covers regulations and standards that are applicable to the all of the specific plan areas; then read the provisions that are specific to each of the Specific Plan areas individually.

This chapter includes the following components:

- A. <u>Goals and Policies</u> to achieve the desired vision for San Pablo Avenue, Pinole Valley Road, and Appian Way. The goals and polices are intended to promote flexibility, to introduce new land uses while maintaining existing viable uses, and to promote an intensity of development that is appropriate in Pinole
- B. <u>Land Use Districts</u> describe each of the Specific Plan zoning districts shown on the land use map. The land use districts identify primary land uses and development intensities.
- C. <u>Land Use Maps</u> identify specific land use designations for individual property throughout the corridors.
- D. <u>Land Uses Matrix</u> identifies allowed uses, and permit requirements for all of the zoning districts located in the Specific Plan Areas.
- E. <u>Development Standards</u> establish rules for development density, intensity, and building form within each of the zoning districts located in the Specific Plan Areas.

## 6.2 LAND USE GOALS AND POLICIES

This section identifies goals, policies and objectives to create new residential development, enhance Old Town, and promote community enhancing economic development. The Specific Plan focuses on the uses that have been identified as desirable, both through the community visioning process, Steering Committee process and the economic analysis.

The land use plan has specific goals for each of the nine districts that are being created, and seeks to accomplish the following objectives:

- To allow for and encourage flexibility for future development and redevelopment;
- To enrich the revitalization effort through establishing a pedestrian oriented land use pattern;
- To intensify the development footprint and create an urban core in the Old Town; and
- To reinforce the land use pattern through mixed use districts and encourage land uses that are responsive to market opportunities; and
- To promote the integration of compatible land uses in a vertical and/or horizontal manner that provides additional opportunities for commercial, office, residential, and industrial uses in the City.

To accomplish these objectives, the Specific Plan is proposing following policies:

LAND USE POLICY 1	Provide for a variety of housing types throughout the plan areas.
LAND USE POLICY 2	Promote residential units on the upper stories of structures and encourage commercial activity at the street level.
LAND USE POLICY 3	Provide affordable housing within the plan areas consistent with the City's General Plan.
LAND USE POLICY 4	Ensure development of "Opportunity Sites" with high quality mixed use or high density housing.
LAND USE POLICY 5	Support existing viable uses while encouraging a new mix of uses.
LAND USE POLICY 6	Actively promote the "revitalization" of underutilized land
LAND USE POLICY 7	Promote a variety of retail uses that are compatible with a pedestrian environment.
LAND USE POLICY 8	Encourage the development of mixed-use office buildings in proximity to existing transit stops.
LAND USE POLICY 9	Promote a variety of smaller retail and office space opportunities for small business.
LAND USE POLICY 10	Where possible, preserve and restore historic buildings

# 6.3 GENERAL LAND USE PROVISIONS APPLICABLE TO ALL SPECIFIC PLAN AREAS

#### **LAND USE DISTRICTS**

The Specific Plan Land Use Map/Zoning Maps (Figures 6.2, 6.4 and 6.6) establish the following land use zones to implement the goals, policies, and objectives of the City's General Plan. Mixed-Use zones allow for vertical and/or horizontal distribution of commercial, office, residential, and industrial uses in the City. Land use descriptions herein include development assumptions in terms of mix of uses and density and intensity of development.

Table 6.1
Land Use Districts

Symbol	Land Use	Purpose and Characteristics
MDR	Medium Density Residential	The purpose of the Medium Density Residential District is to allow development of diverse housing types with a density between 10.1 and 20.0 dwelling units per acre. This designation is intended for small lot single-family detached, single-family attached (e.g., townhomes, condominiums, brownstones), and small apartment complexes and provides a transition between the single-family residential neighborhoods of the City to the more built-up centers.
HDR	High Density Residential	The purpose of the High-Density Residential District is to allow development of housing types with a density between 20.1 and 35.0 dwelling units per acre.  This district is intended for higher density housing including townhomes, condominiums, and apartment complexes.
HDRO	High-Density Residential	The purpose of the High-Density Residential District Overlay is to identify sites where a minimum number of residential units must be developed. In the HDRO districts, a minimum of 20 percent of the site shall develop at a density of between 20.1 and 35.0 dwelling units per acre. This requirement may also be satisfied by integrating an equivalent number of dwelling units within a mixed use project.
VHDR	Very High Density Residential	The purpose of the Very High-Density Residential District is to designate property for the development of a wide range of housing types with a density between 35.1 to 50 dwelling units per acre.  This district is intended for higher density housing including townhomes, condominiums, brownstones and apartment complexes.
RMU	Residential Mixed Use	The purpose of the Residential Mixed Use District is to designate property for the development of medium- and high-density housing that may incorporate office and/or commercial services. While the predominant use of the development is residential, the City encourages the vertical and/or horizontal integration of commercial and/or office uses that are compatible with the residential development. This Designation does not preclude development that is solely residential, but rather encourages a mix of uses. At least a minimum of 51 percent of the total floor area should be residential. Consistent with the General Plan, this Designation allows between 20.1 and 35.0 dwelling units per acre. Lower residential densities may be allowed to accommodate special housing needs within this designation. This Designation promotes the integration of retail and office uses into the edges of neighborhoods with all of the commercial use at the ground floor on the street front wherever possible.

Symbol	Land Use	Purpose and Characteristics
СМU	Commercial Mixed Use	The purpose of the Commercial Mixed Use District is to designate property for vibrant commercial and mixed-use development. While predominately commercial (this category is designed to provide for the integration of retail and service commercial uses with office and/or residential uses. In multiple story buildings, retail uses are the predominant use on the ground floor. Commercial retail and service uses (including general retail, personal services, and minor auto services) are permitted by right and more intense commercial and service uses are conditionally permitted. Business and professional office uses, as well as residential uses (density between 20.1 and 30.0 units per acre), are also permitted by right when integrated vertically or horizontally with commercial uses. At least 51 percent of the total floor area should be commercial retail and service use. However, up to 100 percent of total floor area may be residential use for development with an affordable housing agreement and when that development includes community benefits as specified in the General Plan.
OIMU	Office Industrial Mixed Use	The purpose of the Office Industrial Mixed Use District is to allow a wide range of office and light industrial development. This designation is intended for office and light industrial uses with supporting retail and service uses. Offices may be developed in an office park setting, but most office and light industrial development stands alone. Commercial and other support services may be integrated vertically and/or horizontally in this district, but the predominant use of integrated developments is office and/or light industrial. Retail must be ancillary to the principal industrial activity of the property and cannot exceed 10% of total usable floor area. Public and quasipublic uses (community centers, public library, city hall, parks and other community-serving uses etc) are permitted.
OPMU	Office Professional Mixed Use	The purpose of the Office Professional Mixed Use District is to allow development of larger office buildings and business parks with supporting retail and service uses. The predominant use is office, but commercial uses may be integrated into office buildings or located horizontally in freestanding buildings. At least 51 percent of the total usable floor area should be office uses. This designation also allows residential uses to be integrated with office use where compatible at a density between 20.1 and 30.0 units per acre. All of the commercial use should be at the ground floor on the street front wherever possible. Public and quasi-public uses (community centers, public library, city hall, parks and other community-serving uses etc) are also permitted in this designation.
PQI	Public / Quasi-Public / Institutional	The purpose of the Public/ Quasi-Public/ Institutional District is to allow development of public, quasi-public and institutional uses (community centers, fire stations, schools, public library, city hall, parks and other community-serving uses etc). Development standards are deliberately flexible to provide for a variety of uses that deliver beneficial services to the public.
OS	Open Space	The purpose of the Open Space District is to define areas where urbanization is not appropriate given topographic or resource constraints within areas designated for these uses in the land use element of the General Plan.

### **ALLOWED LAND USES**

Allowed use provisions for each of the land use districts described in this section are identified in the Land Use Matrix for each individual Specific Plan area in sections 6.5, 6.12 and 6.18 respectively. Each permitted, or conditionally permitted land use must conform to the density limitations and development standards established by the governing sub-area and district.

#### SIMILAR USES

Recognizing that every conceivable use cannot be identified in this Specific Plan and, anticipating that new uses will evolve over time, this section gives the Community Development Director or his/her designee authority to approve uses when their operation, impacts, and scale are similar to those uses listed in this Specific Plan. In making a determination that a use is "similar," the Director shall make all of the following findings:

- (a) The characteristics of, and activities associated with, the proposed use are equivalent to one or more of the listed uses and will not involve a higher level of activity or population density than the uses listed in the sub-district.
- (b) The proposed use is consistent with the purposes of the applicable land use designation.
- (c) The proposed use is consistent with the General Plan and this Specific Plan.

Determinations will be made in writing and will contain the facts that support the determination. The Department will maintain a record of all such determinations for public review. All determinations will be provided to the Planning Commission, City Council, City Manager, City Attorney, and City Clerk as an information item within 30 days of the Director's determination.

#### **APPLICABILITY/ ADMINISTRATION**

#### 1) Plan Administration

This section, in combination with the Chapter 10-Implementation, describes how the Specific Plan will be administered to address such issues as the development review process, vertical and horizontal integration of mixed uses, and nonconforming structures and uses. The Community Development Director or his/her designee has authority to administer the Specific Plan, and may refer projects and policy determinations to the Planning Commission or review and approval. In the event that the Community Development Director position is vacant, the City Manager or his/her designee has the authority to administer the Specific Plan.

#### 2) Design Review

All development projects within the Plan Area are required to go through a Design Review process. Within the Old Town Sub-Area, projects will be evaluated for conformance with the Old Town Design Guidelines. The Old Town Design Guidelines shall be retained and amended by the City to be consistent with the Specific Plan to assist with implementation of the Old Town Sub-Areas of the San

Pablo Avenue and Pinole Valley Road corridors and to help protect the existing positive characteristics of the Old Town Area. Some projects may qualify for Expedited Design Review as described below.

### **Expedited Design Review**

The Expedited Design Review process is administrative because projects that qualify for Expedited Design Review must meet all the objectives and standards contained within this Specific Plan as determined by the Community Development Director or his/her designee.

To qualify for Expedited Design Review, a project must:

- Implement the Area-Wide Land Use Development Standards and Design Guidelines as well as the corridor specific Development Standards and Design Guidelines for the applicable corridor, which can be found in Chapter 6 - Land Use Standards and Chapter 7 - Private Realm Design Guidelines;
- Implement applicable improvements specified in Chapter 4 Circulation and Chapter 8 –
   Public Realm Standards and Design Guidelines; and
- Be identified by the City as a Priority Project or demonstrate a high level of sustainability in development or redevelopment.

### 3) Nonconforming Uses and Structures

### Purpose and Applicability

The Specific Plan land use designations are not intended to force conversion of existing development and uses along the corridor, but to allow redevelopment to occur over time as land values increase and market conditions evolve. Thus, existing uses and structures in these areas that legally exist prior to the adoption of this document may continue in perpetuity with some limitations on replacement and expansion as outlined in the Pinole Municipal Code.

#### **DEVELOPMENT POTENTIAL PROJECTIONS**

The Specific Plan land use designations modify development potential within the San Pablo Avenue, Pinole Valley Road, and Appian Way corridors. The actual development that will occur under the Specific Plan is difficult to predict given the economic, physical, and regulatory variables that will govern land use. Never the less, in-fill development and redevelopment in the approximately 300 acre Specific Plan area will result in the potential for additional development as the result of the following land use changes:

- Replace single use commercial zoning with various mixed use zones;
- Eliminate commercial Floor Area Ratio (FAR) as a development constraint;
- Increase opportunities for residential development; and

Appendix A describes the assumptions were used to respond to the following conditions:

- Development Constraints: Regulatory, physical, and market constraints will have an effect on build-out conditions:
  - Development Intensity: Residential density and commercial floor area limitations establish
    the maximum potential development within the Specific Plan area;
  - Topography: Slope conditions will reduce the development potential on some properties;
  - Parking: Parking requirements have the potential to limit development more than density or development standards; and
  - Market Demand: Limited demand for certain development types, particularly retail, will further limit development potential.
- <u>Large Lot</u>: Development of properties that are over an acre in size will typically result in more
  efficient parking and circulation layouts.
- Small Lot: Smaller scale properties typically require a higher percentage of the property to be utilized for parking and circulation.
- Redevelopment: Because most of the properties within the Specific Plan boundaries are already developed, redevelopment of these sites will likely occur slowly and in an incremental manner.

The land use assumptions, account for these conditions and establish a realistic development scenario that could be compared with potential development under the 1995 General Plan.

# 6.4 GENERAL DEVELOPMENT STANDARDS APPLICABLE TO ALL SPECIFIC PLAN AREAS

This section contains development standards for private development within the Specific Plan areas. These standards are designed in concurrence with the public and streetscape improvements recommended for the Specific Plan areas and described in Chapter 8 (Public Realm Standards and Design Guidelines). These standards are intended to simplify, streamline, and customize the standards and requirements described in the Pinole Zoning Ordinance (PMC Title 17). They reinforce the desired urban character, enhance the overall appearance and serve as an incentive for private reinvestment in the three areas. In addition to the general development standards applicable to all three Specific Plan areas, section 6.5, 6.6 and 6.7 includes those development standards that are unique to individual Specific Plan areas.

#### **APPLICABILITY**

Development standards are mandatory requirements that must be satisfied for all new projects and qualifying building renovations as outlined below:

- <u>Exemptions</u>: Exemptions include minor modifications, such as painting, maintenance or repair, re-roof, and modifications that increase square footage by less than 200 square feet (within any 2-year period).
- Major Modifications: Major modifications are defined in this Specific Plan as any alterations of more than 10% of an existing building façade or more than 10% increase in existing building square footage.
- Renovations: For expansion or other renovation applications, the Community Development
  Director or his/her designee retains authority to determine major and minor modifications,
  exemptions, and give relief from Specific Plan standards based on the determination that they
  are safe and have no significant adverse impacts upon surrounding property.

#### **GENERAL HEIGHT REQUIREMENTS AND EXCEPTIONS**

Height requirements are provided for each Specific Plan area are included in sections 6.5, 6.6 and 6.7, respectively. The Community Development Director or his/her designee may review and approve architectural features such as tower elements, elevator service shafts, and roof access stairwells that extend above the height limit by less than 10%. The Planning Commission shall review architectural features that extend above the height limit by 10% or more.

Telecommunications antennas and service structures located on rooftops may also exceed the maximum building height but shall be hidden to the maximum extent possible using appropriate screening and concealing technologies and shall require a use permit in each Specific Plan Area. On the recommendation of the Planning Commission, the City Council may approve buildings that exceed the maximum height adopted in any district in conjunction with a variance application request.

#### **GENERAL PARKING STANDARDS**

Parking requirements are established in the Pinole Municipal Code (PMC Title 17). To attract development and enhance the pedestrian environment within the Specific Plan Area, parking requirements shall be reduced when one or more of the following conditions exists:

- 1) Shared parking arrangements can demonstrate that peak hour parking demand will not coincide to generate demand that is greater than the parking provided;
- 2) The property is adequately served by, and is in close proximity to frequent transit service;
- 3) Transportation characteristics of persons residing, working, or visiting the site are such that they reduce parking demand;
- 4) A transportation demand management program will reduce parking demand at the site; or
- 5) The applicant has provided on-site parking for car share vehicles.

For additional Old Town Pinole parking standards, please refer to Chapter 17.48 of the Pinole Municipal Code.

For definitions of development standard terminology, refer to the Pinole Zoning Ordinance (PMC Title 17).

#### 6.5 SAN PABLO AVENUE SPECIFIC PLAN AREA

This section of the chapter includes provisions that are unique to the San Pablo Avenue Specific Plan Area. These provisions are in addition to the general land use, development standards, and design guidelines listed in other sections of this chapter. Where the provisions of this section conflict with the general provisions that are applicable to all Specific Plan areas, the provisions in this section shall apply.

### **VISION**

The vision for San Pablo Avenue is that of a tree lined corridor promoting pedestrian movement, bicycle mobility and safe vehicular movement. At entrances to San Pablo Avenue and the City limits of Pinole on San Pablo Avenue, gateways will provide San Pablo and the City of Pinole and distinct sense of identity. An expanded pedestrian environment will be established by providing wider sidewalks, street trees and on-street parking serving as a buffer where appropriate.

San Pablo Avenue is further divided into three sub-areas – Old Town, Service, and Mixed Use as shown in Figure 6.1.

Old Town Sub-Area: The vision for Old Town sub-area district is to maintain and enhance its quaint residential, commercial and mixed-use nature, architectural scale, character, style, and serve as a connection to Old Town Pinole.

Service Sub-Area: The Service sub-area will serve as the gateway into Pinole and will encourage light and green industry. This sub-area will serve as a transition area between the Mixed Use sub-area and the western City limits of Pinole. It will embrace the area's historic character as an industrial area while moving towards newer cleaner industry while allowing land use flexibility.

Mixed Use Sub-Area: The Mixed Use sub-area creates a bridge between historic Old Town Pinole and the Service sub-area. It is composed of existing single-family residential homes with good views of local hillsides and toward San Pablo Bay. Parks and open spaces will serve as central neighborhood unifying features and multi-family residences will be designed to be compatible in scale with existing development. A varied mix of uses (low density residential, multi-family residential, professional office, and service commercial) will play a vital role in fulfilling this vision.

#### **LAND USES**

#### A. Land Use Plan

The San Pablo Avenue Land Use Plan/Zoning Map (Figure 6.2) establishes six base land use districts for the Project Area. The districts include Medium-Density Residential (MDR), Very High Density Residential (VHDR), Residential Mixed Use (RMU), Commercial Mixed Use (CMU), Office Professional Mixed Use (OPMU), Office Industrial Mixed Use (OIMU) and Public/Quasi-Public/Institutional (PQP) which are described in section 6.3.1. These districts will facilitate San Pablo Avenue's transformation from its current land use configuration to the one described by the Specific Plan's vision. This configuration includes the mixing of commercial retail and service uses, office uses, and residences at key locations, the inclusion of new mixed-density neighborhoods and the intensification of existing neighborhoods throughout the Plan Area, and the continued inclusion of office uses in select locations.

The proposed land plan introduces significant changes to the existing land use pattern along San Pablo Avenue. Currently, this corridor contains a wide variety of commercial uses including office parks, small businesses and a local shopping mall; light industrial uses such as Sugar City; and, historic homes and buildings. The Specific Plan seeks to protect existing industry from conflicting land uses, and focuses the commercial and residential activity in pulse points to make San Pablo Avenue into a pedestrian-oriented corridor.

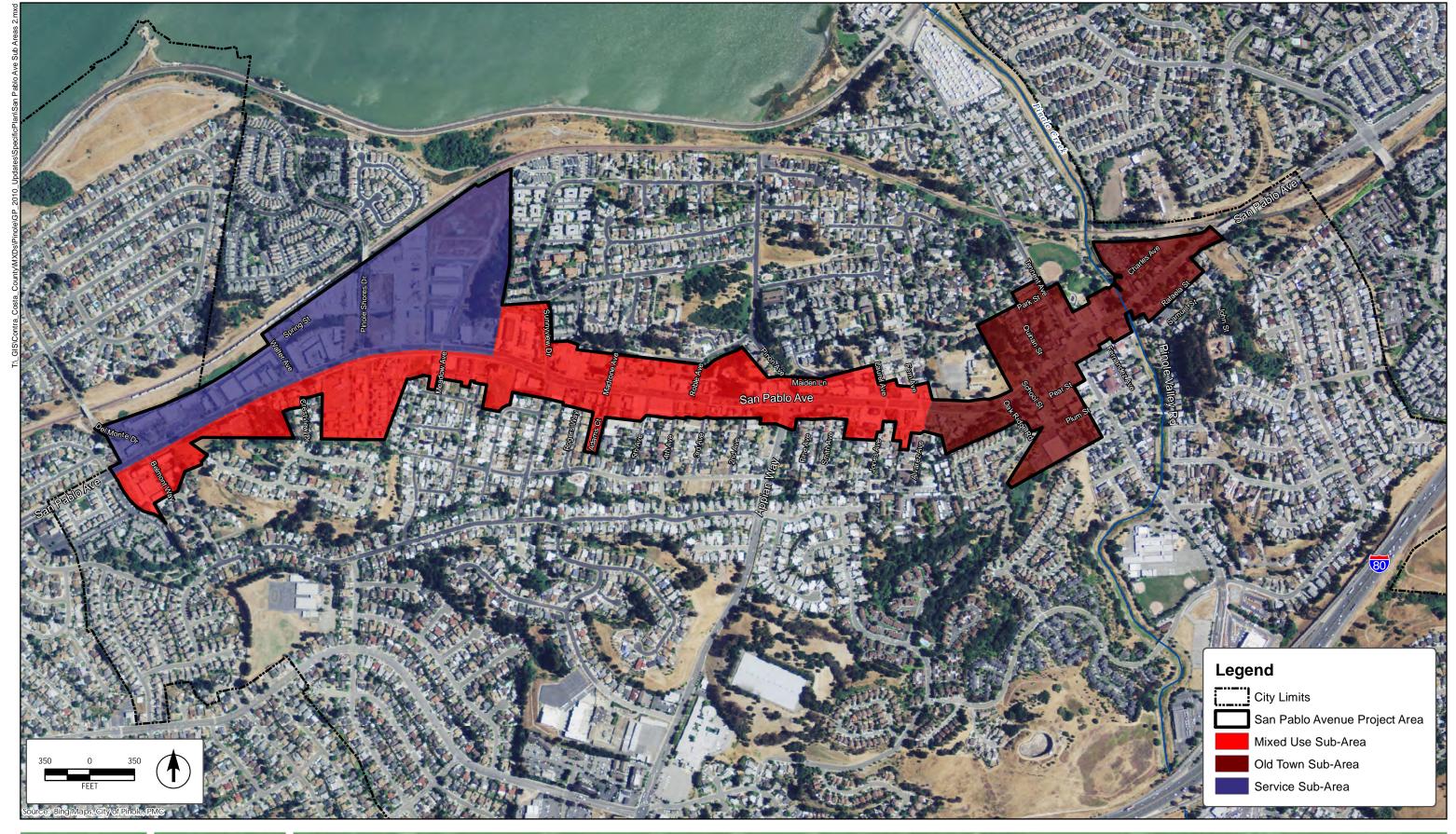
In addition, the land use districts along San Pablo Avenue give preference to replacing vacated and underutilized commercial areas with new residential and commercial mixed-use developments and increasing the density of a few existing residential areas along the Corridor.

The plan calls for the eventual transformation of underutilized sites in this area, expanding office uses and introducing commercial service uses onto the parcels adjacent to Pinole Shores. This plan allows for increased residential densities in the neighborhood. The Specific Plan also allows, by right, public and quasi-public uses in all base designations.

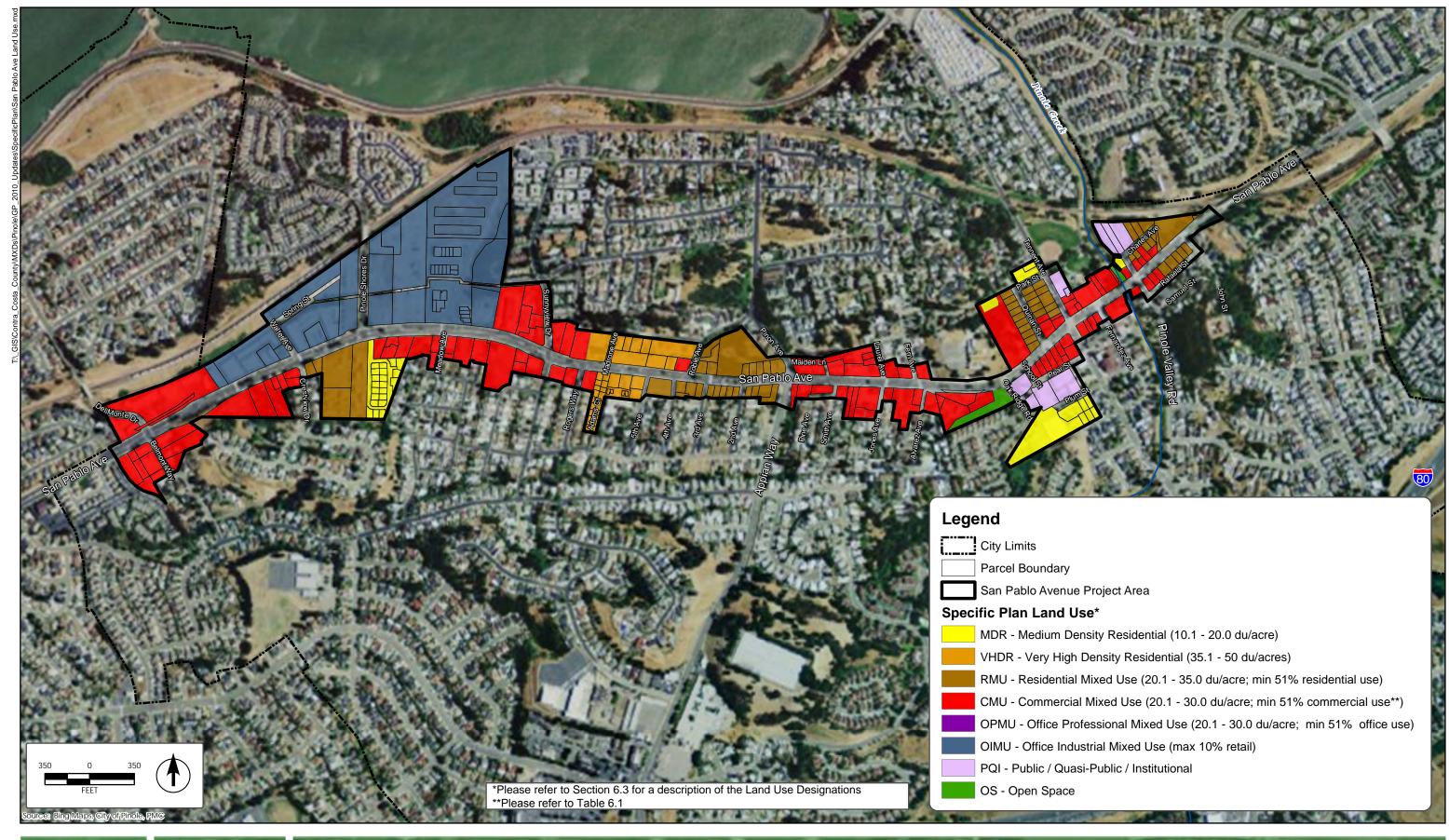
#### B. Land Use Matrix

Permitted land uses for the San Pablo Avenue sub-areas are listed below in **Table 6.2** Use listings are grouped by a general category on the basis of common function, product, or compatibility characteristics and are defined in the Glossary. Each use is subject to the density limitations and development standards established by the governing sub-area and district.

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# Table 6.2 Permitted Land uses for San Pablo Avenue

Permitted Use provisions are symbolized in the table as follows:  $P = Land \ Use \ permitted \ by \ right$   $CUP = Land \ Use \ permitted \ with \ approval \ of \ Conditional \ Use \ Permit$   $N = Land \ Use \ not \ permitted$   $N/A = Not \ Applicable$ 

LAND USE		Old T	own Sul	o-Area			Se	rvice Sub-	Area			Mixe	d-Use Su	b-Area	
CLASSIFICATIONS	MDR		RMU	CMU	OPMU	PQI	CMU	OPMU	OIMU	PQI	VHDR	RMU	CMU	OPMU	PQI
Residential Use Listings															
Adult Day Care Home	Р		Р	Р	Р	N	Р	Р	Р	N	Р	Р	Р	Р	Р
Child Day Care Facility	CUP		CUP	CUP	CUP	CUP	CUP	CUP	N	CUP	CUP	CUP	CUP	CUP	CUP
Dwelling, Multifamily	Р		Р	P⁵	P⁵	N	P⁵	P⁵	CUP	N	Р	Р	P⁵	P⁵	N
Dwelling, Second Unit	CUP		N/A <sup>e</sup>	N/A <sup>e</sup>	N/A <sup>e</sup>	N	N/A <sup>e</sup>	N/A <sup>e</sup>	N/A <sup>e</sup>	N	N/A <sup>e</sup>	N/A <sup>e</sup>	N/A <sup>e</sup>	N/A <sup>e</sup>	N
Dwelling, Single-family	Р		Р	N	N	N	N	N	N	N	N	N	N	N	N
Dwelling, Two-family	P <sup>d</sup>		P <sup>d</sup>	N <sup>d</sup>	N	N	N	N	N	N	N	N	N	N	N
Family Day Care Home, Small (6 children or less)	Р		Р	Р	Р	Р	Pd	P <sup>d</sup>	N	N	Р	Р	Pd	Р	N
Family Day Care Home, Large (7 – 24 children)	Pª		P <sup>a</sup>	Pª	Pª	N	Р	Р	N	Р	Р	Р	Р	N	N
Home Occupation	Р		Р	Р	Р	N	Pª	P <sup>a</sup>	Pª	Р	P <sup>d</sup>	P <sup>d</sup>	Pª	Р	N
Live-Work Facility	Р		Р	Р	Р	Р	Р	Р	Р	Р	N	Р	Р	Р	Р
Residential Care Home	CUP		Р	Р	Р	CUP	CUP⁵	CUP⁵	CUP	CUP	CUP	Pª	CUP⁵	CUP⁵	N
Emergency Shelter/ Temporary Home	N		N	N	N	N	CUP⁵	CUP <sup>b</sup>	Р	CUP	CUP	N	CUP⁵	CUP <sup>b</sup>	N

LAND USE	Old Town Sub-Area						Se	rvice Sub-	Area			Mixe	d-Use Su	b-Area	
CLASSIFICATIONS	MDR		RMU	CMU	OPMU	PQI	CMU	OPMU	OIMU	PQI	VHDR	RMU	CMU	OPMU	PQI
Community Service Use L	istings														
Clubs, Lodges, Private Meeting Halls	N		Р	Р	Р	Р	Р	Р	Р	Р	N	Р	Р	Р	Р
Indoor Amusement/ Entertainment	N		CUP	CUP	CUP	CUP	CUP	CUP	CUP	CUP	N	CUP	CUP	CUP	CUP
Indoor Fitness and Sports Facilities	N		CUP	CUP	CUP	CUP	CUP	CUP	CUP	CUP	N	CUP	CUP	CUP	CUP
Libraries and Museums	Р		Р	Р	Р	Р	Р	Р	Р	Р	N	N	Р	Р	Р
Outdoor Commercial Recreation	N		N	Р	Р	Р	Р	Р	Р	Р	N	N	Р	Р	Р
Parks and Public Plazas	Р		Р	Р	Р	Р	Р	Р	Р	Р	Р	Р	Р	Р	Р
Public Safety Facility	CUP		Р	Р	Р	Р	Р	Р	Р	Р	CUP	Р	Р	Р	Р
Religious Institutions	CUP		CUP	CUP	CUP	CUP	CUP	CUP	CUP	CUP	CUP	CUP	CUP	CUP	CUP
Schools – Public	CUP		Р	Р	Р	Р	Р	Р	Р	Р	CUP	Р	Р	Р	Р
Schools – Private, Special/Studio	N		Р	Р	Р	Р	Р	Р	Р	Р	N	Р	Р	Р	Р
Theatres and Auditoriums	N		Р	Р	Р	Р	Р	Р	Р	Р	N	Р	Р	Р	Р
Utility, Transportation, and	Commun	ication U	se Listing:	S											
Bus and Truck Terminal Parking	N		N	N	N	N	N	N	CUP	N	N	N	N	N	N
Heliports	N		N	N	N	N	N	N	N	N	N	N	N	N	N
Park and Ride Facility	N		N	N	CUP	CUP	N	CUP	CUP	CUP	N	N	CUP	CUP	CUP
Public Parking	Р		Р	Р	Р	Р	Р	Р	Р	Р	Р	Р	Р	Р	Р
Telecommunications Facility	CUP		CUP	CUP	CUP	CUP	CUP	CUP	CUP	CUP	N	CUP	CUP	CUP	CUP
Transit Stations and Terminals	CUP		CUP	CUP	CUP	CUP	CUP	CUP	CUP	CUP	CUP	CUP	CUP	CUP	CUP

LAND USE	Old Town Sub-Area						Se	ervice Sub-	Area			Mixe	d-Use Su	b-Area	
CLASSIFICATIONS	MDR		RMU	CMU	OPMU	PQI	CMU	OPMU	OIMU	PQI	VHDR	RMU	CMU	OPMU	PQI
Retail, Service, and Office	Listings														
Adult-Related Business	N		N	N	N	N	N	N	CUP	N	N	N	N	N	N
Alcoholic Beverage Sales	N		CUP	CUP	CUP	CUP	CUP	CUP	CUP	CUP	N	CUP	CUP	CUP	CUP
Banks and Financial Services	N		Р	Р	Р	N	Р	Р	Р	N	N	Р	Р	Р	N
Bars and Nightclubs	N		CUP	CUP	CUP	N	CUP	CUP	CUP	CUP	N	CUP	CUP	CUP	N
Bed and Breakfast	CUP		CUP	CUP	N	N	CUP	N	N	N	CUP	CUP	CUP	N	N
Building Materials Stores and Yards	N		CUP	CUP	N	N	N	N	CUP	N	N	N	N	N	N
Business Support Services	N		N	CUP	CUP	N	Р	Р	Р	N	N	Р	Р	Р	N
Drive-in/Drive-through sales/services	N		N	N	N	N	N	CUP	CUP	N	N	N	CUP	CUP	N
Equipment Sales and Rental	N		CUP <sup>C</sup>	Р	CUP	N	N	CUP	CUP	N	N	N	N	CUP	N
Home Improvement Supplies	N		CUP	Р	CUP	N	Р	CUP	CUP	N	N	CUP°	Р	CUP	N
Hotels/Motels	N		N	CUP	CUP	N	CUP	CUP	CUP	N	N	CUP	CUP	CUP	N
Kennels	N		N	CUP	CUP	N	CUP	CUP	CUP	N	N	N	CUP	CUP	N
Medical Services - General	N		N	CUP	CUP	N	CUP	CUP	CUP	N	N	N	CUP	CUP	N
Medical Services – Hospital	N		N	CUP	CUP	Р	Р	Р	Р	Р	N	N	CUP	CUP	Р
Office – Business and Professional	N		Р	Р	Р	Р	Р	Р	Р	Р	N	Р	Р	Р	Р
Personal Services	CUP		Р	Р	Р	Р	Р	Р	Р	Р	CUP	Р	Р	Р	Р
Personal Services - Restricted	N		CUP	CUP	CUP	N	CUP	CUP	CUP	N	N	CUP	CUP	CUP	N
Restaurants	N		Р	Р	Р	N	Р	Р	Р	N	N	Р	Р	Р	N

LAND USE		Old	Town Su	b-Area			Se	ervice Sub-	Area			Mixe	d-Use Su	b-Area	
CLASSIFICATIONS	MDR		RMU	CMU	OPMU	PQI	CMU	OPMU	OIMU	PQI	VHDR	RMU	CMU	OPMU	PQI
Retail	N		Р	Р	Р	N	Р	Р	Р	N	N	Р	Р	Р	N
Veterinary Facility	N		CUP	Р	Р	N	Р	Р	Р	N	N	CUP	Р	Р	N
Automotive and Vehicle U	se Listings	S													
Auto Parts Sales	N		CUP	Р	Р	N	Р	Р	Р	N	N	CUP	Р	Р	N
Car Wash and Detailing	N		N	N	N	N	N	N	CUP	N	N	N	N	N	N
Service Stations	N		N	N	N	N	N	N	CUP	N	N	N	N	N	N
Vehicle Services	N		N	CUP	CUP	N	CUP	CUP	CUP	CUP	N	N	CUP	CUP	N
Industrial, Manufacturing,	and Proce	essing Us	e Listings												
Manufacturing - Minor	N		CUP	CUP	N	N	N	N	CUP	N	N	N	N	N	N
Cottage Industry	N		Р	Р	N	N	Р	Р	Р	N	N	CUP	N	N	N
Personal Storage Facility	N		N	N	N	N	N	N	CUP	N	N	N	N	N	N
Printing and Publishing	N		CUP	Р	Р	N	Р	Р	Р	N	N	CUP	Р	Р	N
Recycling Facility – Collection	N		Р	Р	Р	Р	Р	Р	Р	Р	Р	Р	Р	Р	Р
Recycling Facility – Processing	N		N	N	N	N	N	N	CUP	N	N	N	N	N	N

#### Footnotes:

- a Home occupations must have no external evidence of business activity, (e.g. signs, noise, odor, vibration, etc.) or reduce available parking. No customers or employees are allowed at the home and no advertising which gives the home address is allowed.
- b Not allowed on first floor at street frontage, except for residential development on a property with an affordable housing agreement and when that development includes community benefits as specified in the General Plan.
- c Square footage not to exceed 10,000 Square feet.
- d Not permitted if the same use is preexisting and within 200'
- e See Multi-Family Residential

#### **DEVELOPMENT STANDARDS**

### A. Building Height Requirements

The maximum height in San Pablo Avenue varies by sub-area, and is intended to preserve the character of Old Town, the small town feel of Pinole while encouraging compact, walkable, development that will stimulate economic growth.

Table 6.3 Height Requirements

Maximum Height	Old Town	Mixed Use	Service
	40' or 3 stories*	50' or 4 stories*	50' or 4 stories*

<sup>\*</sup> See Special Height Requirements:

### **B. Building Height Exceptions**

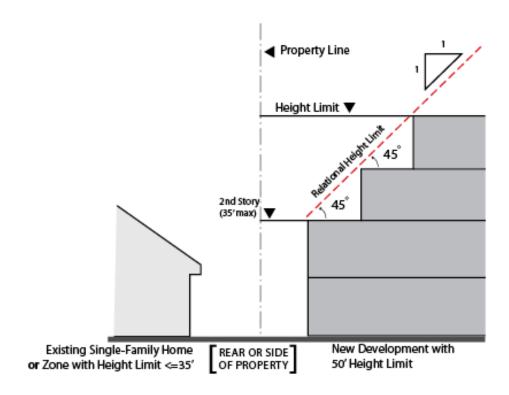
- Minor projections. Minor projections for the purpose of shelter for mechanical equipment, elevator and mechanical equipment enclosures, radio or television antennas, and similar structures and necessary mechanical appurtenances may be erected on a building to exceed the height limits of the applicable Zoning District, or of that use, by a maximum of 15 feet, provided the projections are screened by a parapet or pitched roof or other method acceptable to the Community Development Director.
- 2) Architectural features. Clock towers, cupolas, towers, and similar structures may exceed the height limit as listed below. Signs shall not be included within the additional height allowed.
  - i) Up to 20 feet, if located at a street intersection.
  - ii) Up to 12 feet, it located midblock. These features shall not exceed a width of 25 feet or one-third of the length of the building façade, whichever is less.

The following regulations are established to create an appropriate relationship between new development within the Specific Plan Area and existing single family buildings or existing zones with lower allowable heights.

### C. Special Height Requirements

- 1) Where new development is on a parcel abutting a parcel (either within or outside of the Specific Plan Boundary) with an existing single family home or zoned with a height limit of 35' or less:
  - i) As shown in Figure 6.3 Special Height Requirements (Rear or Side Setback), any additional stories above the 2nd story must be set back at a one to one height to setback ratio, or a 45 degree slope.

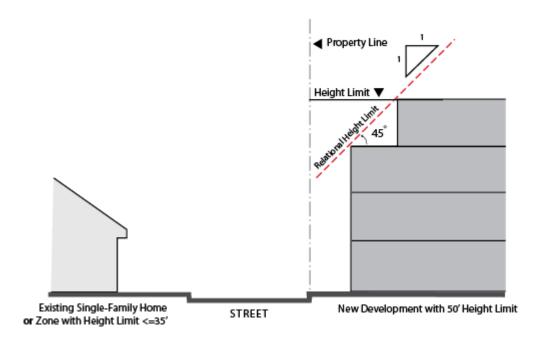
Figure 6.3
Special Height Requirements (Rear or Side Setback)



2) Where new development is on a parcel **across the street** from a parcel (either within or outside of the Specific Plan Boundary) with an existing single family home **or** zoned with a height limit of 35' or less:

i) As shown in **Figure 6.4** Special Height Requirements (Across the Street Setback), any additional stories above the 3rd story must be set back at a one to one height to setback ratio, or a 45 degree slope.

Figure 6.4
Special Height Requirements (Across the Street Setback)



### D. Building Placement

The setbacks and "Build-To" lines for San Pablo Avenue are intended to enhance social interactions in the historic Old Town core while simultaneously providing appropriate levels of privacy in residential areas.

Table 6.4 displays setback requirements for each sub-area in San Pablo Avenue

# Table 6.4 Setback Requirements

Building Placement	Old Town	Mixed-Use	Service
"Build To" Front Line*	Minimum: 0' Maximum: 5'	Minimum: 0' Maximum: 10'	Minimum: 0' Maximum: 5'
Minimum Side Yard**	0' except when adjacent to residential uses. When abutting existing residential, minimum setback is 15'.	0' except when adjacent to residential uses. When abutting existing residential, minimum setback is 15'.	0' except when adjacent to residential uses. When abutting existing residential, minimum setback is 15'.
Minimum Rear Yard	5' except when adjacent to residential uses. When abutting existing residential, minimum setback is 15'	0' except when adjacent to residential uses. When abutting existing residential, minimum setback is 15'.	0' except when adjacent to residential uses. When abutting existing residential, minimum setback is 15'.

<sup>\*</sup>Build To Front Line Exceptions:

Within the Old Town Sub-Area, the maximum Build to Front Line standard may be increased through project review in order to reduce conflicts with neighboring development patterns (e.g. adjoining craftsman style homes) that establish a different prevailing setback.

### \*\*Side Yards:

Within the Old Town Sub-Area, the side yard setback distances are total separation distances (i.e., combined side yard setback distances rather than a minimum distance for each side yard).

### E. Allowed Frontage Types

Frontage type refers to the architectural composition of the front façade of a building; particularly concerning how it relates and ties into the surrounding public realm. The San Pablo Avenue frontage types are intended to enhance social interactions and stimulate interest while simultaneously providing appropriate levels of privacy in residential areas. Allowed frontage types in the different sub-districts districts are listed in **Table 6.5** and defined below. An "X" means that the frontage type is allowed; a blank cell means that the frontage type is not allowed.

Table 6.5 Frontage Types

Frontage Type	Old Town	Mixed-Use	Service
Arcade	X	Х	X
Gallery	X	Х	X
Shopfront & Awning	X	Х	X
Stoop	X	Х	X
Dooryard/Terrace	X	Х	X
Porch	X	X	X
Minimum Frontage Requirement	80%	60%	30%

<sup>1.</sup> Arcades shall have a minimum depth of 10 feet from the building line to the inside column face and a clear height of 10 feet.

### F. Allowable Building Types

The following building types are intended to provide a variety of flexible building styles appropriate for the small town character of Pinole that can be used to guide future development. Allowed building types in the different sub-districts are listed in **Table 6.6** and defined below. An "X" means that the building type is allowed; a blank cell means that the building type is not allowed.

Table 6.6 Building Types

Allowed Building Type	Old Town	Mixed-Use	Service
Podium	Х	Х	X
Full Block Liner			X
Half Block Liner	Х	X	X
Quarter Block/ Infill Housing	Х	X	X
Stacked		X	X
Terraced	Х	X	X
Carriage House	X	X	
Du/Tri/Quadplex	X	X	X
Multi-Family Faux House	X		
Rowhouse	X	X	X
Courtyard Housing		X	

### G. Allowable Parking Types

The following parking types are intended to provide a variety of flexible parking styles appropriate for San Pablo Avenue that can be used to guide and promote future development. Allowed parking types in the different sub-districts are listed in **Table 6.7** and defined below. An "X" means that the building type is allowed; a blank cell means that the building type is not allowed.

Table 6.7
Parking Types & Parking Ratios

Allowed Parking Type	Old Town**	Mixed-Use	Service
Parking Structure	X*	X	X
Behind Structure	Х	X	X
Next to Structure	Х	Х	Х
Alley Access	Х	Х	
Subterranean	Х	Х	Х
Tucked-under Structure	Х	X	Х

<sup>\*</sup> Lined with ground floor retail

Parking Ratios for Old Town Pinole: Within Old Town Pinole, the City will apply the Old Town Parking Overlay District. However, the Specific Plan increases the development potential in the Old town core which will create an increased demand for off-street parking. The City should periodically evaluate parking demand in Old Town for the purpose of determining whether a public parking structure should be provided to support commercial, civic, and recreational activity in Old Town. A public parking structure could support economic development while minimizing surface parking area.

### 6.6 PINOLE VALLEY ROAD SPECIFIC PLAN AREA

This section of the chapter includes provisions that are unique to the Pinole Valley Road Specific Plan Area. These provisions are in addition to the general land use, development standards, and design guidelines listed in other sections of this chapter. Where the unique provisions in this section conflict with any general provisions applicable to all Specific Plan areas, the provisions in this section shall apply.

#### **VISION**

The vision for Pinole Valley Road is that it will be a pedestrian-friendly corridor where each use has its own distinct character, and walking from one use to another will be an interesting experience. Pinole Valley Road will serve as a 'gateway' into Pinole and will accommodate various entertainment and outdoor activities. Widened sidewalks with landscaping will have the 'look and feel' of a well-landscaped neighborhood. Side streets will be lined with trees and streetlights will relate to the quality of landscaping and paving.

Pinole Valley Road is further divided into the Old Town, Service and Corridor Sub-Areas as shown in Figure 6.3.

**Old Town Sub-Area:** The vision for Old Town sub-area is to maintain and enhance its quaint residential, commercial and mixed-use nature, architectural scale, character, style, and serve as a connection to Old Town Pinole

Service Sub-Area: Service sub-area will serve as the gateway into Pinole and will maintain existing employment opportunities while creating the potential for future housing opportunity through mixed use development with a variety of housing densities. The scale and massing of existing and new services will be compatible with Pinole's character. This district will serve as a transition area between the urban core and the existing single-family residential areas in Old Town and outside the Specific Plan boundaries. It will embrace the area's historic character and be respectful of the existing single-family residences in this neighborhood; many of these residences will be converted to office, restaurant, and other uses.

Corridor Sub-Area: The vision for Corridor sub-area is to provide civic identity and circulation amenities in the transition between the Service sub-area and rural lands to the South of the City. The corridor will be upgraded to integrate a bicycle path or a trail system that encourages pedestrian movement, and to provide gateway identification.

#### **LAND USES**

#### A. Land Use Plan

The Pinole Valley Road Land Use Plan/Zoning Map (Figure 6.4) establishes five land use districts for the corridor. The districts include Medium-Density Residential (MDR), Residential Mixed Use (RMU), Commercial Mixed Use (CMU), Office Professional Mixed Use (OPMU), and Public/Quasi-Public/Institutional (PQP) which are described in section 6.3.1. These districts will facilitate Pinole Valley Road's transformation from its current land use configuration to the one described by the Specific Plan's vision. This configuration includes the mixing of commercial retail and service uses, office uses, and residences at key points, the inclusion of new mixed-density neighborhoods and the densification of existing neighborhoods throughout the Planning Area, and the continued inclusion of office uses in select locations.

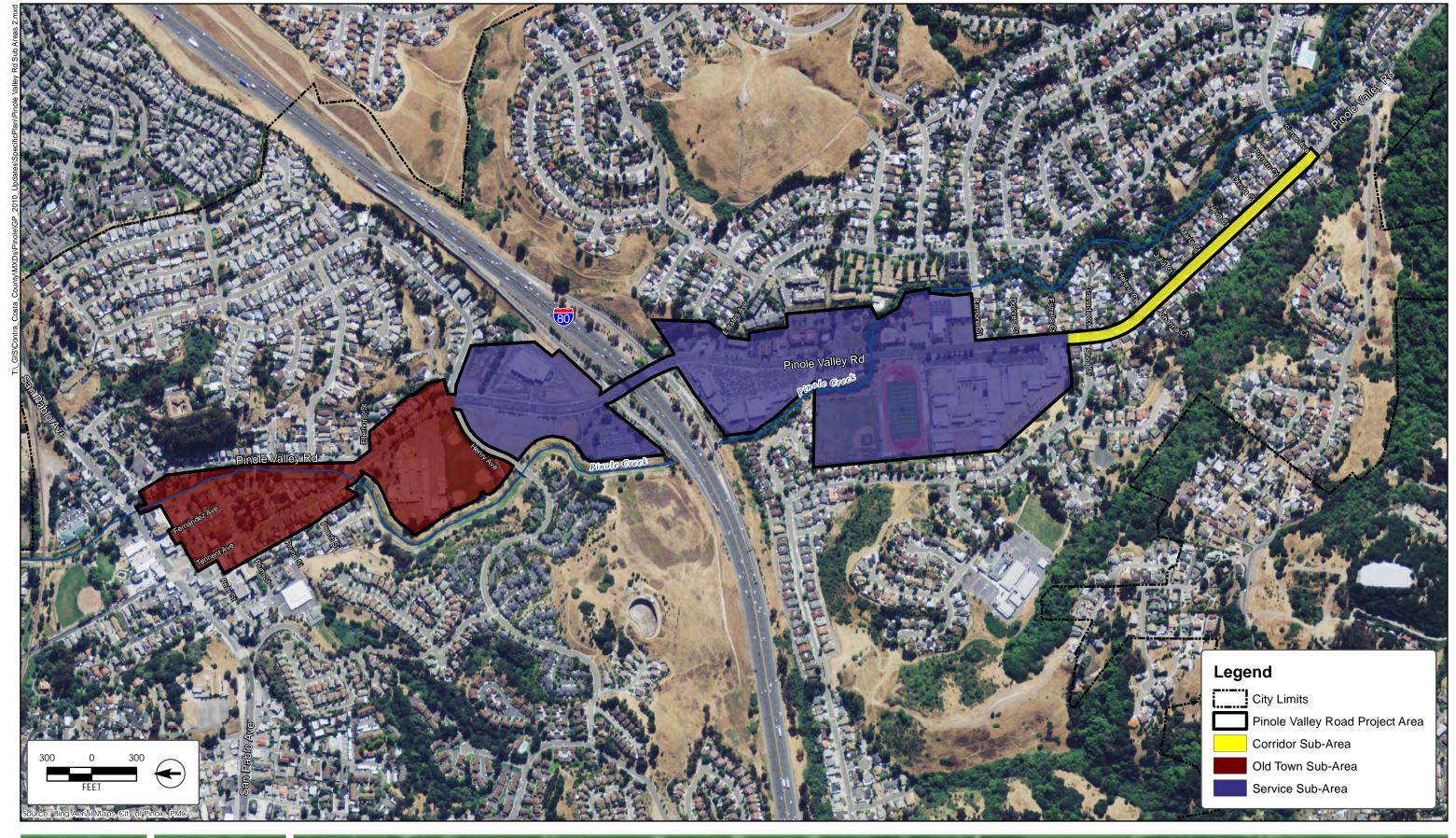
The proposed land plan and designations introduce significant changes to the existing land use pattern along Pinole Valley Road. Currently, Pinole Valley Road is lined with commercial retail and service uses. The Specific Plan seeks to focus these commercial activities in combination with office and residential uses to make Pinole Valley Road into a pedestrian-oriented corridor.

In addition, policies give preference to replacing vacated and underutilized commercial areas with new office professional developments and increasing the density of a few existing residential areas along the Corridor.

The plan calls for the eventual transformation of underutilized sites in this area, expanding office uses and introducing commercial service uses onto the parcels adjacent to Ramona Street. This plan allows for an increase in residential densities in the neighborhood (of high-quality development). The Specific Plan also allows, by right, public and quasi-public uses in all base designations.

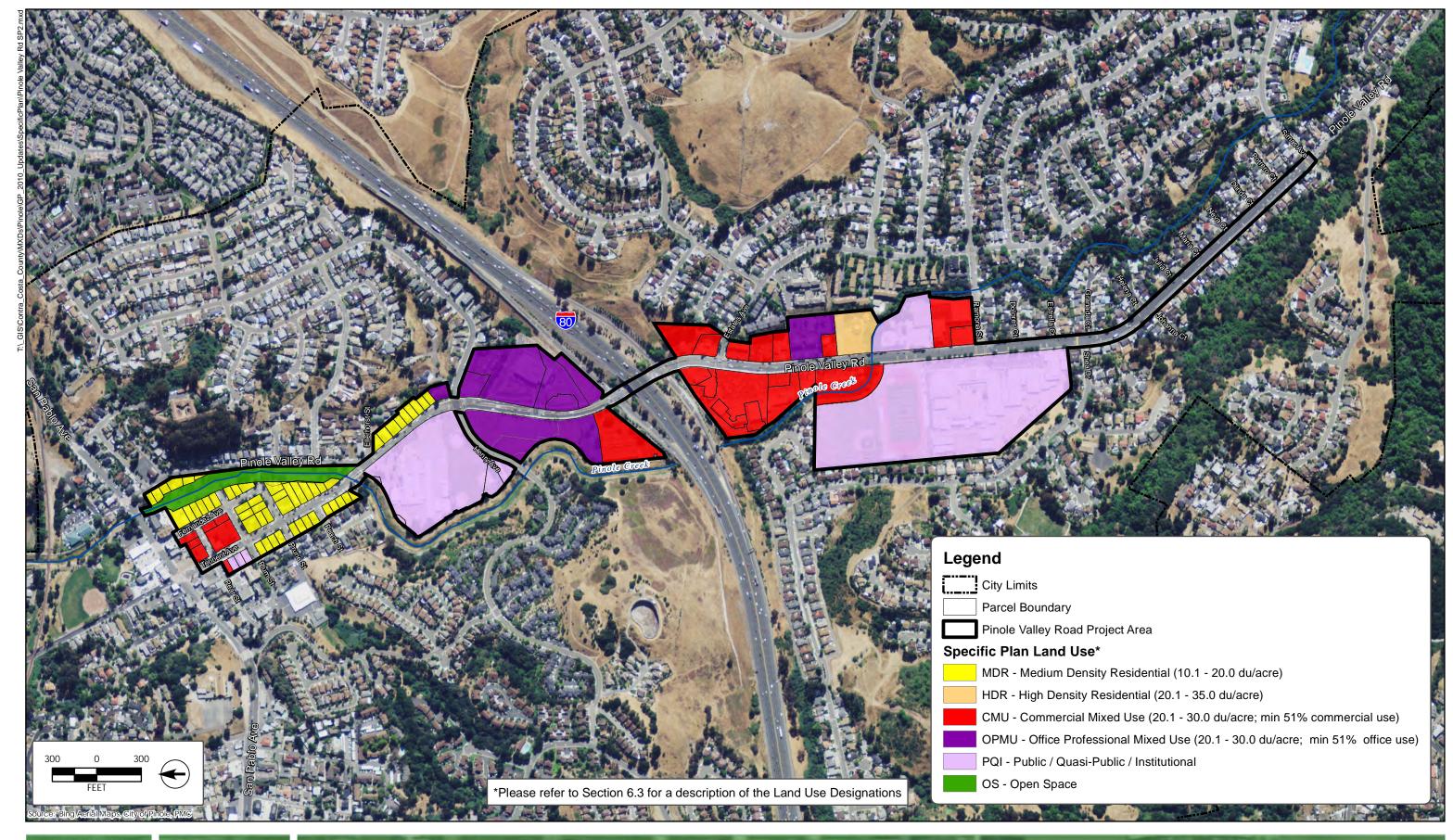
#### **B.** Land Use Matrix

Permitted land uses for the Pinole Valley Road sub-areas are listed below in **Table 6.8**. Use listings are grouped by a general category on the basis of common function, product, or compatibility characteristics and are defined in the Glossary.











# Table 6.8 Permitted Use Table for Pinole Valley Road

Permitted Use provisions are symbolized in the table as follows: P = Land Use permitted by right CUP = Land Use permitted with approval of Conditions Use Permit N = Land Use not permitted N/A = Not Applicable

Land Has Obserification	Old Town Sub-Area				Service Sub-Area		
Land Use Classifications	MDR	HDR	CMU	PQI	CMU	ОРМИ	PQI
Residential Use Listings							
Adult Day Care Home	Р	Р	Р	N	Р	Р	N
Child Day Care Facility	CUP	CUP	CUP	N	CUP	CUP	N
Dwelling, Multifamily	Р	Р	P⁵	N	P <sup>b</sup>	P⁵	N
Dwelling, Second Unit	N/A <sup>e</sup>	N/A <sup>e</sup>	N/A <sup>e</sup>	N	N/A°	N/A <sup>e</sup>	N
Dwelling, Single-family	Р	N	N	N	N	N	N
Dwelling, Two-family	N/A <sup>e</sup>	N/A <sup>e</sup>	N/A <sup>e</sup>	N	N/A°	N/A <sup>e</sup>	N
Family Day Care Home, Small (6 children or less)	N	Р	Р	Р	P <sup>d</sup>	P <sup>d</sup>	N
Family Day Care Home, Large (7 – 24 children)	CUPª	CUPª	CUPª	N	Р	Р	Р
Home Occupation	N	Р	Р	N	Pª	Pª	Р
Live-Work Facility	Р	Р	Р	Р	Р	Р	Р
Residential Care Home	CUP	Р	Р	CUP	CUP⁵	CUPb	CUP
Emergency Shelter/ Temporary Home	N	N	N	N	CUP⁵	CUP <sup>b</sup>	CUP

Land Har Classifications		Old Town	n Sub-Area	Service Sub-Area			
Land Use Classifications	MDR	HDR	CMU	PQI	СМИ	ОРМИ	PQI
Community Service Use Listings							
Clubs, Lodges, Private Meeting Halls	N	CUP	CUP	CUP	CUP	CUP	CUP
Indoor Amusement/ Entertainment	N	CUP	CUP	CUP	CUP	CUP	CUP
Indoor Fitness and Sports Facilities	N	CUP	CUP	CUP	CUP	CUP	CUP
Libraries and Museums	N	Р	Р	Р	Р	Р	Р
Outdoor Commercial Recreation	N	N	Р	Р	Р	Р	Р
Parks and Public Plazas	Р	Р	Р	Р	Р	Р	Р
Public Safety Facility	CUP	CUP	Р	Р	Р	Р	Р
Religious Institutions	CUP	CUP	Р	Р	Р	Р	Р
Schools – Public	CUP	CUP	Р	Р	Р	Р	Р
Schools - Private, Special/Studio	N	N	CUP	CUP	CUP	CUP	CUP
Theatres and Auditoriums	N	N	CUP	CUP	CUP	CUP	CUP
Utility, Transportation, and Communication U	se Listings						
Heliports	N	N	N	N	N	N	N
Park and Ride Facility	N	N	N	CUP	CUP	CUP	CUP
Public Parking	Р	Р	Р	Р	Р	Р	Р
Telecommunications Facility	CUP	CUP	CUP	CUP	CUP	CUP	CUP
Transit Stations and Terminals	CUP	CUP	CUP	CUP	CUP	CUP	CUP

Lord Hoo Classifications		Old Town	Service Sub-Area				
Land Use Classifications	MDR	HDR	СМИ	PQI	СМИ	ОРМИ	PQI
Retail, Service, and Office Listings							
Adult-Related Business	N	N	N	N	N	N	N
Alcoholic Beverage Sales	N	CUP	CUP	CUP	CUP	CUP	CUP
Banks and Financial Services	N	Р	Р	N	Р	Р	N
Bars and Nightclubs	N	CUP	CUP	N	CUP	CUP	CUP
Bed and Breakfast	CUP	CUP⁵	CUP⁵	N	CUP⁵	N	N
Building Materials Stores and Yards	N	N	N	N	N	N	N
Business Support Services	N	Р	Р	N	Р	Р	N
Drive-in/Drive-through sales/services	N	N	N	N	N	CUP	N
Equipment Sales and Rental	N	N	N	N	N	CUP	N
Home Improvement Supplies	N	CUP <sup>c</sup>	Р	N	Р	CUP	N
Hotels/Motels	N	CUP	Р	N	Р	CUP	N
Kennels	N	N	Р	N	Р	CUP	N
Medical Services - General	N	Р	Р	N	Р	Р	N
Medical Services – Hospital	N	N	Р	Р	Р	Р	Р
Office – Business and Professional	N	Р	Р	Р	Р	Р	Р
Personal Services	CUP	Р	Р	Р	Р	Р	Р
Personal Services - Restricted	N	CUP	CUP	N	CUP	CUP	N
Restaurants	N	Р	Р	N	Р	Р	N
Retail	N	Р	Р	N	CUP	CUP	N
Veterinary Facility	N	CUP	Р	N	Р	Р	N

	Old Town Sub-Area				Service Sub-Area		
Land Use Classifications	MDR	HDR	СМИ	PQI	СМИ	OPMU	PQI
Automotive and Vehicle Use Listings							
Auto Parts Sales	N	N	N	N	N	N	N
Car Wash and Detailing	N	N	N	N	N	N	N
Service Stations	N	N	N	N	N	N	N
Vehicle Services	N	N	CUP	CUP	CUP	CUP	CUP
Industrial, Manufacturing, and Processing Use	Listings						
Cottage Industry	N	N	Р	N	Р	Р	N
Personal Storage Facility	N	N	N	N	N	N	N
Printing and Publishing	N	MUP	Р	N	Р	Р	N
Recycling Facility - Collection	N	Р	Р	Р	Р	Р	Р

#### Footnotes:

a – Home occupations must have no external evidence of business activity, (e.g. signs, noise, odor, vibration, etc.) or reduce available parking. No customers or employees are allowed at the home and no advertising which gives the home address is allowed.

b – Not allowed on first floor at street frontage

c - Square footage not to exceed 10,000 Square feet.

d - Not permitted if the same use is preexisting and within 300'

e - See Multi-Family Residential

#### **DEVELOPMENT STANDARDS**

### A. Building Height Requirements

The maximum height requirements for Pinole Valley Road is intended to preserve the compact, walkable, historic old town core, offer a gradual transition into the surrounding residential neighborhoods while simultaneously stimulating economic development along the Service subdistrict.

Table 6.9 displays height requirements for each sub-area in Pinole Valley Road

Table 6.9 Height Requirements

Maximum Height	Old Town	Service
	40' or 3 stories*	50' or 4 stories*

<sup>\*</sup> See Special Height Requirements:

### **B. Building Height Exceptions**

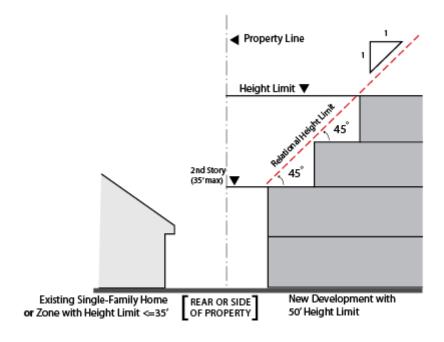
- Minor projections. Minor projections for the purpose of shelter for mechanical equipment, elevator and mechanical equipment enclosures, radio or television antennas, and similar structures and necessary mechanical appurtenances may be erected on a building to exceed the height limits of the applicable Zoning District, or of that use, by a maximum of 15 feet, provided the projections are screened by a parapet or pitched roof or other method acceptable to the Community Development Director.
- 2) Architectural features. Clock towers, cupolas, towers, and similar structures may exceed the height limit as listed below. Signs shall not be included within the additional height allowed.
  - i) Up to 20 feet, if located at a street intersection.
  - ii) Up to 12 feet, it located midblock. These features shall not exceed a width of 25 feet or one-third of the length of the building façade, whichever is less.

### C. Special Height Requirements

The following regulations are established to create an appropriate relationship between new development within the Specific Plan Area and existing single family buildings or existing zones with lower allowable heights.

- 1) Where new development is on a parcel abutting a parcel (either within or outside of the Specific Plan Boundary) with an existing single family home or zoned with a height limit of 35' or less:
  - i) As shown in **Figure 6.7** Special Height Requirements (Rear or Side Setback), any additional stories above the 2nd story must be set back at a one to one height to setback ratio, or a 45 degree slope.

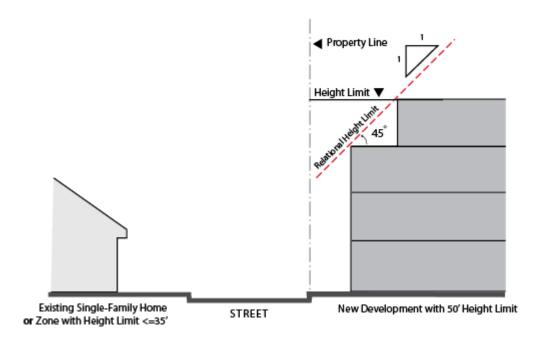
Figure 6.7
Special Height Requirements (Rear or Side Setback)



2) Where new development is on a parcel across the street from a parcel (either within or outside of the Specific Plan Boundary) with an existing single family home or zoned with a height limit of 35' or less:

i) As shown in **Figure 6.8** Special Height Requirements (Across the Street Setback), any additional stories above the 3rd story must be set back at a one to one height to setback ratio, or a 45 degree slope.

Figure 6.8
Special Height Requirements (Across the Street Setback)



### D. Building Placement

The setbacks and "Build-To" lines for Pinole Valley Road are intended to enhance social interactions between the uses while simultaneously providing appropriate levels of privacy in residential areas.

Table 6.10 displays setback requirements for each sub-area in Pinole Valley Road

# Table 6.10 Setback Requirements

<b>Building Placement</b>	Old Town	Service
"Build To" Front Line*	Minimum: 0'	Minimum: 0'
Maximum: 5'		Maximum: 5'
Minimum Side Yard**	0' except when adjacent to residential uses. When abutting existing residential, setback is 15'.	0' except when adjacent to residential uses. When abutting existing residential, setback is 15'.
Minimum Rear Yard	5' except when adjacent to residential uses. When abutting existing residential, setback is 15'.	0' except when adjacent to residential uses. When abutting existing residential, setback is 15'.

<sup>\*</sup>Build To Front Line Exceptions:

Within the Old Town Sub-Area, the maximum Build to Front Line standard may be increased through project review in order to reduce conflicts with neighboring development patterns (e.g., adjoining craftsman style homes) that establish a different prevailing setback.

#### \*\*Side Yards:

Within the Old Town Sub-Area, the side yard setback distances are total separation distances (i.e., combined side yard setback distances rather than a minimum distance for each side yard).

### E. Allowed Frontage Types

Frontage type refers to the architectural composition of the front façade of a building; particularly concerning how it relates and ties into the surrounding public realm. The Pinole Valley Road frontage types are intended to enhance social interactions and stimulate interest while simultaneously providing appropriate levels of privacy in residential areas. Allowed frontage types in the different sub-districts districts are listed in **Table 6.11** and defined below. An "X" means that the frontage type is allowed; a blank cell means that the frontage type is not allowed.

Table 6.11 Frontage Types

Frontage Type	Old Town	Service
Arcade	X	X
Gallery	Х	Х
Shopfront & Awning	Х	Х
Stoop	Х	
Dooryard/Terrace	X	
Porch	X	
Minimum Frontage Requirement	80%	30%

## F. Allowable Building Types

The following building types are intended to provide a variety of flexible building styles appropriate for the diverse quaint, residential and commercial character of Pinole Valley Road that can be used to guide future development. Allowed building types in the different sub-districts are listed in **Table 6.12** and defined below. An "X" means that the building type is allowed; a blank cell means that the building type is not allowed.

Table 6.12 Building Types

Allowed Building Type	Old Town	Service
Podium	Х	X
Full Block Liner		X
Half Block Liner	Х	x
Quarter Block/ Infill Housing	Х	x
Stacked		x
Terraced	Х	x
Carriage House	Х	
Du/Tri/Quadplex	Х	
Multi-Family Faux House	Х	
Rowhouse	Х	
Courtyard Housing		
Front Yard Housing	Х	

## G. Allowable Parking Types

The following parking types are intended to provide a variety of flexible parking styles appropriate for Pinole Valley Road that can be used to guide and promote future development. Allowed parking types in the different sub-districts are listed in **Table 6.13** and defined below. An "X" means that the building type is allowed; a blank cell means that the building type is not allowed.

Table 6.13 Parking Types

Allowed Parking Type	Old Town	Service
Parking Structure	X*	X
Behind Structure	X	Х
Next to Structure	X	Х
Alley Access	X	
Subterranean	X	Х
Tucked-under Structure	X	Х

<sup>\* -</sup> lined with ground floor retail

## 6.7 APPIAN WAY SPECIFIC PLAN AREA

This section of the chapter includes provisions that are unique to the Appian Way Specific Plan Area. These provisions are in addition to the general land use, development standards, and design guidelines listed in other sections of this chapter. Where the unique provisions in this section conflict with any general provisions applicable to all Specific Plan areas, the provisions in this section shall apply.

#### **VISION**

The vision for Appian Way is that of that of a large-scale shopping and service corridor that attracts new commercial development, while simultaneously improving open space access, automobile flow, and pedestrian and bicycle circulation. Appian Way will serve as a regional "gateway" into Pinole at the I-80 interchange, and at the City Limits boundary with El Sobrante. Architectural elements, landscaping and streetscaping improvements including street trees, landscaped medians, unique gateway monuments, innovative directional signage, and attractive street lighting will ultimately add to the quality of the Corridor and provide a positive image of place and pride to those visiting from other areas.

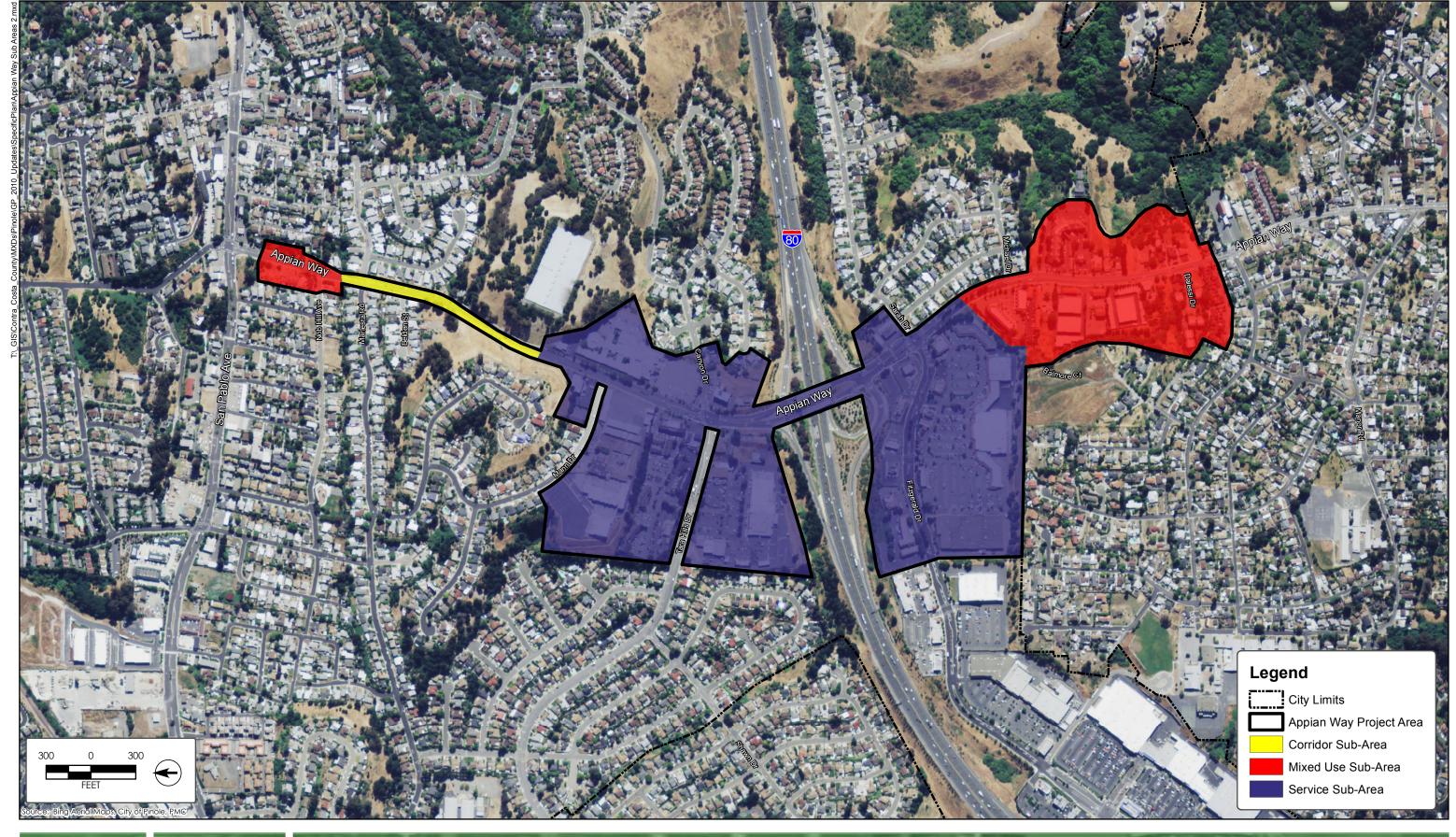
Appian Way is further divided into three sub-areas – Corridor, Service and Mixed-Use.

Corridor Sub-Area: The vision for the Corridor Sub-Area is to provide enhanced streetscape improvements that create a sense of arrival while improving pedestrian safety and circulation as well as supporting multiple modes of transportation. This will be accomplished through upgrades to the vehicular, pedestrian, and bicycle circulation, and the introduction of landscaping, signs, public art and similar amenities.

**Service Sub-Area:** Service sub-area will serve as the regional gateway into Pinole and will capitalize on its existing reputation as a regional shopping center by upgrading existing development, and attracting increasingly desirable commercial tenants.

**Mixed-Use Sub-Area:** Mixed-Use sub-area will encourage mixed use residential and commercial developments with an emphasis on locally serving businesses. Access to transit will highly promote walkability and encourage the mix of uses. A varied mix of uses (multi-family residential, professional office, and service commercial) will play a vital role in fulfilling this vision.

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#### **LAND USES**

#### A. Land Use Plan

The Appian Way Land Use Plan/Zoning Map (Figure 6.10) establishes five land use districts for the Project Area. The districts include High Density Residential (HDR), Residential Mixed Use (RMU), Commercial Mixed Use (CMU), Office Professional Mixed Use (OPMU), and Public/Quasi-Public/Institutional (PQP) districts. These districts will facilitate Appian Way's transformation from its current land use configuration to the one described by the Specific Plan's vision. This configuration includes the mixing of commercial retail and service uses, office uses, and residences at key points, the inclusion of new mixed-density neighborhoods and the densification of existing neighborhoods throughout the Planning Area, and the continued inclusion of office uses in select locations.

The proposed land plan and designations introduce significant changes to the existing land use pattern along Appian Way. Currently, Appian Way is lined with commercial retail, office professional and service uses. The Specific Plan seeks to focus these commercial activities in combination with office, and residential uses to make Appian Way into a regional service and commercial corridor.

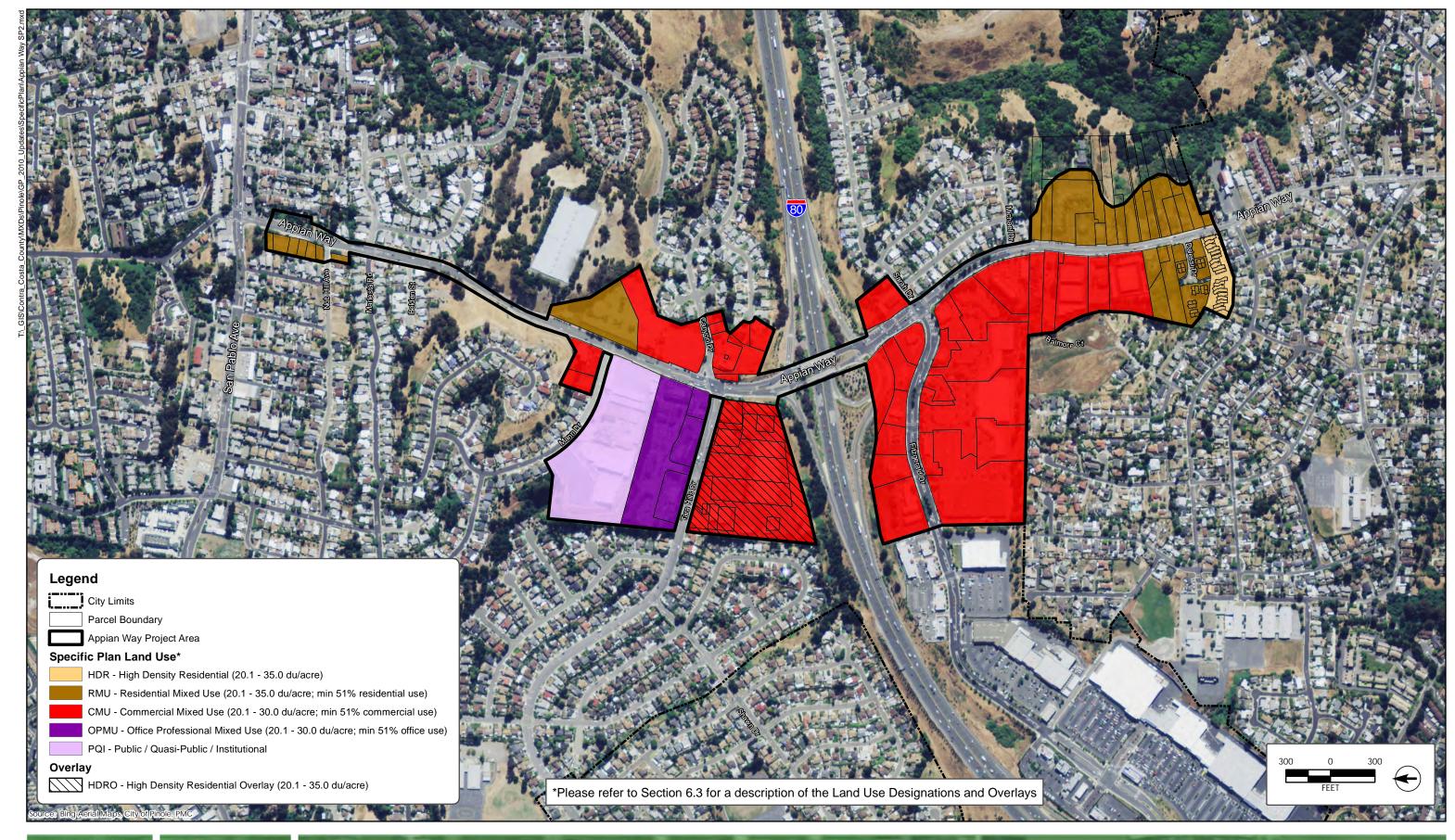
In addition, policies give preference to replacing vacated and underutilized commercial and areas with new office professional and commercial developments and increasing the density of a few existing residential areas along the Corridor.

The plan calls for the eventual transformation of underutilized sites in this area, expanding office uses and introducing commercial service uses onto the parcels that include the existing Doctor's Medical Hospital and Offices, Appian 80 shopping Center and Residential properties south of I-80 fronting Appian Way. This plan allows for an increase in residential densities in the neighborhood (of high-quality development). The Specific Plan also allows, by right, public and quasi-public uses in all base designations.

#### B. Land Use Matrix

Permitted land uses for the Appian Way sub-areas are listed below in **Table 6.14**. Use listings are grouped by a general category on the basis of common function, product, or compatibility characteristics and are defined in the Glossary.

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# Table 6.14 Permitted Use Table for Appian Way

Permitted Use provisions are symbolized in the table as follows: P = Land Use permitted by right CUP = Land Use permitted with approval of Conditions Use Permit N = Land Use not permitted N/A = Not Applicable

	Service Sub-Area			Mixed-Use Sub-Area				
Land Use Classifications	RMU	CMU	ОРМИ	PQI	HDR	RMU	CMU	PQI
Residential Use Listings								
Adult Day Care Home	Р	Р	Р	N	Р	Р	Р	N
Child Day Care Facility	CUP	CUP	CUP	N	CUP	CUP	CUP	CUP
Dwelling, Multifamily	Р	Р	P <sup>b</sup>	N	Р	Р	P⁵	N
Dwelling, Second Unit	N/A <sup>e</sup>	N/A <sup>e</sup>	N/A <sup>e</sup>	N	N/A <sup>e</sup>	N/A <sup>e</sup>	N/A <sup>e</sup>	N
Dwelling, Single-family	N	N	N	N	N	N	N	N
Dwelling, Two-family	N/A <sup>e</sup>	N/A <sup>e</sup>	N/A <sup>e</sup>	N	N/A <sup>e</sup>	N/A <sup>e</sup>	N/A <sup>e</sup>	N
Family Day Care Home, Small (6 children or less)	$P^d$	P <sup>d</sup>	$P^d$	N	P <sup>d</sup>	$P^d$	P <sup>d</sup>	N
Family Day Care Home, Large (7 – 24 children)	CUP	CUP	CUP	CUP	N	CUP	CUP	CUP
Home Occupation	Pª	Pª	Pa	Р	Pª	Pª	Pª	Р
Live-Work Facility	Р	Р	Р	Р	Р	Р	Р	Р
Residential Care Home	CUP	CUP⁵	CUP⁵	CUP	CUP	CUP	CUP⁵	CUP
Emergency Shelter/ Temporary Home	N	N	N	N	N	N	N	N

		Service Sub-Area			Mixed-Use Sub-Area			
Land Use Classifications	RMU	СМИ	ОРМИ	PQI	HDR	RMU	СМИ	PQI
Community Service Use Listings								
Clubs, Lodges, Private Meeting Halls	CUP	CUP	CUP	CUP	N	CUP	CUP	CUP
Indoor Amusement/ Entertainment	CUP	CUP	CUP	CUP	N	CUP	Р	CUP
Indoor Fitness and Sports Facilities	CUP	CUP	CUP	CUP	N	CUP	CUP	CUP
Libraries and Museums	Р	Р	Р	Р	N	Р	Р	Р
Outdoor Commercial Recreation	N	CUP	CUP	CUP	N	N	CUP	CUP
Parks and Public Plazas	Р	Р	Р	Р	Р	Р	Р	Р
Public Safety Facility	Р	Р	Р	Р	N	Р	Р	Р
Religious Institutions	CUP	CUP	CUP	CUP	N	CUP	CUP	CUP
Schools – Public	Р	Р	Р	Р	N	Р	Р	Р
Schools - Private, Special/Studio	CUP	CUP	CUP	CUP	N	CUP	CUP	CUP
Theatres and Auditoriums	CUP	CUP	CUP	CUP	N	CUP	CUP	CUP
Utility, Transportation, and Communication	Use Listings							
Heliports	N	N	N	N	N	N	N	N
Park and Ride Facility	N	CUP	CUP	CUP	N	N	CUP	CUP
Public Parking	Р	Р	Р	Р	Р	Р	Р	Р
Telecommunications Facility	CUP	CUP	CUP	CUP	CUP	CUP	CUP	CUP
Transit Stations and Terminals	CUP	CUP	CUP	CUP	CUP	CUP	CUP	CUP

		Service Sub-Area				Mixed-Use Sub-Area			
Land Use Classifications	RMU	СМИ	ОРМИ	PQI	HDR	RMU	CMU	PQI	
Retail, Service, and Office Listings									
Adult-Related Business	N	N	N	N	N	N	N	N	
Alcoholic Beverage Sales	CUP	CUP	CUP	CUP	N	CUP	CUP	CUP	
Banks and Financial Services	CUP	CUP	CUP	N	N	CUP	CUP	N	
Bars and Nightclubs	CUP	CUP	CUP	CUP	N	CUP	CUP	CUP	
Bed and Breakfast	CUP <sup>b</sup>	CUP⁵	N	N	CUP	CUP⁵	CUP⁵	N	
Building Materials Stores and Yards	N	N	N	N	N	N	N	N	
Business Support Services	Р	Р	Р	N	N	Р	Р	N	
Drive-in/Drive-through sales/services	N	N	CUP	N	N	N	N	N	
Equipment Sales and Rental	N	N	CUP	N	N	N	N	N	
Home Improvement Supplies	CUP°	CUP	CUP	N	N	CUP°	CUP	N	
Hotels/Motels	CUP	CUP	CUP	N	N	CUP	CUP	N	
Kennels	N	Р	CUP	N	N	N	CUP	N	
Medical Services - General	Р	Р	Р	N	N	Р	Р	N	
Medical Services – Hospital	N	Р	Р	Р	N	N	Р	Р	
Office - Business and Professional	Р	Р	Р	Р	N	Р	Р	Р	
Personal Services	Р	Р	Р	Р	CUP	Р	Р	Р	
Personal Services - Restricted	CUP	CUP	CUP	N	N	CUP	CUP	N	
Restaurants	Р	Р	Р	N	N	Р	Р	N	
Retail	Р	Р	Р	N	N	Р	Р	N	
Veterinary Facility	CUP	Р	Р	N	N	CUP	Р	N	

		Service Sub-Area			Mixed-Use Sub-Area			
Land Use Classifications	RMU	CMU	ОРМИ	PQI	HDR	RMU	CMU	PQI
Automotive and Vehicle Use Listings								
Auto Parts Sales	CUP	Р	Р	N	N	CUP	Р	N
Car Wash and Detailing	N	N	CUP	N	N	N	N	N
Service Stations	N	N	CUP	N	N	N	CUP <sup>1</sup>	N
Vehicle Services	N	CUP	CUP	CUP	N	N	CUP	CUP
Industrial, Manufacturing, and Processing	Use Listings							
Manufacturing	N	N	N	N	N	N	N	N
Cottage Industry	CUP	CUP	CUP	N	N	CUP	CUP	N
Personal Storage Facility	N	N	N	N	N	N	N	N
Printing and Publishing	CUP	Р	Р	N	N	CUP	Р	N
Recycling Facility – Collection	Р	Р	Р	Р	Р	Р	Р	Р
Recycling Facility – Processing	N	N	N	N	N	N	N	N

<sup>&</sup>lt;sup>1</sup> The service station must be accompanied with a larger retail establishment with a minimum of 50,000 square feet

#### Footnotes:

a – Home occupations must have no external evidence of business activity, (e.g. signs, noise, odor, vibration, etc.) or reduce available parking. No customers or employees are allowed at the home and no advertising which gives the home address is allowed.

b - Not allowed on first floor at street frontage

c – Square footage not to exceed 10,000 Square feet.

d - Not permitted if the same use is preexisting and within 300'

e – See Multi-Family Residential

#### **DEVELOPMENT STANDARDS**

## A. Building Height Requirements

The maximum height requirements for Appian Way are intended to emphasize the commercial nature while highlighting its character as the Regional Gateway into Pinole.

**Table 6.15** displays height requirements for each sub-area in Appian Way

Table 6.15
Height Requirements

Maximum Haight	Corridor	Service	Mixed-Use
Maximum Height	N/A	75' or 6 stories*	35' or 4 stories

<sup>\*</sup> See Special Height Requirements:

## **B. Building Height Exceptions**

- Minor projections. Minor projections for the purpose of shelter for mechanical equipment, elevator and mechanical equipment enclosures, radio or television antennas, and similar structures and necessary mechanical appurtenances may be erected on a building to exceed the height limits of the applicable Zoning District, or of that use, by a maximum of 15 feet, provided the projections are screened by a parapet or pitched roof or other method acceptable to the Community Development Director.
- 2) Architectural features. Clock towers, cupolas, towers, and similar structures may exceed the height limit as listed below. Signs shall not be included within the additional height allowed.
  - i) Up to 20 feet, if located at a street intersection.
  - ii) Up to 12 feet, it located midblock. These features shall not exceed a width of 25 feet or one-third of the length of the building façade, whichever is less.

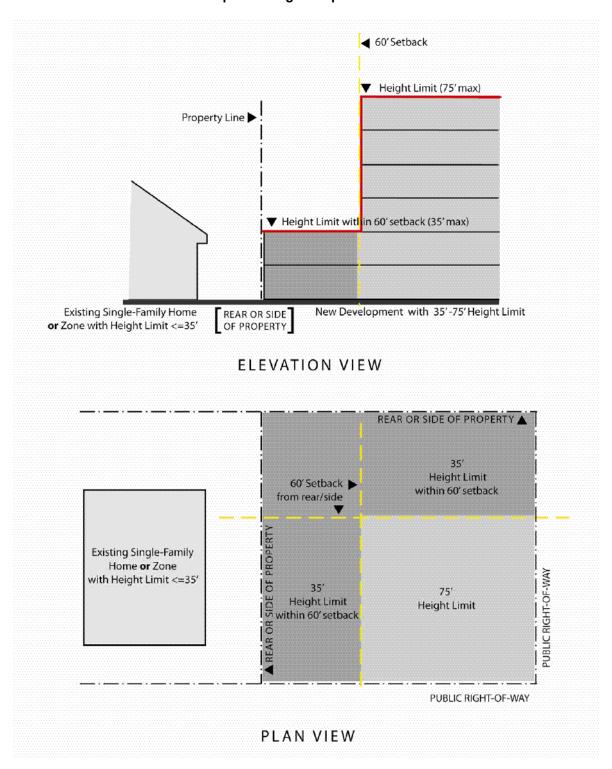
## C. Special Height Requirements

To create an appropriate relationship between new development within the Specific Plan Area and existing single family buildings or existing zones with lower allowable heights, building heights shall be limited to 35' in height where the development is located within 60' of a parcel (either within or outside of the Specific Plan Boundary) with an existing single family home or zoned with a height limit of 35' or less (Refer to **Figure 6.11** Special Height Requirements). Actual building heights may be

reduced through the development review process in order to preserve compatibility with adjoining land uses and respond to the unique topographic conditions of individual properties in order to:

- 1) Preserve solar access for neighboring sites;
- 2) Ensure adequate building transitions between properties that have lower height limits; and
- 3) Preserve views and scenic qualities in the project vicinity.

Figure 6.11
Special Height Requirements



## D. Building Placement

The setbacks and "Build-To" lines for Appian Way Road are intended to enhance social interactions between the uses while simultaneously providing appropriate levels of privacy in residential areas.

Table 6.16 displays setback requirements for each sub-area in Appian Way

Table 6.16 Setback Requirements

<b>Building Placement</b>	Corridor	Service	Mixed-Use
"Build To" Front Line*		Minimum: 0' Maximum: 5'	Minimum: 0' Maximum: 10'
Minimum Side Yard	No Minimum Required	0' except when adjacent to residential uses. When abutting existing residential, minimum setback is 20'.	0' except when adjacent to residential uses. When abutting existing residential, minimum setback is 20'.
Minimum Rear Yard		0' except when adjacent to residential uses. When abutting existing residential, minimum setback is 20'.	0' except when adjacent to residential uses. When abutting existing residential, minimum setback is 20'.

## E. Allowed Frontage Types

Frontage type refers to the architectural composition of the front façade of a building; particularly concerning how it relates and ties into the surrounding public realm. The Appian Way frontage types are intended to enhance social interactions and simulate interest while simultaneously providing appropriate levels of privacy in residential areas. Allowed frontage types in the different sub-districts districts are listed in **Table 6.17** and defined below. An "X" means that the frontage type is allowed; a blank cell means that the frontage type is not allowed.

Table 6.17 Frontage Types

Frontage Type	Corridor	Service	Mixed-Use
Arcade		X	X
Gallery		Х	X
Shopfront & Awning		X	X
Stoop			X
Dooryard/Terrace			X
Porch			X
Minimum Frontage Requirement	N/A	30%	60%

## F. Allowable Building Types

The following building types are intended to provide a variety of flexible building styles appropriate for the commercial nature of Appian Way that can be used to guide future development. Allowed building types in the different sub-districts are listed in **Table 6.18** and defined below. An "X" means that the building type is allowed; a blank cell means that the building type is not allowed.

Table 6.18 Building Types

Allowed Building Type	Corridor	Service	Mixed-Use
Podium	X	Х	X
Full Block Liner		X	
Half Block Liner	X	X	X
Quarter Block/ Infill Housing	X	X	X
Stacked		X	X
Terraced	X	X	X
Carriage House	Χ		X
Du/Tri/Quadplex	Χ	X	X
Multi-Family Faux House	X		
Rowhouse	X	X	X
Courtyard Housing			X

## G. Allowable Parking Types

The following parking types are intended to provide a variety of flexible parking styles appropriate for Appian Way that can be used to guide and promote future development. Allowed parking types in the different sub-districts are listed in **Table 6.19** and defined below. An "X" means that the building type is allowed; a blank cell means that the building type is not allowed.

Table 6.19 Parking Types

Allowed Parking Type	Corridor	Service	Mixed-Use
Parking Structure		X*	X*
Behind Structure		Х	Х
Next to Structure		Х	Х
Alley Access		Х	Х
Subterranean		Х	Х
Tucked-under Structure		Х	Х

<sup>\* -</sup> lined with ground floor retail

7.0 Pr	ivate Realm Design Guidelines	
7.1 Int	Introduction and Purpose	
7.2 Int	2 Interpretation of Terms	
7.3 Ge	General Design Guidelines Applicable to All Specific Plan Areas	
7.4 OI	d Town Sub-Area Design Guidelines	7.0-16
7.5 Se	.5 Service Sub-Area Design Guidelines	
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Table 7.1	Maximum Sign Size	7.0-30
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Figure 7.2	Types of Signs	7.0-26

## 7.1 INTRODUCTION AND PURPOSE

This chapter provides design direction for private property within the San Pablo Avenue, Pinole Valley Road, and Appian Way areas and will guide new development and reuse of existing buildings.

General guidelines and standards that address design goals that are common to all three Specific Plan Areas – San Pablo Avenue, Pinole Valley Road, and Appian Way – are included in section 7.3 of this chapter. Guidelines and standards establish criteria used by the City in reviewing proposed development and are intended to encourage high-quality design and innovative development in the three areas. Standards and design guidelines uniquely tailored to address specific design issues in one of the five Specific Plan sub-areas are included in sections 7.4, 7.5, 7.6, 7.7, and 7.8 respectively.

The purpose of the design guidelines and standards is to guide improvements, renovations, and future development in the San Pablo Avenue, Pinole Valley Road, and Appian Way areas to be consistent with the vision and urban design principles developed for the area as described in Chapter 2 of this document. These guidelines and standards describe and illustrate building and landscape designs that are appropriate for transforming Pinole into an economically vibrant and multi-modal destination. The design guidelines and standards are intended to strengthen Pinole's sense of place by promoting high-quality building design, active and landscaped outdoor spaces, and safe and convenient circulation while preserving the historic character of Old Town.

This section also incorporates principles of Crime Prevention Through Environmental Design (CPTED) and "Sustainability". The intent of these principles is to create healthy, balanced, and safe districts. CPTED is primarily concerned with promoting safety by providing natural surveillance ("eyes on the street"), controlling and directing access to permitted areas and deterring access to unauthorized or inappopriate areas, and creating a sense of ownership through regular maintenance and a clear delineation between private and public space. Sustainability balances environmental, economic, and social impacts of new development to enable communities to meet current and future needs. Sustainability relates to the private realm in many ways, such as providing gathering spaces in multi-family housing projects to foster a sense of community, using recycled building materials, and using drought-tolerant landscaping to conserve water resources. These issues are important to the City of Pinole and are integrated into the design guidelines and standards outlined in this chapter.

Applicable sections are divided into the following specific sub-sections as applicable:

1. Site Planning and Design

6. Parking

2. Site Amenities

7. Service and Storage

3. Architecture

8. Lighting

4. Landscape and Hardscape

9. Signage

5. Circulation

10. Green Design

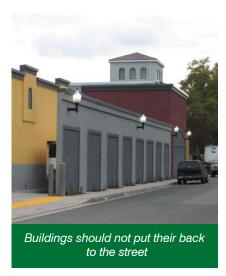
## 7.2 INTERPRETATION OF TERMS

It is important to note that the following sections contain both interpretive design guidelines delineated by the words "encouraged" or "may," as well as **mandatory design standards which are delineated in bold** and contain the words "shall," "must," or "will." Where the term "should" is used in the document, it is a guideline that is strongly encouraged.

# 7.3 GENERAL DESIGN GUIDELINES APPLICABLE TO ALL SPECIFIC PLAN AREAS

#### 1. SITE PLANNING AND DESIGN

- a. Site design shall be pedestrian-oriented, not dominated by parking lots, automobile use, or unattractive utilities.
- b. Buildings should be arranged to define and connect pedestrian paths and public spaces.
- c. Buildings and building entrances should be oriented toward the street to enhance street frontage, safety, and social interaction.
- d. Buildings should be situated to face plazas and other public spaces to increase natural surveillance and enhance a sense of safety.



- e. Projects on the corners of prominent intersections should be treated as community gateways and should be of the highest design quality.
  - Gas station canopies, fast-food restaurants with drivethroughs, light industries, and similar uses should be located away from the corner.
  - ii. Corners should be defined with appropriate architectural treatment.



- f. Consideration should be given to the privacy concerns of adjoining properties and buildings when orienting structures on a site. Building, window, and balcony orientation shall maximize views while preserving the privacy of surrounding neighbors.
- g. Locate buildings and windows to maximize visibility of entryways, pathways, and parking lots to promote natural surveillance and enhanced security.
- h. Commercial buildings should be arranged to provide convenient access to transit stops.
- i. Sidewalks and on-site pedestrian paths shall provide direct and safe access to building entrances and transit stops.
- j. Sites should be strategically planned to minimize potential nuisances from noise and odor producing activities by locating trash enclosures, loading areas, mechanical equipment, and restaurant vents away from residential uses. To minimize conflict between adjoining land uses, the following guidelines are suggested:
  - i. New commercial uses should be required to install adequate buffers (hardscape and/or landscape) along any boundary shared with a property zoned for residential use.
  - ii. If an alley is located between the commercial and residential uses, the commercial applicant should be required to provide an adequate buffer on the residential side of the alley, subject to the agreement of the residential property owner.
  - iii. Trash enclosures shall be included in all projects. All trash enclosures shall be designed consistent with the following:

- 1. Trash enclosures should be built of non-combustible materials (wood not permitted).
- 2. Materials/finishes should relate to and be of same quality as materials used on building.
- 3. Landscaping and screening should be included to help visually buffer loading area and enclosure, but avoid providing opportunities to hide.
- iv. Mechanical Equipment and utilities should be sited and designed to minimize noise conflicts with surrounding uses, and to be screened by buildings and/or landscaping from public lands and adjoining land uses.
- v. Rooftops that are visible from public vantage points should be designed similar to other building elevations with appropriate consideration given to color, materials and form, and with adequate screening for mechanical equipment.
- n. Building coverage may be affected by site constraints including, but not limited to, native trees, topography, easements, or other natural or physical site constraints.
- o. Natural topography should be integrated into site design to the extent feasible and grading should be minimized.

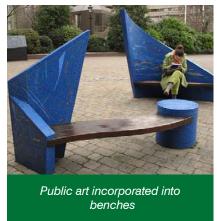
#### 2. SITE AMENITIES

- a. Plazas, Courtyards, and "Outdoor Rooms" serve as a gathering space for the community and contribute to the pedestrian experience. Development should be designed to:
  - i. Recognize the importance of spaces between buildings as "outdoor rooms" on the site.

    These spaces should be utilized as active open space in the form of covered pedestrian walkways, outdoor

seating areas, or pedestrian plazas.

 Incorporate outdoor activity spaces for sitting, relaxing in sun or shade, eating, strolling, and gathering within commercial, office, high-density residential, and mixeduse projects.



- iii. Encourage pedestrian use of "outdoor rooms" by incorporating shelters, trellises, fountains, art, seating, and shade trees.
- iv. Provide amenities for varying light and climate conditions and for protection from sun and wind.
- v. Ensure that outdoor spaces and plazas are visible from the street and/or adjoining buildings to help promote site safety.
- b. Paving and furniture should complement public streetscape elements when appropriate.
- c. Bicycle parking shall be provided at all multi-family and commercial projects at the following levels:
  - i. Multifamily Residential: Provide at least one accessible, indoor, secure bicycle storage space per occupant for 30% of the planned occupancy but no fewer than one per unit. Provide secure visitor bicycle racks on-site, with at least one bicycle space per 10 dwelling units but no fewer than four spaces per project site.
  - ii. Retail: Provide at least one accessible, indoor, secure bicycle storage space per retail worker for 10% of retail worker planned occupancy. Fractional spaces greater than 0.01 shall be rounded up to ensure sufficient workforce bicycle parking. Provide secure visitor/customer bicycle racks on-site, with at least one bicycle space per 5,000 square feet

of retail space, but no fewer than one bicycle space per business or four bicycle spaces per project site, whichever is greater.

- iii Commercial Non-Retail: Provide at least one accessible, indoor, secure bicycle storage space per occupant for 10% of planned occupancy. Provide secure visitor bicycle racks on-site with at least one bicycle space per 10,000 square feet of commercial non-retail space but not fewer than four bicycle spaces per building.
- d. Site amenities should be low-maintenance, graffiti-resistant, and made of high-quality, long-lasting materials.



e. Public artwork should be integrated into private development projects. In addition to typical art projects that include water features, murals, and sculpture, public art may incorporate, or be incorporated into, public infrastructure (e.g. bike racks, gates, benches, and bollards).

#### 3. ARCHITECTURE

## **Design & Character**

- a. Development should employ a consistent design concept. Building design should be consistent with the defined architectural style and expressed on all exterior elevations of the building.
- b. Projects shall reflect the context within which they occur, but should be original in design and avoid duplication. Developments should have their own identity while complementing the character of the Specific Plan Area.
- d. Corporate tenants shall design their buildings to fit the desired scale and character of the Specific Plan Area. The use of corporate chain or franchise architecture is not allowed in the Specific Plan Area unless the design is consistent with the desired scale and character of the Specific Plan Area.
- e. Renovation of existing buildings with historic value shall preserve those aspects of the site and structure that contribute to the historic character and context.

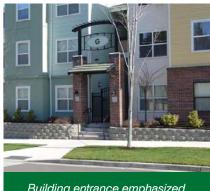


This figure illustrates how corporate design can be created at a pedestrian scale.

## Form, Massing & Articulation

- a. Varied and well-articulated wall planes, rooflines, and building form shall be created to reduce the overall massing and scale of buildings while providing visually engaging designs.
- b. Buildings should be well-articulated to foster a more intimate, pedestrian-friendly scale.
- c. Long, flat, blank walls shall be avoided, particularly when facing a street, public open space, pedestrian walkway, or parking area. Architectural details such as fenestrations, recessed planes, display cases, murals, and trellises can be used to soften blank walls.
- d. The number of windows facing streets, pedestrian walkways, plazas, and other public spaces should be maximized.

- e. Awnings, colonnades, arcades, and canopies of durable quality are encouraged to provide shadow lines, articulate flat façades, further enhance recessed entries, and provide protection from the weather.
- f. New buildings should be in proportion to surrounding structures, and should maintain a pedestrian scale along the street. Elements should be incorporated into the design of structures to soften the edge of the building and provide a transition to human scale, such as covered walkways, building arcades, trellises, and landscaping.
- g. Building entries should be prominent and visible. Entries should be emphasized through building articulation and form and architectural details such as windows, trellises, arcades, changes in material, and landscaping.



Building entrance emphasized

h. Residential buildings shall have main entrances from the street to facilitate pedestrian activity and increase security through more "eyes on the street."

#### **Materials and Color**

- a. Building materials finishes and use of color should be consistent with the overall architectural style of the project.
- b. The use of durable, low-maintenance, and high-quality building materials are encouraged.
- c. Variation in color and materials is encouraged to enhance the visual quality of building façades. Building elevations facing public areas should be given particular emphasis.



and buffer walkways from building.

#### 4. LANDSCAPE HARDSCAPE AND FENCING

## Landscape

- a. Landscaping should be used extensively to achieve a number of objectives including:
  - Adding texture to walls and other vertical surfaces;
  - ii. Strengthening the pedestrian scale;
  - iii. Buffering pedestrian walkways from the street and buildings;
  - iv. Providing shade in public spaces and parking lots;
  - v. Complementing the architecture;
  - vi. Relieving the visual appearance of large expanses of hard surfaces;
  - vii. Screening undesirable views;
  - viii. Providing a visual and noise buffer;
  - ix. Minimizing the impact of incompatible land uses; and
  - x. Defining perimeters and establishing transition between adjacent developments.
- b. Accent plantings should be used at key activity areas and entries to provide focus and visual interest.
- Evergreen and deciduous or flowering trees should be used in combination to create visual interest and a dynamic landscape.



Use of drought-tolerant landscaping

- d. Native plants or compatible species of drought-tolerant plants should be used in landscape design to reduce water consumption.
  - i. Turf should be limited to accent areas, activity areas, or in parkway areas between sidewalks and street curbs.

- ii. Plants should be grouped according to their water needs and irrigated separately from other groupings with dissimilar water needs.
- e. Plant selection should be based on the climate and environment of the area as well as site characteristics such as exposure, light intensity, soil analysis, site drainage, and irrigation.
- f. Existing trees in good health and appearance should be preserved.
- g. Landscaping shall be well maintained. Dead plant material shall be removed promptly and replaced in a timely fashion to preserve the well-kept appearance of the property.
- h. Parking areas should be shaded by trees to the greatest extent possible.
- i. Open space, parking areas, and building entries should not be obscured by landscaping. Planting should maintain open and clear sightlines to eliminate places of concealment.

## **Hardscape**

- a. Project should employ special paving to enhance a project's design. The use of brick, interlocking pavers, grasscrete, and cobblestones as an alternative to a solid paved driveway, parking lot, plaza, pedestrian walkway, or primary building entrance is encouraged.
- b. Hardscape materials used in pedestrian-oriented spaces such as plazas, paths, and sidewalks should be attractive, durable, easy to maintain, slip-resistant, of high quality, and compatible in color and pattern with a project's design. Surfaces in pedestrian circulation areas shall be constructed from materials that provide a hard, stable surface and that permit maneuverability for people of all abilities.



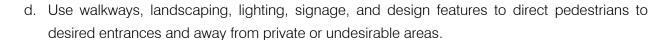
c. Pedestrian pathways crossing an on-site vehicle drive aisle, loading area, or parking area should be made identifiable by the use of an alternative hardscape material such as pavers, patterned, or stamped or colored concrete.

## **Fencing**

- a. The use of fencing around the perimeter of developments is discouraged. Where fencing may be necessary, it should be used in combination with landscaping and berming. Front fences should be predominantly open in design, such as picket or wrought iron fencing.
- b. Fencing colors and materials shall be compatible with the design of the project. The incorporation of pilasters and caps into wall and fence design is also encouraged.
- c. Encourage the use of high-quality materials in the construction of fencing to decrease long-term maintenance costs and to make it less likely that neglected, unsightly areas will occur.
- d. The use of chain link, fabric, or concrete block fencing is prohibited.

#### 5. CIRCULATION

- a. Circulation systems shall be designed to avoid conflicts between vehicular, bicycle, and pedestrian traffic.
- b. Site circulation shall allow for and facilitate emergency access to the site and all buildings.
- c. Projects should include convenient and safe pedestrian connections to the surrounding network of paths and public rights-of-way.



- e. Blank wall space adjacent to pedestrian circulation paths should be articulated and softened by landscaping, covered walkways, and/or articulated wall planes.
- f. Provide buffers between pedestrians and vehicular traffic.
- g. Access by individuals with disabilities shall be incorporated into the overall pedestrian circulation system.
- h. Bicycle racks shall be located in a well-lit, secure, and highly visible location, near the primary entrance(s) to the development, and shall not obstruct the designated pedestrian walkways.



Pedestrian paths to connect areas

- i. Increase the convenience of transit use by incorporating the following site design techniques:
  - i. Place transit facilities and supportive commercial uses (coffee or news stand) in close proximity whenever possible;
  - ii. Where possible, incorporate transit stops into attractive public spaces that act as a node between the project and the stop;
  - iii. Provide easy access from transit stops to the front door of buildings.
- j. Pedestrian and vehicular entrances must be clearly identified and easily accessible to create a sense of arrival. The use of enhanced paving, landscaping, signage, and special architectural features and details is strongly encouraged.
- k. Reciprocal access between non-residential developments is strongly encouraged to reduce the number of driveway curb cuts and provide for convenience, safety, and efficient circulation.

#### **Alleys**

- a. Alleys should be integrated into the circulation network whenever possible. Alleys function as a semi-public space, providing parking, service access, and occasionally even pedestrian access for the businesses and residences along them. When alleys are to be used as pedestrian access, provide clear signage and adequate lighting to direct pedestrians to building entrances and avoid creating hiding places.
- b. When utilized, pedestrian alleys should be spaced at not more than 100 feet on center.

#### 6. PARKING

- a. Development should allow for on-street parking in front of buildings and provide meaningful pedestrian areas in front of shops and businesses, and off-street parking should be consolidated behind, next to, under, or within the building.
- b. Parking should be located behind, next to, or tucked under buildings, subterranean, alley access, or in parking structures. When off-street parking in the rear is not possible, parking should be screened from view by landscaped berms and/or low walls.



Clearly designated pedestrian corridors that provide landscaping and shade

- c. Parking areas shall be clearly visible from the building or street.
- d. Large masses of parking shall be avoided. It is preferable to create small, connected parking lots utilizing shared driveways and located on side streets or alleyways.
- e. Shared parking and reciprocal access is strongly encouraged between adjacent developments and businesses.
- f. Pedestrian drop-off locations should be incorporated within overall parking lot circulation patterns and should be convenient and safe.
- g. Create wide, well-lit pedestrian walkways connecting on-site pedestrian circulation systems in parking lots to off-site public sidewalks and building entries.
- h. Designated pedestrian corridors in parking lots should have landscaping on at least one side of the walkway or alternating from one side to the other to provide a comfortable walking environment, including shade for pedestrians.
- The layout of parking areas should be designed so that pedestrians walk parallel to moving cars.

#### 7. SERVICE AND STORAGE

- a. Mechanical equipment, utilities and storage, including garbage collection, recycling, fire, and meters, shall be adequately planned and screened.
  - i. Outdoor storage shall be screened from public view through a combination of building design, landscaping, berming, walls, and/or location.
  - ii. Wall screening shall be of similar colors, materials, and finishes as primary buildings.
  - iii. Services should be visible and loading areas should be designed to avoid creating places to hide.



b. All refuse containers shall be placed within screened storage areas or enclosures.

- Refuse containers should be sufficiently buffered from project entries, main building entries, and main pedestrian paths.
- ii. Recycling containers shall be incorporated into trash enclosure areas or stored within a separate enclosure.
- iii. Enclosure materials and colors should be consistent with, and complementary to, building materials and finishes. A minimum 3-foot landscape buffer should be provided on all non-accessible sides of trash enclosures. A larger buffer area will be required when adjacent to single-family residential areas.
- c. Chain-link gates or fences are not appropriate for screening and shall not be permitted in the Specific Plan Areas.
- d. Sheet metal vents, pipe stacks, and flashing shall be painted to match the adjacent roof or wall material.
- e. Vents, antennas, stacks, and other roof-mounted equipment shall be completely screened from public view.

#### 8. LIGHTING

- a. Lighting shall provide security and visual interest.
- b. Exterior light fixtures should be attractively designed to complement the architecture of the project.
- c. Light poles shall be scaled for the pedestrian.
- d. Wall-mounted lights should be used to the greatest extent possible to minimize the total number of freestanding light standards.
- e. Exterior light fixtures used for pedestrians and vehicles shall direct illumination downward. Exposed bulbs are prohibited.





- f. When security lighting is necessary, it shall be recessed, hooded, and located to illuminate only the intended area. Off-site glare and light trespass shall be prevented.
- g. Pedestrian areas, sidewalks, plazas, parking areas, and building entrances shall be adequately lit to improve public safety and security in these areas.
- h. Parking lot lighting shall be designed to have a minimal effect on surrounding properties and buildings. Lighting shall be directed downward to minimize glare, and light intensity shall provide adequate visibility and security at night.
- i. Light fixtures shall be routinely maintained and bulbs replaced as needed.

#### 9. SIGNAGE

All signs must comply with the regulations contained in the City of Pinole Zoning Code (PMC Title 17 Chapter 17.28), unless as indicated in the Specific Plan, in which case the Specific Plan will take precedence.

- a. Design signs in harmony with the style and character of the development and as an integral design component of the building architecture. Signs shall coordinate with building design, colors, materials, and scale. Signs shall not dominate the building façade.
- b. Signage shall not obstruct or cover up architectural details or windows.
- c. Sign letters and materials should be professionally designed and fabricated.
- d. All signs shall be maintained in good repair, including the display surface, which shall be kept neatly painted or posted.
- e. Pole signs are not allowed.
- f. A-frame signs are not allowed anywhere with the exception of Old Town.
- g. Signage for businesses that have ceased operation for 90 days shall be removed.

#### 10. GREEN DESIGN

- Surface water and pollutant runoff should be reduced by maximizing the use of pervious surfaces and vegetative ground cover.
  - i. Use of permeable paving, pavers, turf stone, brick, and decomposed granite is encouraged.
  - ii. Use of natural topographic features or built swales for filtration of site drainage is encouraged
- b. Use of recycled water is encouraged for landscaping.
- c. Orient the design of the site to take advantage of solar access. For optimum solar conditions, the longest side of a building should face south (within 20 degrees of due south) and have relatively clear access to the sun.



- d. Building windows above the second floor and facing southern and westerly directions should be treated or otherwise designed to increase energy efficiency for the building while still maintaining the architectural integrity of the building and quality design of the site.
- e. The use of solar arrays or other types of solar-based energy generation is encouraged in all new roofing structures. Consider the pitch of roofs and orientation of the building when designing the project so as to maximize solar energy generation.
- Consider the use of green roofs or other innovative methods of reducing impervious areas and heat islands on project sites.



Use of solar panels on roofs

g. Building and paving materials that are less hazardous and/or are made from recycled materials are encouraged.

h. Encourage the design, construction, or retrofit of buildings to utilize green building practices. Consider using LEED, LEEP, or similar standards and thresholds to improve overall site and building quality in terms of energy efficiency and renewable resources.

## 7.4 OLD TOWN SUB-AREA DESIGN GUIDELINES

#### 1. SITE DESIGN

- a. On corner properties, orienting and locating the building at the minimum setback is required.
- b. Corner buildings shall have angled corners or corner walk-throughs to provide drivers visibility when turning right at the intersection.
- c. Pedestrian linkages should be provided to nearby neighborhoods, parks, and activity areas in the vicinity.
- d. Structures that are distinctive because of their age, cultural significance, or unique architectural style should be preserved and incorporated into development proposals.
- e. Link compatible residential and non-residential uses by utilizing access roads, walkways, common landscape areas, building orientation, and unfenced property lines.
- f. Loading areas, access, and circulation driveways, trash and storage areas, and rooftop equipment should be located at the rear or side of buildings and screened from public view.
- g. Employ landscaping to screen parking lots from adjacent residential uses and streets.
- h. Corner buildings should have a strong tie to the front setback lines of each street. Angles building corners or open plazas are encouraged at corner locations.
- i. Site amenities should be preserved and should become part of the new project.
- j. Development shall conform to the Old Town Design Guidelines maintained by the City as periodically updated.

#### 2. SITE AMENITIES

## **Outdoor Seating**

Outdoor seating animates the Old Town environment and complements retail and cultural activities.

- a. Outdoor sidewalk seating for an adjacent commercial use is permitted on limited portions of the public sidewalk within Old Town subject to compliance with the following application procedures and performance standards.
  - i. Application Procedures
    - A completed Conditional Use Permit application, a scaled site plan, and a scaled elevation drawing of the proposed outdoor dining facilities shall be submitted.
    - 2. Applicants seeking to permanently affix items within the sidewalk (e.g., signs, barriers) shall also apply for an encroachment permit.



- 3. The site plan shall show the following: (a) The location of tables, chairs, accessory service facilities, the mandatory perimeter barrier element, and any fixed elements on sidewalk within a 25-foot radius around the outdoor seating area. Fixed elements include light-sign poles, trees/landscape strips, traffic lights, parking meters, raised planter curbs, tree wells, pots, waste containers, and similar items. (b) Include the clear zone dimension from the outer perimeter of the outdoor fixtures to the nearest sidewalk fixtures pursuant to the requirements specified in Figure 7.1.
- 4. Elevation plans shall show front and side views of the outdoor dining facilities, including all proposed above-grade improvements such as planter boxes, awnings, tables, chairs, and similar features.
- 5. Upon approval of a Conditional Use Permit by the Planning Manager, the applicant shall obtain all required building/electrical permits, if applicable, prior to construction.

#### ii. Performance Standards

- 1. Sidewalk seating shall not obstruct sidewalk pedestrian traffic or create public health and safety hazards. Accordingly, all sidewalks shall maintain a 6-foot minimum clear dimension between the outside edge of any sidewalk café fixture and any fixed element within the sidewalk right-of-way. The 6-foot minimum clear dimension may be measured from the outermost point of the mandatory perimeter barrier element as a radius of a circle. The red dotted line in Figure 7.1 represents the 6-foot minimum clear dimension.
- 2. All applicants shall provide proof of insurance for the existing business and the applicant's ability to obtain a policy of general commercial liability insurance for the café use. A condition of approval shall be added to the Conditional Use Permit requiring the applicant to obtain such an insurance policy for the café and that they add the City of Pinole as an additional insured to the policy.
- 3. Outdoor sidewalk seating shall be either open, partially covered, or enclosed by means of umbrellas, awnings, canopies, or similar protective structures or fully enclosed.
- 4. Enclosed cafés shall be constructed of material that is predominantly transparent. Glass shall be clear, not heavily tinted or mirrored. Base walls of enclosed cafés shall not be greater than twelve (12) inches in height. The outside window height shall not be less than eight (8) feet. All permanent structures shall meet building code requirements.

Tree Light Pole Tree

6 feet

Outdoor Seating

Building

Figure 7.1

Minimum Dimensions for Outdoor Cafés

#### 3. ARCHITECTURE

- a. New construction shall be representative of, or compatible with, a specific historical architectural motif and style of the period represented in Old Town Pinole. Project architects shall provide a written explanation identifying their chosen motif and style, noting which architectural elements express that style, and explaining why it is compatible with existing historic development in Old Town.
- b. New construction shall relate to existing construction and development through a careful reflection of the architectural detailing of surrounding development, including features such as building height, materials, and proportion and placement of details such as entries, windows, arches, or wainscots.



New construction of medium/ high density residential development fits into context with surrounding historical structures

- c. Renovations of existing buildings shall preserve existing amenities.
- d. Franchise architecture or corporate prototype design is prohibited. Building elevations shall be designed to fit into the surrounding character of Old Town.
- e. Renovations of registered buildings shall comply with the National Historic Preservation guidelines and all other applicable guidelines.
- f. Parapets shall surround all low slope commercial roofs. The parapets shall be detailed to enhance the overall building design.
- g. The scale of new buildings in Old Town should blend with what currently exists.
- h. All ground-level uses should have a minimum height of 10 feet from floor to the major finish ceiling, excluding soffits.



This figure illustrates how corporate design can be created at a pedestrian scale.

i. Entries should be well defined, face directly to the main public street or street corner, and be well illuminated. Dark or very deep entry spaces are discouraged.

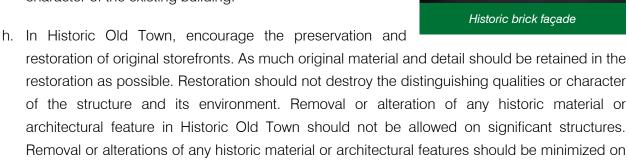
#### **Storefronts**

Storefronts create a seamless and historically sensitive transition from the pedestrian sidewalk space to the building interior; the following guidelines are applicable to storefronts.

- a. Dark-colored, reflective, or obscure glazing is prohibited.
- Blank walls (without doors or windows) longer than 40 percent of a façade or more than 50 feet occurring along sidewalks are prohibited.
- c. Historic prototype storefronts provide a decorative base panel in the wall below the window. Appropriate materials shall be durable commercial grade such as ceramic tile, brick, stone veneers, and wood paneling with moldings.
- d. Enhance the pedestrian experience by unifying storefront elements including signs, graphics, awnings, lighting, and color.



- e. Maximizing glazing at the ground level of new and existing buildings being remodeled is encouraged. Provide visual access to the building's primary activity and orient it to the consumer.
- f. Window size, shape, and style should be responsive to the building design. Traditional storefronts may have smaller windows with intermediate mullions.
- g. Existing storefront upgrades should respond to the building's intrinsic architectural qualities as well as those of the street façade. The design should be responsive to the character of the existing building.



#### **Materials and Color**

less significant structures.

- a. New construction details and materials should follow the pattern and principles of the historic architectural design.
- b. Buildings shall incorporate materials with at least two textures at streetfront elevations. Large uninterrupted and unarticulated monochromatic expanses are prohibited.
- d. Materials shall be durable, and low maintenance. Brick, iron, and other masonry-type materials are encouraged. When using these materials as a veneer, attention shall be paid to the corner treatment and similar details so that the materials do not appear too thin.



- e. Corrugated metal siding, reflective glass, plywood siding, and synthetic siding materials (plastics) are strongly discouraged unless they can be demonstrated attractively.
- f. Stucco-covered foam may be acceptable if properly detailed and applied. Special attention to durability is required at lower levels accessible to the pedestrian.
- g. Construction materials should be consistent and complement the existing buildings found along main streets. Site materials should complement materials as found on existing historic buildings. The use of brick, wood, and iron is encouraged.
- h. Paint colors shall be in keeping with the historic period and characteristics of the building or structure. The period of significance for buildings and structures in the Old Town is 1880 to 1940, and a variety of exterior paint colors are associated with this time period and the types of buildings and structures within Old Town. Therefore, the use of a variety of muted colors that
  - highlight the different periods of use and functions of the buildings and structures in Old Town while retaining its "Main Street" appearance shall be required. One muted color shall be used as a background to unify the overall appearance of the building or structure, while one or two other colors shall be used for accent to highlight architectural details and trim. Different shades of the same color shall be used for variation in color, while maintaining a color palette consistent with the historic period and characteristics of the building or structure.



The bottom floor of this structure is accentuated though the height of the first floor.

An integrated color palette for the entire project shall be submitted with the design.

#### 4. CIRCULATION

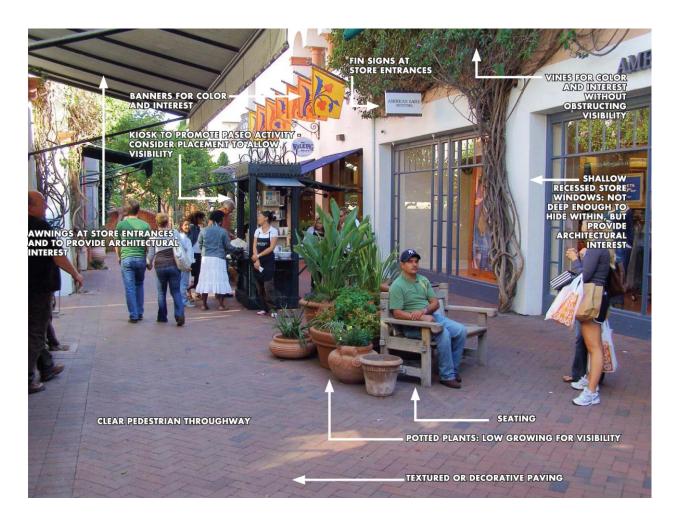
- a. Primary pedestrian access to all buildings shall be through an entry on the street side. Where buildings abut a court yard or plaza, pedestrian access shall be provided.
- b. Vehicle circulation patterns shall be as simple and obvious as possible.
- c. Pedestrian circulation patterns shall be as simple and obvious as possible.
- d. Circulation shall be designed to avoid all conflict between vehicles and people. The pedestrian shall take precedence over the vehicle if a conflict arises.

- e. Pedestrian-scaled lighting is required.
- f. Accessibility and safety (non-slip surfaces) shall be provided.
- g. Alleys function as a semi-public space, providing parking, service access, and occasionally even pedestrian access for the businesses and residences along them.

## **Alleys**

Alleys in Old Town shall comply with the following:

- a. Parcels adjacent to rear alleys shall maintain service access from the rear and provide attractive rear entrances.
- b. Projects adjacent to alleyways shall improve the appearance of the alleyways per current Public Works standards.
- c. Alley entrances should be designed to provide a sense of safety and welcome at both ends and to provide visual cues for pedestrians in the district that these are unique spaces.
- d. An entry gateway arch or similar feature to distinguish the pedestrian alley and reinforce the continuity of the streetwall is required.



e. Access width for pedestrian alleys (building to building or building to property line) shall not exceed 12 feet, unless a greater width is required by a public utility to provide service to the site.

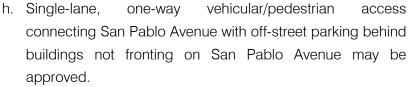


Archways at alley entrances



- f. Vehicle access to parking lots shall be from side streets and not from main thoroughfares. The Planning Commission may approve exceptions to this requirement for parcels without rear access through alleys or through an adjacent parcel through a recorded easement.
- g. When utilized, pedestrian alleys should be spaced at not more than 100 feet on center.







- i. Vehicle, bicycle, and pedestrian circulation should comply with the following:
  - i. Pedestrian paths should be separated from automobile circulation routes.
  - ii. Paving materials with color/texture/pattern should be an integral part of the design where in the right-of-way.
  - iii. Varied paving materials should occur at the overlap of pedestrian and vehicular areas.
  - iv. Pedestrian paths should include trees, lighting, overhead trellises, sitting areas, etc.
  - v. A designated well-lighted route should be provided from buildings to the parking area.

#### 5. SIGNAGE

Signage should be compatible in scale and design with the historic context of Old Town Pinole. The guidelines focus on the location, size, materials, and placement of signs but not on their content.

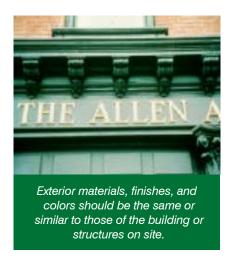
Second floor awning signs Dave Logan, LLP William Thomas, CPA Second floor Accounting Attorney window signs Wall sign XYZ Building Wall sign with →Anna's Homebaked channel letters Awning Sign Directory Sign Projecting Coffee Sandwiche Fresh Donut Window Sign

Figure 7.2 Types of Signs

- a. Signs shall be constructed from wood, material with a natural wood appearance, concrete, metal, screen print on canvas awnings, or painted graphics on building surfaces. Other materials may be approved by the Planning Manager on a case-by-case basis.
- b. Sign size shall not exceed the maximum dimensions defined in Table 7.1 below.
- c. Roof signs, pole signs, neon signs, and flat plastic back-lit signs are prohibited in Old Town.



- d. Commercial signs hand-painted on windows may not exceed 25 percent of the glazed area.
- e. Advertisements and displays in storefront windows should be attractive and should not completely obscure views in and out of shop windows.
- f. Signs not attached to the building may be placed temporarily on sidewalks or in front of buildings, which are either part of the public right-of-way or on private property, and are subject to a Temporary Sign Permit issued by the Planning Manager.
- g. A-frame signs are allowed in Old Town. A-frame or sandwich board signs shall maintain a minimum of 4 feet of clear sidewalk at all times. The sign shall be sufficiently weighted or anchored to prevent movement by wind or other elements. A-frame signs must not be permanently affixed to any object, structure, or the ground.





- h. Signs shall be illuminated by means of either external lighting with fixtures that complement the sign and shield pedestrians or by halo lighting which individually illuminates opaque letters from behind. Internal illumination of signs is prohibited in all circumstances.
- i. No flashing, moving, or animated illumination is permitted on signs in Old Town.
- j. Back-lit awnings are prohibited if they illuminate signage that is mounted or attached on the awning.
- k. All conductors, transformers, and other equipment necessary for signs shall be concealed. Mounting hardware for signs shall be made inconspicuous. Fasteners, bolts, and clips for signs shall be made of non-corroding and stain-free materials.
- Signs should relate proportionally to building elements, should be an integral component of design, and should complement the building façade.



Projecting signs should be oriented to pedestrians. Projecting light fixtures should be simple and unobtrusive in appearance. They should not obscure the graphics of the sign.



Light source must be directed against the sign such that it does not shine into adjacent property or cause glare for motorists and pedestrians.



m. Detached signs placed in front of buildings within the public right-of-way are permitted on limited portions of the public sidewalk within Old Town subject to compliance with the following application procedures:



Wall signs shall be designed to be compatible with the storefront in scale, proportions, and color.

Typically, wall signs should be centered on horizontal surfaces.



- i. A completed Temporary Sign Permit application, a scaled site plan, a scaled drawing of the proposed sign, and a description or sample of the sign materials shall be submitted to Community Development Department – Planning.
- ii. Applicants seeking to permanently affix items within the right-of-way (e.g., signs, banners) shall also apply for an encroachment permit through the City's Public Works Department.
- iii. The site plan shall show the following: (a) The location of the sign and any fixed elements on sidewalk or building within a 25-foot radius around the sign location. Fixed elements
  - include light-sign poles, trees/landscape strips, traffic lights, parking meters, raised planter curbs, tree wells, pots, waste containers, and similar items. (b) Include the clear zone dimension from the sign to the nearest sidewalk fixtures pursuant to the requirements specified below.
- iv. Permits are valid for six months.
- n. Signs and banners shall not obstruct sidewalk pedestrian traffic or create public health and safety hazards. Accordingly, all sidewalks shall maintain a 6-foot minimum clear dimension between any sign and any fixed element within the sidewalk right-of-way.





Sign message and letter style should be designed to be compatible with the building and location

Table 7.1 Maximum Sign Size

Permitted Sign Types	Maximum Area ¹	Additional Area Requirements	Maximum Height
Monument sign	Subject to approval of a use permit from the Planning Manager to ensure compatibility with setbacks  • Limited to <16 sq. ft., excluding architectural supporting elements • 1 monument sign allowed per parcel • Subject to Code visibility requirements		4 feet <sup>2</sup>
Temporary detached signs	Subject to approval of a Temporary Sign Permit from the Planning Manager	< 6 square feet	36 inches
Building signs			
Attached flat to building	Aggregate of all building signs limited to 1 square foot per foot of building frontage	NA	3 feet
Hanging from underside of canopy	Aggregate of all building signs limited to 1 square foot per foot of building frontage	See note <sup>5</sup>	See note <sup>5</sup>
Window sign	Aggregate of all building signs limited to 1 square foot per foot of building frontage	< 10% of glazing	12 inches
3-D signs (letters or logos) <sup>3</sup>	Aggregate of all building signs limited to 1 square foot per foot of building frontage	NA	24 inches
Project from building at right angle	Aggregate of all building signs limited to 1 square foot per foot of building frontage	< 25% of surface	Maximum height indicated is the height of sign structure and not its placement on a building.
Directional signs	Aggregate of all building signs limited to 1 square foot per foot of building frontage	NA	NA
Entrance signs <sup>4</sup>	Aggregate of all building signs limited to 1 square foot per foot of building frontage	1 sq. ft. per entrance	NA

<sup>1 –</sup> The maximum area is determined by the measurement to the outside of a simple boundary perimeter including any voids within such perimeter.

<sup>2 –</sup> The maximum height is measured as the vertical distance from the grade to the highest point of the sign.

<sup>3 –</sup> Permitted only if consistent with historic prototypes.

<sup>4 –</sup> Limited to hours, emergency number, and similar information.

<sup>5 –</sup> Limited to no more than 6 square feet, with a maximum height of 1.5 feet, provided adequate clearance is provided for pedestrian ingress/egress and mobility.

#### Murals

- a. Murals shall be consistent with the standards and guidelines contained in the Old Town sections pertaining to architecture, color, and materials, respectively. In no event are the standards applied from those or other Old Town sections to be the basis for denial or conditioning of a mural on the basis of content.
- b. Murals shall not use the words "stop," "look," "listen," or any other word, phrase, symbol, lights, motion, sound, fumes, mist, or other effluent or character in such manner as to interfere with, mislead, or confuse traffic.
- c. Prior to the commencement of a mural, the applicant shall obtain design review approval from the City's Planning Commission which will apply the standards and guidelines referenced above.
- d. Murals may use up to 50 percent of the area of any single building wall.
- e. The City encourages the portrayal of social, historic, or cultural themes that celebrate Pinole's past, present, and future. This guideline, however, shall not be used by the City as criteria for denying or conditioning any approval or application for a mural based upon its content.

#### 7.5 SERVICE SUB-AREA DESIGN GUIDELINES

#### 1. SITE PLANNING AND DESIGN

- a. Building entrances shall be clearly defined and easily accessible from public transit stops.
- b. A comfortable pedestrian environment shall be created through the design and scale of buildings and outdoor spaces.
- c. Whenever possible and appropriate, pedestrian linkages shall be provided to nearby neighborhoods.
- d. Garage doors and loading docks shall not face the street.
- e. Structures that are distinctive because of their age, cultural significance, or unique architectural style should be preserved and incorporated into development proposals.
- f. Loading areas, access, and circulation driveways, trash and storage areas, and rooftop equipment should be located at the rear or side of buildings and screened from public view.

#### 2. SITE AMENITIES

- a. Natural amenities such as views, mature trees, and similar features unique to a site should be preserved and incorporated into development proposals.
- b. Small open space areas should be grouped into larger, prominent public spaces. Hardscape and vegetation should be combined to create plazas that people can use for rest, recreation, congregating, or dining.



#### 3. ARCHITECTURE

- a. Long, unbroken, horizontal roof lines are discouraged. Roof lines shall not run for more than 50 feet without interruption.
- b. Any building with more than 125 feet of street frontage should have at least one primary building entry.
- c. Design features must be consistent on all elevations of the structure. Side and rear elevations should not be minimized because they are oriented away from public view.
- d. The size and location of doors and windows should relate to the scale and proportions of the overall structure.

#### 4. PARKING

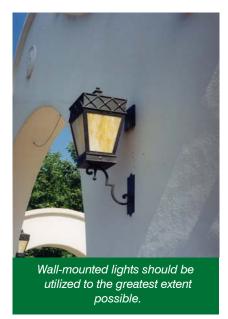
- a. When long expanses of fencing or walls are required, they shall be constructed with varying heights, offset, and architecturally treated to prevent monotony. Landscape pockets shall also be provided.
- b. Fencing and walls shall be set back a minimum of 3 feet from the sidewalk or the street to allow for planting.
- c. Parking lots should be designed with a clear hierarchy of circulation: major access drives with no parking; major circulation drives with little or no parking; and then parking aisles for direct access to parking spaces.
- d. Parking areas should be separated from buildings by a landscaped strip. Conditions where parking stalls directly abut buildings should never be permitted.

#### 5. SERVICE AND STORAGE

- a. Trash enclosures should allow convenient access for all commercial tenants. Siting service areas in a consolidated and controlled environment is encouraged.
- b. Trash enclosures should be located away from residential uses to minimize nuisance for the adjacent property owners. The enclosure doors should not interfere with landscaping or pedestrian or vehicle path of travel.

#### 6. LIGHTING

- a. Lighting should be designed to satisfy function as well as contribute to overall design quality.
- b. Wall-mounted lights should be utilized to the greatest extent possible to minimize the total number of freestanding light standards.
- c. Lighting should be used to accent on-site public art, specimen trees, and architectural features.
- d. Security lighting should be designed as part of a comprehensive lighting plan.
- e. Lighting should improve visual identification of residences and businesses.
- f. Pedestrian-scaled lighting for sidewalk and street illumination is encouraged.
- g. Overhead service wires or exposed conduit should be avoided.



#### 7.6 MIXED-USE SUB-AREA DESIGN GUIDELINES

#### 1. SITE PLANNING AND DESIGN

- a. Alleys or connecting parking lots shall be located along rear property lines when feasible. This provides a less visible location for trash storage and pickup, as well as parking.
- b. On corner properties, orienting and locating the building at the minimum setback is required.
- c. Corner buildings shall have angled corners or corner walkthroughs to provide drivers visibility when turning right at the intersection.



- d. Whenever possible and appropriate, pedestrian linkages shall be provided to nearby neighborhoods, parks, and activity areas in the vicinity.
- e. Structures that are distinctive because of their age, cultural significance, or unique architectural style should be preserved and incorporated into development proposals.
- f. Link compatible residential and non-residential uses by utilizing access roads, walkways, common landscape areas, building orientation, and unfenced property lines.
- g. Loading areas, access, and circulation driveways, trash and storage areas, and rooftop equipment should be located at the rear or side of buildings and screened from public view.
- h. Employ landscaping to screen parking lots from adjacent residential uses and streets.
- i. Corner buildings should have a strong tie to the front setback lines of each street. Angles building corners or open plazas are encouraged at corner locations.
- Site amenities should be preserved and should become part of the new project.

#### 2. SITE AMENITIES

## **Outdoor Seating**

Outdoor seating in the mixed-use sub-area is permitted. Please refer to regulations for outdoor seating in section 6.4.2.a.

#### 3. ARCHITECTURE

- a. Developments are encouraged to have their own identity; however, they must integrate with adjacent properties and provide functional and aesthetically pleasing pedestrian and vehicular circulation.
- b. The appearance of several well-articulated buildings rather than one large building is preferred to foster a more intimate, pedestrian-friendly scale.
- c. When an infill building is more than two stories taller than the surrounding existing buildings, its façade shall be broken down into small, proportioned components and bays, and upper floors stepped back from lower floors.
- d. Building materials and finishes shall be true to the structure's architectural style.
- e. Primary building entrances should be easily identified and provide a prominent sense of entry. The use of projections, towers, columns, change in roofline, entry lobbies, or other design elements is strongly encouraged.
- f. The size and location of doors and windows should relate to the scale and proportions of the overall structure.
- g. Clear windows should be provided at storefront locations.
- h. The physical design of façades should utilize such techniques as:
  - i. Break or articulation of the façade;
  - ii. Vertical and horizontal offsets to minimize large blank walls;
  - iii. Reduced building bulk;
  - iv. Significant changes in façade design;
  - v. Placement of window and door openings.

#### 4. CIRCULATION

- a. Loading areas for the buildings that face main corridors shall be located off such corridors when possible.
- b. Defined pedestrian walkways shall be provided throughout parking areas and the site to connect the users with public sidewalks and surrounding buildings.
- c. Developments should have shared entries when the lot is less than 75 feet wide.



d. When possible, use alleys or side streets for access to parking areas. The use of alleys for parking access must be balanced with other common uses of alleys, including service, utilities, and loading and unloading areas.

#### 5. PARKING

- a. Parking lots should be designed with a clear hierarchy of circulation: major access drives with no parking; major circulation drives with little or no parking;
  - and then parking aisles for direct access to parking spaces.
- b. Parking areas should be separated from buildings by a landscaped strip.
- c. Conditions where parking stalls directly abut buildings should never be permitted.



d. Bicycle parking should be provided at each development and should be easily accessible and integrated into the overall site design.

## 7.7 CORRIDOR SUB-AREA DESIGN GUIDELINES

The corridor sub-area is intended to identify lands within the Specific Plan Corridors that have limited adjoining development potential, but that are critical to the circulation and urban design objectives of the Community Plan. These sub-areas can be enhanced with circulation improvements (including bicycle lanes), landscaping, signs, public art, and other enhancements that contribute to the circulation, scenic, and civic value of the corridors.

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## 8.1 INTRODUCTION

Public spaces and buildings contribute to the unique character of a city. Public spaces include streetscapes, plazas, and parks. Public buildings include police stations, libraries, post offices, and civic buildings.

The public realm design guidelines and development standards will direct and coordinate the design of future streets, sidewalks, public open space, and key intersections, including street furniture, landscaping, entry treatment, public art, and other streetscape enhancements. The intent is to create a unified, safe, and visually attractive environment that strengthens Specific Plan goals and enhances the vitality of the three corridors. The condition of the public realm is important for creating the desired image and maintaining the character that is unique to Pinole. Well-designed and activated streetscapes and public gathering places in the community promote walkability, encourage social interaction, create a sense of safety, and foster community identity. The public realm gives structure to the districts and provides the framework for organizing individual developments into a cohesive whole. Public realm improvements serve to enhance an area's visual quality and act as an investment catalyst, encouraging private property upgrades and new development.

The design guidelines and standards are intended to promote the Specific Plan Urban Design Principles outlined in Chapter 2 that relate to the characteristics of the public realm. Those design principles are as follows:

- Encourage compatibility between residential and non-residential land uses through unifying public urban design features such as streetscape, signage, and landscape improvements.
- Develop standards and guidelines that support the vision to attract investment.
- Enhance pedestrian circulation and use improved safety mechanisms at appropriate locations and crossings to improve the walkability of the area.
- Strengthen bicycle connections.
- Explore opportunities to enhance auto circulation and reduce congestion at appropriate locations and crossings to improve access.
- Improve public transportation connections between key destinations.

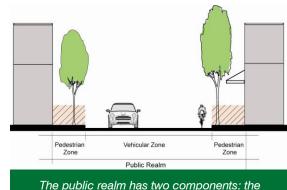
- Enhance connections to neighborhoods, the Bay, the creek, recreational opportunities, schools, shopping centers, and other key destinations.
- Encourage corridor landscaping, particularly along designated opportunity sites and prominent medians.
- Streetscape and landscape improvements will communicate a sense of place and arrival.
- Encourage consistency in street lighting, streetscape and landscape improvements. Consider long-term maintenance and appearance costs when choosing design patterns, streetscape furnishings, and fixtures.

The principles of sustainability and Crime Prevention Through Environmental Design (CPTED) are intended to create healthy, balanced, and safe neighborhoods, streets, and public spaces. CPTED is primarily concerned with promoting safety by providing natural surveillance ("eyes on the street"), controlling and directing access to permitted areas and deterring access to unauthorized or inappropriate access, and creating a sense of ownership through regular maintenance and a clear delineation between private and public space. Sustainability balances environmental, economic, and social impacts of new development to enable communities to meet current and future needs. Sustainability relates to the public realm in many ways, such as providing public gathering spaces to foster a sense of community, furnishing streets with recycling receptacles, and using drought-tolerant landscaping to conserve water resources.

The guidelines in this chapter are a planning tool for public projects that will guide project design and development.

This chapter is organized into the following sections:

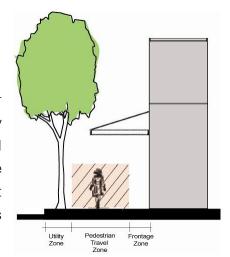
- Landscaping
- Sidewalks and Crosswalks
- Street Furniture
- Public Art



The public realm has two components: the Pedestrian Zone and the Vehicular Zone

- Signage
- Plazas and Public Outdoor Spaces

The public realm contains two distinct zones: the Vehicular Zone and the Pedestrian Zone. The Vehicular Zone generally includes the area between the curbs that is occupied by travel lanes, parking lanes, and medians. The Pedestrian Zone generally includes the outer portions of the right-of-way that flank the street, including sidewalks and any adjoining plazas and parks.



The Pedestrian Zone

The Pedestrian Zone is further divided into three zones: the pedestrian travel zone, the utility zone, and the frontage zone.

The pedestrian travel zone primarily accommodates pedestrian circulation. It needs to be wide enough to provide an unobstructed, linear sidewalk space that is free of street furniture, street trees, planters, and other vertical elements such as light poles and transit facilities. The utility zone generally is adjacent to the street and accommodates public facilities and street furnishings. This zone is the appropriate location for the majority of the amenities that enhance and serve the pedestrian zone, including landscaping, benches, streetlights, and other furnishings. The frontage



zone is adjacent to buildings and serves as a transition area between public and private space. The design of this zone should be responsive to and support the adjoining use, which, depending on context, may mean providing a clear zone for store entrances, a "slow" zone for retail displays and window shopping, or a furnished zone for outdoor dining. These zones are conceptual and therefore may be more clearly represented and delineated on some streets than on others.

Please note that this section contains both mandatory design standards which are delineated in bold and contain the words "shall," "must," or "will" and interpretive design guidelines delineated by the words "encouraged" or "may." Where the term "should" is used in the document, it is a guideline that is strongly encouraged.

#### 8.2 LANDSCAPING

#### 1. GENERAL LANDSCAPE GUIDELINES

Landscaping enhances the character and visual quality of the community by improving the pedestrian experience and unifying elements in the public realm.

- a. Landscaping shall be used extensively to achieve a number of objectives including:
  - Strengthening the pedestrian scale;
  - ii. Buffering pedestrian walkways from the street;
  - iii. Providing shade in public spaces and parking lots;
  - iv. Relieving the visual appearance of large expanses of hard surfaces;
  - v. Screening undesirable views; and
  - vi. Providing a visual and noise buffer.
- b. Accent plantings shall be used at key activity areas and entries to provide focus and visual interest.
- Evergreen and deciduous or flowering trees shall be used in combination to create visual interest and a dynamic landscape.
- d. Native plants or compatible species of drought-tolerant plants shall be used in landscape design to reduce water consumption.



. Turf should be limited to accent areas or activity areas or in parkway areas between sidewalks and street curbs.



Use of landscaping to add color, strengthen pedestrian scale, and relieve expanses of hard surfaces

- ii. Plants should be grouped according to their water needs and irrigated separately from other groupings with dissimilar water needs.
- e. Surface water and pollutant runoff shall be reduced by maximizing the use of pervious surfaces and vegetative ground cover.
- f. Use of recycled water is encouraged for landscaping.
- g. Plant selection should be based on the climate and environment of the area as well as site characteristics such as exposure, light intensity, soil analysis, site drainage, and irrigation.
- h. Existing trees in good health and appearance should be preserved.
- i. Landscaping shall be well maintained. Dead plant material shall be removed promptly and replaced in a timely fashion to preserve the well-kept appearance of the property.
- j. Parking lots should attain a minimum of 50 percent of tree canopy coverage within 10 years of completion of construction to provide shade and minimize visual and environmental impacts.
- k. Parking lots shall integrate landscaped swales, natural vegetation, and permeable paving to reduce surface water and pollutant runoff.
- I. Planting should maintain open and clear sightlines and not foster places of concealment.
- m. Visual surveillance of common open space and parking areas should not be obscured through landscaping.

#### 2. STREET TREES

Street trees are a key element in defining public space. They should be used to create unified street scenes, strengthen pedestrian scale, buffer pedestrian walkways from the street, soften hardscapes, and contribute to the character of the Specific Plan Areas. Street trees should be used extensively throughout the project areas to provide comfort, shade, protection, texture, and color.



- a. For each block on a street, no more than three species are recommended. Mixed species result in better long-term management because they are less prone to diseases and insects than use of a single species. Alternatively, too many species create a lack of visual unity along the street.
- b. Trees that provide attractive fall colors, seasonal flowers, or large amounts of shade are preferred for the Old Town sub-area.
- c. Tree grates are required within plazas and unlandscaped sidewalks as they allow for improved accessibility and increased sidewalk usability area, and they are consistent with the desired urban character.
- d. Street tree placement shall be carefully considered to avoid conflicts with adjacent business functions, and overhead power lines, utility lines, and structures. Trees should align with property lines and preserve views of storefront signage to the greatest extent possible.
- e. Street trees should be spaced approximately 30 to 50 feet on center.
- f. A program should be established for street tree maintenance and trimming of tree canopies and other plant materials, particularly around light poles.

#### 3. STREET TREE PALETTE

The Specific Plan street tree palette was selected to conform with the City of Pinole Parks Department Street Tree Planting Program and is applicable to Specific Plan Area planting along sidewalks and medians. When selecting street trees from the palette, consideration should be given to the following tree classifications:

- a. Primary street trees provide shade for pedestrians, soften and frame the street, and define the public space.
- b. Secondary trees complement and support the primary trees in form and function.
- c. Accent trees are used to define entrances, add variety in form and color, or highlight other focal points of the street.

Table 8.1 Street Tree Palette

Botanical Name	Common Name	Classification	Image
Aesculus x carnea 'Briotii'	Scarlett Horse Chestnut	Accent	
Ceratonia siliqua	Carob Tree	Primary	
Eucalyptus ficifolia	Red Flowering Gum	Accent	
Eucalyptus polyanthemos	Silver Dollar Gum	Primary	

Botanical Name	Common Name	Classification	Image
Eucalyptus sideroxylon "Rosea"	Red Iron Bark	Accent	
Ginko biloba	Maidenhair Tree	Secondary	
Koelreuteria paniculata	Goldenrain Tree	Primary	
Liquidamber styraciflua	Sweet Gum	Secondary	

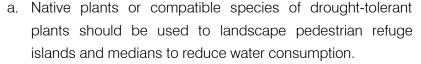
Botanical Name	Common Name	Classification	lmage
Liriodendron tulipifera	Tulip Tree	Accent	
Maytenus boaria	Mayten Tree	Primary	
Melaleuca leucadendra	Cajeput Tree	Primary	
Metrosideras excelsus	New Zealand Christmas Tree	Accent	
Quercus ilex	Holly Oak	Primary	

Botanical Name	Common Name	Classification	Image
Zelkova serrata	Sawleaf Zelkova	Secondary	

<sup>\*</sup>Myoporum laetum has been omitted from the Specific Plan Area Tree Palette. It is considered to be an invasive plant species to California by the California Invasive Plant Council and therefore does not meet the sustainable landscaping objectives of this Specific Plan.

### 4. LANDSCAPING IN PEDESTRIAN REFUGE ISLANDS AND MEDIANS

Pedestrian refuge islands and medians provide a visual separation between oncoming traffic, create a narrowed lane perception that slows traffic, provide protected places for pedestrians while crossing wide streets, and aesthetically enhance traffic corridors. The following guidelines give direction for pedestrian refuge islands and medians in the Specific Plan Areas:





- ii. Plants should be grouped according to their water needs and irrigated separately from other groupings with dissimilar water needs.
- b. Keep plant materials under 3 feet tall to maintain sight distance lines for passing vehicles.
- c. At crosswalks and pedestrian refuge islands, keep plant material below 18 inches for pedestrian visibility and safety.





Landscaping in sidewalk areas buffers Pedestrian Zone from Vehicular Zone

### 5. LANDSCAPING IN SIDEWALK AREAS

Sidewalk area plantings are of human scale and enhance pedestrian comfort by providing a buffer between pedestrian and vehicular traffic flows. Sidewalk area planting in the Specific Plan Areas should include planter pots, raised planters, landscaped parkways, plaza landscaping, and

parking lot screening and shading. In order to achieve a cohesive appearance and promote a small town atmosphere, joint participation and cooperation between private property owners and the City will be required.

- a. The three main traffic corridors in the Specific Plan Areas shall, where space is available, include landscaped planters, parkways, and/or street trees between the sidewalk and street to buffer pedestrians from the busy vehicular traffic.
- b. The use of accent planting with special qualities such as spring flowers and/or autumn color should be used to accent entry drives and intersections to unify the three primary corridors in the Specific Plan Areas.



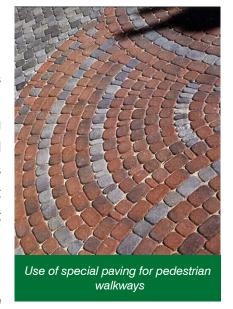
Use of landscaping to add color, strengthen pedestrian scale, and relieve expanses of hard surfaces

- c. Native plants or compatible species of drought-tolerant plants should be used to reduce water consumption.
- d. Choose hardy species that need minimal maintenance to help maintain healthy appearance.
- e. At crosswalks and driveways, keep plant material below 18 inches for pedestrian visibility and safety.
- f. A hanging flower basket program throughout the Old Town area is encouraged to enhance and beautify downtown and to establish a sense of community pride.

### 8.3 SIDEWALKS AND CROSSWALKS

### 1. SIDEWALKS

Sidewalks provide pedestrian access to community activities and provide critical connections between other modes of travel, including vehicular, public transit, and bicycle. Encouraging pedestrian travel is important to the social, economic, and environmental health of the community. Wide and continuous sidewalks, street trees and landscaping, and a consistent street furniture palette all contribute to a desirable pedestrian street scene.



- a. Continuous sidewalks, or equivalent provisions for walking, shall be provided along both sides of streets within the Specific Plan area.
- b. Design features such as enhanced paving on walkways, trellis structures, landscaping, and low-level lighting shall be used to distinguish the pedestrian route from the vehicular route.
- c. On-street parking, raised planters, and landscaped sidewalk planting strips shall be used to provide a buffer between pedestrians and moving vehicles.
- d. Sidewalks on all streets shall be a minimum of 6 feet wide.
- e. To ensure pedestrian safety and smooth flow of traffic, sidewalks shall have a pedestrian-travel zone of a minimum of 4 feet wide that is unobstructed.
- f. Throughout the Specific Plan Area, existing sidewalks that are to remain shall be surveyed for hazardous irregularities and repaired and maintained to the

City standards.

### 2. CROSSWALKS

Pedestrian crossings are a critical component of the circulation system in Pinole. All intersections along the three primary corridors in Pinole play a vital role in enhancing Pinole's streetscape due to their frequent use and high visibility.



Accent paving at crosswalks

- a. Incorporate accent paving at all intersections.
- Permeable crosswalk paving shall be implemented to achieve reduced stormwater flow and increased stormwater retention.
- c. Crossing distances shall be minimized to the greatest extent possible.



Landscaped median and pedestrian refuge island

- d. Corner curb extensions or sidewalk bulbouts should be used where feasible to reduce crossing distances and to calm traffic.
- e. Pedestrian refuge islands are extensions of the median that create a protected crosswalk area in the middle of the street. Where medians are present,

pedestrian refuge areas shall be provided.

 Pedestrian refuge islands shall be a minimum of 4 feet in order to reduce the possibility of island users from projecting into the traffic lanes. The width of the refuge island walkway shall not be less than the width of the crosswalk.



- ii. Pedestrian refuge areas shall be level with the crosswalk and have an accented paving surface different in color and texture to surrounding surfaces.
- iii. The median shall be extended a short distance beyond the edge of the crosswalk in order to ensure that turning vehicles do not encroach on the median pedestrian refuge area.
- f. Pedestrian crosswalks shall be adequately lighted, have clear sight distances, and be free from visual obstructions such as foliage and poles at crosswalk entries and median refuge islands.
- g. In-pavement flashers should be used at higher risk crossing areas with high traffic speeds and pedestrian volumes.
- h. Countdown pedestrian signals should be employed where high pedestrian count and traffic volume occurs at lighted intersections.

### 8.4 STREET FURNITURE

Street furnishings serve an aesthetic as well as utilitarian function and can create inviting outdoor spaces used for public interaction. Street furniture includes all items placed within the public right-of-way, such as street lamps, benches, bicycle racks, plant containers, trash receptacles, and



Furnishings enliven streetscapes

kiosks. Appropriate design and location of these furnishings will reinforce a unified design theme and create a lively atmosphere throughout the Specific Plan Areas.

Some of the envisioned public improvements will require private property owner participation and/or cooperation. Others can be implemented by the City as funding is secured and programmed as part of the capital improvement program.

### 1. GENERAL STREET FURNITURE GUIDELINES

- a. Streetscapes should include many pedestrian amenities that will create the character and identity for each of the three Specific Plan Areas.
- b. The design and selection of street furniture shall include considerations for the security, safety, comfort, and convenience of the user. Prior to selecting street furniture, the Public Works Department should review choices for durability, recycled content of materials, and ease of maintenance after installation.
- c. To ensure pedestrian safety and smooth flow of traffic, street furniture and public utilities

should be consolidated in the utility zone to keep them from becoming obstacles in the pedestrian travel zone.

- becoming obstacles in the pedesthan travel zone.
- d. Sidewalk cafes are encouraged within the frontage zone as a use that activates and energizes the public realm and creates 'eyes on the street' to enhance sense of safety.
- e. Pedestrian amenities shall be placed at regular intervals along the street edge of sidewalks without obstructing the required 4-foot minimum sidewalk width.



- f. To create a more organized and efficient use of sidewalk space, furnishings should be grouped together rather than scattered.
- g. A greater number and type of furnishings should be located in higher-use pedestrian traffic areas.
- h. Items should be securely anchored to the sidewalk, and a graffiti-resistant coating shall be applied to street furniture to maintain appearance.
- i. Provisions to accommodate persons with disabilities shall be incorporated into the design and siting of furnishings. This includes a provision for space adjacent to walkways for wheelchair and/or stroller parking.

### 2. LIGHTING

Street lighting plays an important role in providing safety and aesthetic quality in the Specific Plan Areas.

- a. Street lighting should achieve a number of objectives, including:
  - i. Contribute to the safe and efficient use of Pinole streets
  - ii. Enhance security
  - iii. Strengthen pedestrian scale
  - iv. Unify the Specific Plan Area
  - v. Create an attractive setting
  - vi. Enhance and encourage evening activities
  - vii. Prevent glare onto adjacent streets
  - viii. Minimize light pollution
- b. Provide sufficient lighting to ensure pedestrians are visible to motorists and to provide illumination of vehicles, street furniture, and other objects that should be avoided.



- c. Select and light routes that are intended to be the focus of pedestrian activity after dark. Avoid lighting areas not intended for nighttime use to avoid giving a false impression of use or safety.
- d. Pole and fixture design should be coordinated with other street furniture and amenities to establish an attractive and unified design character.
- e. The height of light fixtures should be kept low to promote a pedestrian scale in the public realm and to minimize light spill to adjoining properties.
- f. To reinforce the charm and pedestrian-scale of Old Town, the height of pole-mounted light fixtures in that sub-area should not exceed 16 feet in height from grade to light source.
- g. Pedestrian-scale street lights shall be provided at bus stops to provide additional security for transit users.
- h. Lighting sources shall have cut-off lenses and should be located to avoid light spillage and glare on adjacent properties and in private spaces.
- i. Energy efficiency and overall effect should be considered for lighting design.
- j. Light fixtures shall be of durable and vandal-resistant materials and construction.
- k. The style of lighting in a public parking lot shall relate to the overall architectural design of the surrounding commercial uses, shall not exceed 23 feet in height from grade to light source, and shall minimize glare into the night sky and adjacent areas.
- I. Lighting in public parking lots should fall throughout the entire parking area and pedestrian walkways.
- m. Accent lighting and uplighting on architectural and landscape features are required to add interest and focal points.
- n. Provisions for seasonal lighting are encouraged. Electrical service for seasonal/event lighting in trees should be provided.
- o. Light fixtures shall be routinely maintained and replaced as needed.

### 3. BENCHES & TRASH RECEPTACLES

- a. A 6-foot bench and trash and recycling receptacles should be placed on all three corridors at regular intervals no greater than 200 feet.
- b. Benches and trash and recycling receptacles shall be clustered at transit stops and public plazas.
- c. The City should consider selecting trash receptacles with an integrated container for recycling bottles and cans.

### 4. BICYCLE RACKS

- a. Bike racks shall be located throughout Old Town, near transit stops, parking lots, shopping centers, employment centers and places with high pedestrian activity. These amenities are intended to encourage and support bicycle ridership.
- b. The "U" shape style rack is encouraged because it allows bikes to be parked parallel to the sidewalk, which keeps them out of the pedestrian walkway.
- c. Integration of public art into bike rack design is encouraged.



### 5. Bus Stop Amenities

- a. All bus transit stops should provide benches and lighting that create an attractive, safe, and comfortable place for transit users.
- b. Bus shelters should be provided at bus stops located near shopping areas, parking areas, and public facilities and at heavily used transit stops through coordination with the City Transit Manager and the Western Contra Costa Transportation Authority.
- **c.** Bus shelters should be illuminated during evening service hours to five average maintained foot-candles.
- d. Shelters should be distinctive and reflect the character of the districts.

- e. Bus shelters should be designed to promote transit and energy efficiency by incorporating features such as solar panels, LED lights, etc.
- f. Transit stop design and siting shall address the needs of disabled users.
- g. All bus stops should be prominently signed and all pertinent route and schedule information should be posted.
- h. Consider coordinating the design and location of bus stops with plazas in order to integrate amenities, activate plazas, and make efficient use of public space.

#### 6. ADDITIONAL FURNISHINGS

- a. Newspaper racks, drinking fountains, kiosks, etc. should be carefully located throughout the Specific Plan Areas.
- b. Drinking fountains should be located in public gathering areas.
- c. Waist-high safety bollards should be used to define public plazas to delineate between Vehicular and Pedestrian zones and create a safe walking environment.
- d. Newspaper racks shall be clustered in groups to minimize a cluttered sidewalk appearance. Newspaper racks should be carefully located at higher use activity and transit nodes along the three corridors to avoid circulation conflicts.
- e. Kiosks should be located at key locations between parking and shopping areas and at key plazas. Kiosks should effectively direct visitors to restrooms, plazas, shopping areas, parking, and public facilities and from one district to another.



### 8.5 PUBLIC ART

Public art adds vitality, culture, and character to street life and public gathering places. Public art can contribute to a sense of identity and pride in the community. Art can act as a focal point in a public space as well as a functional feature (e.g., benches). The City should consider initiating an "Art in Public Places" program which can provide incentives as well as lay out guidelines for design and placement of art in Pinole.

- a. All capital improvement and development projects should explore the integration of public art into the design of public streetscape elements (e.g., paving, street furniture, transit shelters, lighting).
- b. Public art is encouraged to be used in a variety of locations to enhance the quality and use of the public space or downtown street environment.
- c. Public art should consider the incorporation of functional streetscape amenities, such as benches, walls, stairs, paving, bike racks, and bus stop shelters.
- d. Public art should reflect the spirit and character of the community. Murals are a colorful form of public art that can celebrate the history and culture of Pinole.





Public art integrated into bike racks

e. The design and materials of the artwork should be durable against weather and vandalism, and should not require extensive maintenance.



### 8.6 SIGNAGE

A wayfinding program can help people to navigate through the Specific Plan Areas and enhance their sense of safety and comfort. Signage should work in concert with lighting, landscaping, and paving to clearly direct people to their destination. Signage can also be used to provide a unifying theme for the Specific Plan Areas that respects the architectural and cultural traditions of each of the corridors. A Specific Plan Area logo and signage program should be established to help distinguish each project area from other areas of Pinole. The logo can be placed on directional signs, street signs, and banners to develop both a sense of place and an identity. The Old Town sub-area of the San Pablo Avenue Specific Plan has an existing, distinct logo and signage program.

### 1. DIRECTIONAL SIGNAGE

The signage program should include a common directional sign with directional arrows and labeling to denote key shopping areas, parking, civic buildings, and tourist attractions.

a. Directional signs shall be oriented to both pedestrian and vehicular traffic. Signs should be lighted, landscaped, and placed permanently at roadsides at key locations throughout the Specific Plan Area.



b. The directional signs shall reflect design components of the street banners and street signs.

### 2. STREET SIGNAGE

Every street in the Specific Plan Areas shall be identified in a uniform way to provide a recognizable sense of place.

- a. The Old Town sub-area has its own distinct street signage to emphasize its unique character and status of Old Town.
- b. Street signs shall include unifying elements, such as:

- i. A unique color;
- ii. A font selection consistent with the desired character of the area and consistent with the gateway signs;
- iii. A logo or phrase to identify the area;
- iv. A design reflective of the banner and directional signs.

#### 3. STREET BANNERS

Banners or flags for use on area light standards shall be included in the signage program.

- a. Banners with an appropriate logo and graphic representing a community-wide special event or festival should be developed.
- b. Banners may be changed periodically to provide advertisement for special events and promotions in Pinole.

### 4. GATEWAYS

Pinole contains a number of natural "Gateways" that are identified in Chapter 3. These locations create an opportunity to incorporate gateway features that provide a sense of arrival and a transition into the Specific Plan Areas and into Old Town Pinole. These visual gateway features are civic in emphasis and serve to identify and promote the distinct identity of Pinole's primary corridors. Features are coordinated to reinforce the overall design theme of each project area.

- a. Special entrance treatments should be provided at the following gateways:
  - i. Entry into Pinole through Appian Way and Pinole Valley Road











- ii. Entry into Pinole at Appian Way and El Sobrante
- iii. Entry into Specific Plan Area from Appian Way
- iv. Entry into Old Town from Pinole Valley Road/Tennent Avenue
- v. Entry into Old Town from Hercules (City boundary)
- vi. Others as identified by the City in future
- b. Entrance features may consist of a combination of plant materials, hardscape elements (such as walls, paving, and/or monuments), and signage. Gateways can provide an opportunity for architectural features, monuments, public art, murals, banners, and lighting features.
- c. Gateways should enhance the visitors' experience when entering the project area and Old Town Pinole. These features serve as landmarks and shall be of high quality design materials.

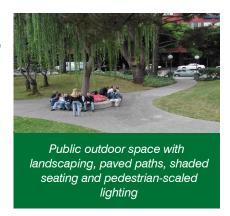


### 8.7 PLAZAS AND PUBLIC OUTDOOR SPACES

Throughout the Specific Plan Areas, plazas of a variety of sizes should be incorporated into commercial, mixed-use, and transit areas to accommodate different types of activities. These public outdoor spaces serve to establish a sense of place and identity and provide space for participating in public life. Plazas create opportunities for socializing and foster a sense of community.

- a. Public outdoor spaces shall be well designed and incorporate a combination of accent materials, site furniture, shade structures, accent lighting, color, texture, art, or other focal elements.
- b. Soft landscaping as well as hard-surfaced areas shall be incorporated into the overall plaza design.
- c. Plazas should be designed to accommodate a range of desired activities such as sitting, eating, strolling, gathering, entertainment, and festivals.

- d. Plazas shall provide pedestrian amenities like seating, lighting, planters, fountains, drinking fountains, distinctive paving, public art, landscaping, and bicycle racks.
- e. Focal points such as sculptures and water features should be incorporated into plaza design.
- f. Plaza design should provide amenities for varying light and climate conditions and protection from sun and wind.



- g. Covered areas along the perimeter of parks and plazas (i.e., vine-covered pergola) are strongly encouraged to provide protection from rain and/or sun.
- h. A plaza should have an articulated edge (buildings, benches, landscaping, etc.) where feasible to define the plaza and create comfortable space. Plaza edges which open to pedestrian through-traffic should be defined with amenities such as a planter, low seating wall, or sculpture.
- i. Site furniture should be selected not only for its functional and aesthetic qualities but also focus on the quality of materials and finishes that provide long-term durability and resistance to vandalism.

j. Plazas should be located in highly visible locations to encourage use and discourage unwanted activity.

- k. Planting should maintain open and clear sightlines and not foster places of concealment.
- Visual surveillance of plazas and public outdoor spaces from adjacent streets and buildings should not be obscured through landscaping.
- m. Public space lighting shall be low in height with a maximum height of 16 feet from grade to light source.
- n. Plaza lighting should incorporate pedestrian-oriented lights, such as light bollards, pole lights, and wall-mounted lights.



Covered areas along perimeter of plaza

- o. Uplighting/downlighting of trees and other site features is encouraged.
- p. Paving material content should incorporate a portion of recycled materials, such as recycled aggregate, recycled asphalt, or rubberized asphalt concrete from scrap tires, as appropriate to the base material.
- q. Permeable paving is encouraged to achieve reduced stormwater flow and increased stormwater retention.

9.0	Infrastructure and Public Facilities			
9.1	Introduction	9.0-1		
9.2	Water Supply, Treatment, and Distribution	9.0-2		
9.3	Sewer/Wastewater	9.0-3		
9.4	Solid Waste Collection, Disposal, and Recycling	9.0-4		
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9.7	Public Safety	9.0-10		
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### 9.1 INTRODUCTION

This chapter addresses issues concerning water, waste water, solid waste, stormwater/drainage, public safety, and other public services within the three Specific Plan project areas. The three Specific Plan corridors are developed areas with existing infrastructure (e.g., roadway network, water, sewer, storm drainage) and essential services (e.g., police, fire, schools, parks, street lighting, and utilities).

Currently, infrastructure and services are provided and maintained by the City of Pinole and several other public and private agencies. The City's General Plan and Redevelopment Plan identify policies and actions to improve the infrastructure and services for the entire City in general and for the Planning Areas in particular. Of critical importance to the Specific Plan is the capacity and condition of the sewage treatment plant. Large portions of the existing collection system are old and experience substantial infiltration that results in wet weather flows that exceed the capacity of the treatment plant. This issue will have to be corrected in order to accommodate new development without further exacerbating treatment deficits.

Service Providers in Specific Plan Project Areas			
Water Supply, Treatment, and Distribution	East Bay Municipal Utility District (EBMUD)		
Sewer/ Waste Water	City of Pinole and West County Sanitary District (WCWD)		
Solid Waste Collection, Disposal and Recycling	Richmond Sanitary Services (RSS) Portero Hills Landfill		
Storm Drainage and Hydrology	City of Pinole Public Works Department		
	Electrical and Natural Gas: Pacific Gas and Electric (PG&E)		
Dry Utilities	Telephone and Telecommunications: AT&T and Comcast Pinole Local Cable Access – Channel 26 and 28		
	Police: City of Pinole Police Department.		
Public Safety	Fire: City of Pinole Fire Department		
	Ambulance: American Medical Response		
Parks and Public Facilities			
City Government Facilities	City Hall		
School Districts	West Contra Costa Unified School District (WCCUSD) - 8 schools		
School districts	Private Schools: 4 schools		

Service Providers in Specific Plan Project Areas			
	Library: Pinole Library, Contra Costa County Library system		
	Parks: East Bay Regional Park District (EBRPD) (Regional Parks, regional inter-park trails and trails within EBRPD parklands) and City of Pinole (community and neighborhood parks)		
	City provides recreational facilities including parks and trails – 1 regional park, 4 community parks, 5 neighborhood parks and 4 unimproved open space public areas		
Parks and Community Facilities	Community Recreation Facilities:		
,	Pinole Youth Center		
	City of Pinole Recreation Department		
	Tiny Tots, a child care service and facility		
	Pinole Valley Tennis Courts		
	Pinole Swim Center		
	Pinole Playhouse (Memorial Hall)		
	Pinole Senior Center		
	Western Contra Costa Transit Authority (WestCAT)		
Public Transportation	Alameda Contra Costa Transit Authority (AC Transit)		
	Eastbay Paratransit		

### 9.2 WATER SUPPLY, TREATMENT, AND DISTRIBUTION

### **EXISTING CONDITIONS**

Water Services throughout the City of Pinole is provided by the East Bay Municipal Utility District (EBMUD). EBMUD's primary water source is the Mokelumne River in the Sierra Nevada, with a secondary water source from runoff in the local watersheds at EBMUD terminal reservoirs in the East Bay area. EBMUD's water supply system consists of a network of reservoirs, aqueducts, water treatment plants, pumping plants, and distribution facilities. During normal hydrologic years, average local supply is 15 to 25 million gallons per day (MGD). Average daily water consumption for EBMUD was approximately 205 MGD in 2005.

EBMUD operates five terminal reservoirs including Briones, San Pablo and Upper San Leandro reservoirs, which serve the District throughout the year, and Chabot and Lafayette reservoirs, which serve as emergency sources of supply. These five reservoirs have a total storage capacity of 151.8 thousand acre-feet (49.5 billion gallons). The City of Pinole's water comes from the San Pablo reservoir. Potential threats to the Pinole water supply include a declining snow pack in the Sierra Nevada as a result of rising global temperatures and water pollution from hazardous waste that may be leaching into the ground, affecting the local water table of Pinole.

### WATER TREATMENT AND DISTRIBUTION

The City of Pinole's water is treated by the Sobrante treatment plant in El Sobrante. Water is filtered through sand and anthracite or carbon and is disinfected and fluoridated. EBMUD's Laboratory Services Division performs water quality analysis throughout the day and every day to ensure the safety of drinking water.

### **BUILD-OUT CONDITIONS**

New development must "pay for itself." Therefore, if new development proposed within the Specific Plan Area causes an increased demand on the system, the developer would be responsible for paying for the necessary system improvements (new distribution pipelines, water supply or storage) in order to meet the increased demand and/or higher flow requirements.

### 9.3 SEWER/WASTEWATER

### **EXISTING CONDITIONS**

The City of Pinole is primarily responsible for sewer service to the City, with the West County Sanitary District providing service to approximately 0.4 square miles within Pinole.

### **West County Sanitary District (WCSD)**

WCSD is responsible for collection and treatment of flows tributary to their lift stations and treatment plant. A portion of the flow that enters the WCSD Tara Hills Lift Station is generated



The Pinole/Hercules Water Pollution Control Plant in Pinole

within the City of Pinole. All wastewater that enters the two WCSD lift stations along Fitzgerald Drive is generated within the City of Pinole.

Wastewater north of Interstate 80 flows by gravity to the Tara Hills Pump Station. The approximate average dry weather flow (ADWF) is 0.14 million gallons per day (MGD). Wastewater

south of Interstate 80 flows by gravity into the Pinole Lift Station and the Fitzgerald Lift Station. The approximate ADWF for this area is 0.13 MGD.

### Pinole/Hercules Water Pollution Control Plant (PHWPCP)

The Pinole/Hercules Water Pollution Control Plant (PHWPCP) is located at the end of Tennent Avenue in the City of Pinole. The treatment plant was originally built in 1955. The plant has had two major expansions and several modifications in order to meet population growth and higher effluent treatment standards.

In 1972, the plant was upgraded from a primary to a secondary treatment facility to a flow capacity of 3.2 MGD ADWF. In 1985, the plant was upgraded to handle a flow of 4.06 MGD in dry weather and 10 MGD in wet weather.

Pipes of varying size (6 to 30 inches wide) transport wastewater to the plant. Once the wastewater reaches the PHWPCP, the treatment processes that are employed include screening, primary treatment, activated sludge, secondary sedimentation, chlorination, and de-chlorination. Waste solids are thickened and treated in anaerobic digesters, then separated from liquid before being disposed at the landfill. There are no wastewater storage tanks on the plant's site. There are, however, five chemical tanks, one diesel tank, one gasoline tank, and one solid storage tank on-site.

### **BUILD-OUT CONDITIONS**

In 2004, Carollo Engineers completed the Pinole Wastewater Transfer Study to determine the feasibility of transferring wastewater flow tributary generated within the City's jurisdiction to WCSD's San Pablo Avenue and Hazel Street lift stations. The City rejected the alternative in favor of expanding the PHWPCP wastewater treatment plant. In light of the City's decision, WCSD designed a replacement force-main from the Tara Hill's Pump Station that does not include additional capacity for wastewater flows from the City of Pinole.

The City's sewer system has a significant amount of inflow and infiltration that results in flows that exceed the capacity of the treatment plant during wet weather. The additional development contemplated in the Specific Plan will only exacerbate this problem. The Public Works Department is evaluating alternative capital improvements that will correct this problem. Any such solution will have to be designed to address current capacity problems while planning for future development within the cities of Pinole and Hercules.

### 9.4 SOLID WASTE COLLECTION, DISPOSAL, AND RECYCLING

### **EXISTING CONDITIONS**

### **Solid Waste Collection**

Richmond Sanitary Services (RSS) provides weekly solid waste collection and disposal services to residents and businesses in Pinole. The RSS service area covers the cities of Pinole, Hercules, Richmond, and San Pablo and the unincorporated areas of west Contra Costa County.

In 2005, the City of Pinole generated 10,395 tons of garbage (8 percent of the county's garbage) and 2,588 tons of recyclables (12 percent of the county's generated recyclables).

### **Solid Waste Disposal**

The West Contra Costa Integrated Waste Management Authority (WCCIWMA) is a joint powers agency made up of the cities of El Cerrito, Hercules, Pinole, Richmond, and San Pablo and Contra Costa County and oversees the waste for these cities and areas.

The West Contra Costa County Sanitary Landfill (WCCSL), which received waste from these cities for the past 50 years, closed in October 2006. Solid waste from Pinole is now transported to the Golden Bear Transfer Station. Golden Bear Transfer Services, Inc., a subsidiary of Republic Services, Inc, opened the transfer facility. From the Golden Bear Transfer Station, the waste is transported to the Portrero Hills Landfill in Suisun City in Solano County (25 miles away) for disposal. The WCCIWMA has a contract with Portero Hills Landfill through 2014, at which point the contract will be renegotiated. The estimated remaining capacity of Portrero Hills Landfill is 38.1 percent.

### **Recycling and Green Waste**

Richmond Sanitary Services (RSS) exclusively provides biweekly curbside collection of aluminum, bimetal, tin, all paper products, CRV glass bottles and jars, #1 PETE, #2 HDPE and #3-#7 plastics, milk jugs, water, juice and soda bottles, and used motor oil and filters. RSS also collects source separated cardboard and other materials from high volume commercial and industrial generators. RSS is obligated by contract to continue services through the year 2011.

In 2005, the City of Pinole generated 2,588 tons of recyclables (24.9 percent of the City's



Richmond Sanitary Service provides weekly solid waste and recycling collection to Pinole.

generated waste was recycled). RSS estimates that the City of Pinole diverts a moderate percentage of its waste to green waste; the City averages 174.1 tons of green waste per month, which is a diversion rate of 13.6 percent.

RSS delivers recyclables to the WCCIWMA Integrated Resource Recovery Facility (IRRF), which opened in January 1997 as a recyclables processing facility. The IRRF runs one recyclables sorting shift and one materials bailing shift each day; approximately 200 tons are processed daily.

Household hazardous waste is also managed by the West Contra Costa Integrated Waste Management Authority, a joint powers agency created by the cities of El Cerrito, Hercules, Pinole, Richmond and San Pablo. The West County Drive-Through Household Waste Collection Facility charges no fee to drop off hazardous waste.

### **Build-Out Conditions**

If new development proposed within the Specific Plan Area causes an increased demand on the solid waste collection system, the developer will have to coordinate with the City and service providers in order to meet the increased demand.

### 9.5 STORM DRAIN SYSTEM

### **EXISTING CONDITIONS**

The City of Pinole Public Works Department does not have a storm drainage master plan. The Department does, however, maintain an inventory of facilities. The Department plans to prepare a drainage master plan in the 2008/09 fiscal year.

Except for localized flooding and standing water, which may occur during brief, intense storms when runoff exceeds stormwater capacity, creek flows along Pinole Creek would largely be contained within the existing creek bank during a 100-year storm.

Flooding in Pinole is generally the result of two phenomena:

- Storm runoff originating within Pinole and flooding lowlands due to inadequate drainage channels and pipes necessary to transport this water into San Pablo Bay (sheet flooding).
- High water in the San Pablo Bay that, in turn, pushes saltwater into the stream channels and inundates all lands below the tide level that are not protected by a levee. The elevation of the water surface in the Bay is largely dependent upon the tide and wind and wave effects.

Over the last 100 years, Pinole Creek has periodically flooded downtown Pinole. In 1965, the U.S. Army Corps of Engineers straightened the lowest one and a half miles of Pinole Creek and constructed a flood control channel in an effort to protect the City from the 50-year flood.

The City of Pinole is part of the Pinole Creek watershed. In response to landowner and stakeholder concerns regarding the control of sediment erosion, the maintenance of aquatic habitat, and maintaining flood conveyance in the Pinole Creek watershed, several initiatives and ecological

studies have been undertaken since 2000. The Pinole Creek Watershed Vision Plan includes a proposed lower flood control channel restoration project that aims to improve creek habitat while providing flood protection.

#### **IMPACT OF FLOODING**

Potential damage to the bayside area from flooding includes property damage to yards, outstructures, foundations, garages, interiors of houses, streets, curbs, and other public infrastructure. Flooded streets can cause other types of problems, such as economic losses to businesses in nearby areas and even loss of life indirectly due to the inability of emergency vehicles to access flooded areas. Thus far, street flooding has been localized and temporary.

Water from storm drains flows directly into San Pablo Bay without treatment.

### **REGULATORY PROVISIONS**

The Contra Costa Clean Water Program Evaluation determines a permittee's compliance with the National Pollutant Discharge Elimination System (NPDES) permit and evaluates the current implementation status of the permittee's performance standards. The City of Pinole maintains several programs and initiatives relating to stormwater management. The City tracks illicit discharges and stormwater inspections and uses a detailed database to track municipal maintenance and to schedule inspections and cleaning activities. The purpose of the City of Pinole's Stormwater Ordinance is to eliminate non-stormwater discharges to the City's stormwater system, to protect the City's stormwater system from spills, dumping or disposal of materials other than stormwater, and to reduce pollutants in stormwater discharges to the maximum extent practicable.

As a participant in the National Flood Insurance Program (NFIP), Pinole is required to adhere to floodplain management policies that represent sound land use practices. The Federal Emergency Management Agency (FEMA) administers the NFIP through the Federal Insurance Administration. FEMA produces Flood Insurance Rate Maps (FIRMs) for the communities participating in the NFIP, which identify flood hazard areas and restrict development in these areas. FEMA is currently updating its flood hazard area maps.

### **BUILD-OUT CONDITIONS**

Any changes in land use or redevelopment will require an evaluation of the existing drainage system to check for adequacy. This evaluation would involve an analysis of impacts to the drainage system due to higher intensity land use and identify locations where inadequacies already exist.

If upgrades to the existing drainage structures prove necessary, they would have to be in compliance with City of Pinole Drainage Improvement Standards. Possible upgrades include constructing more storm drains, building detention basins in the area, creating overland release pads, increasing drainage pipelines, and increasing drainage pipe sizes.

### 9.6 DRY UTILITIES

Dry utilities include electricity, natural gas, and telecommunications (i.e., telephone and cable).

### **ELECTRICAL AND GAS SYSTEMS**

### **Existing Conditions**

Pacific Gas and Electric (PG&E) provides electricity and natural gas to the City of Pinole. The majority of energy that PG&E provides the City is re-

PG&E service truck in Pinole.

gas to the City of Pinole. The majority of energy that PG&E provides the City is renewable (57 percent), and the remaining energy sources are gas (42 percent) and coal (1 percent).

### Alternative Energy and Energy Efficiency

Though there are few energy efficiency projects in the City of Pinole, the City's Housing Element promotes citizen awareness of energy conservation programs in concert with rehabilitation loan programs. PG&E has numerous programs that promote energy efficiency, many of which are free to public agencies. PG&E's Energy Partners Programs provides assistance for energy conservation improvements.

#### **Build-Out Conditions**

With the City approaching buildout, it is unlikely that PG&E will have difficulties in maintaining gas and electric service to Pinole.

Post build-out, the design requirements for serving the Specific Plan project areas are unclear at this time and will depend on schedule and load requirements and any other future new business development in the area. However, possible service solutions include extending new distribution mains from adjacent facilities and installing system upgrades to the existing system, as needed, to extend service to new development within the Specific Plan Area per PG&E's utility obligation to serve in accordance with its Gas Rules and Tariffs on file with the California Public Utilities Commission.

### **TELEPHONE AND TELECOMMUNICATIONS**

### **Existing Conditions**

AT&T provides local telephone service to the City, while cable television service is provided through Comcast. The City of Pinole also hosts "Pinole TV," a community access television station that provides public, educational, and governmental information and programming. All three providers have the ability to maintain these services to meet the need of City residents and businesses in the future.

Both the City and Contra Costa County have experienced a dramatic increase in demand for telecommunications products and services in the last decade as this industry has emerged to provide a new form of customer phone and related services. The City of Pinole requires all wireless communication facilities to have a use permit. Use permit applicants are required to submit a master plan for all related facilities, a computer-enhanced photo image of the site, a mock-up of an antenna if proposed, and a preliminary report based on the current FCC rules, regulations and standards, and alternative site analysis. The use permit requires Planning Commission approval before wireless communication facilities may be installed.

The California Public Utilities Commission regulates telecommunications and maintains that local jurisdictions cannot prohibit or otherwise unduly restrict utilities, such as cellular phone installations.

Non-commercial, community access television is provided through Pinole TV channels 26 and 28. Pinole TV regularly televises Redevelopment Agency and City Council meetings in addition to providing informative and educational programs.

### **Build-Out Conditions**

AT&T will increase capacity as necessary based on demand at the time of the service request, and no crucial change in cable and satellite television and Internet level of service is expected upon build-out of the Specific Plan project areas. No such expansion would be anticipated for cable TV services provided by Comcast and Pinole TV.

### 9.7 PUBLIC SAFETY

The incremental growth anticipated by the Specific Plan will increase the need for public safety services from police and fire departments. Though additional personnel (sworn officers and firefighters) will be needed to serve this additional population, existing facilities and equipment appear to be adequate to serve the incremental growth of the Specific Plan project areas.



### **POLICE**

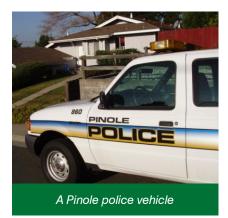
### **Existing Conditions**

Police protection services within the City of Pinole are provided by the City of Pinole Police Department. The main police station is located at 880 Tennent Avenue in the Public Safety Building, which it shares with the Fire Department.

The Pinole Police Department also provides protection services for the City of Hercules through a mutual aid agreement.

### Staffing

The Police Department has one chief of police, one deputy chief of police, one lieutenant, five sergeants, and 20 officers, plus administrative staff and volunteers. Volunteers assist with evidence gathering, parking, and office responsibilities.



### **Public Education and Outreach**

The Pinole Police Youth Academy hosts sessions to introduce 14- to 17-year-olds to the career of police officers and personal safety.

### **Build-Out Conditions**

The Pinole Police Department currently has no plans to upgrade or reconstruct the Public Safety Building in the future. They are currently going through a bidding process to ergonomically

upgrade and remodel the front office and the watch commander's office. Additionally, the Police Department provides ongoing maintenance to the facility.

### **FIRE PROTECTION**

### **Existing Conditions**

The City of Pinole Fire Department provides fire protection services within the incorporated area of the City. The Department uses two fire stations:

- 1. Station 73 located at 880 Tennent Avenue in the Public Safety Building; and
- 2. Station 74 located at 3700 Pinole Valley Road.

Opened in May 2002, Station 74 primarily serves the Pinole Valley residents located south of Interstate 80 and Pinole Valley Road south of the interstate, including surrounding wildlands. Station 74 also provides the secondary engine on any multiple responses or multi-engine responses.



### **Staffing**

The Fire Department has 18 full-time firefighters on a 56-hour shift schedule, one fire chief, one full-time administrative assistant, and six Reserve Division firefighters. The Reserve Division firefighters are similar to volunteer firefighters.

### **Calls for Service and Response Times**

In 2005 and 2006, the Fire Department responded to 2,318 and 2,402 calls for service. Emergency Medical Service (EMS) accounts for 64 percent of call volume. Target response time for fire protection services is five minutes or less. The Fire Department also provides paramedic response and transport services for the City. The Pinole Fire Department also provides aid to the Rodeo/Hercules, Richmond, and Contra Costa County Fire Departments on an as-needed basis through a mutual aid agreement.

### **Build-Out Conditions**

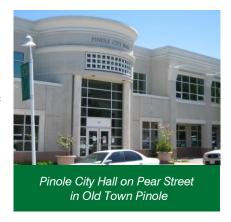
### **Joint Powers Agreements**

The Fire Department operates under the State of California Master Mutual Aid System. Additionally, the Fire Department operates in Battalion 7, an automatic aid agreement with the Contra Costa County and Rodeo-Hercules Fire Districts. Under a longstanding agreement with Contra Costa County, the Fire Department provides service to the unincorporated areas of Tara Hills, Bayview, and Montara Bay Mobile Park. In addition, the Contra Costa County Regional Fire Communications Center provides fire and medical dispatch services for the City of Pinole.

### 9.8 PARKS AND PUBLIC FACILITIES

### **CITY GOVERNMENT FACILITIES**

The City of Pinole, incorporated in 1903, offers a wide range of services to its residents. The City operates through election of five City Council members, who in turn appoint a city manager. The Council also appoints citizens to a number of commissions and committees, including:



- Planning Commission
- Youth Commission
- Economic Development and Housing Advisory Committee
- Traffic and Pedestrian Safety Committee
- Community Services Commission
- Ad hoc Safety Advisory Committee
- CCC Mosquito & Vector Control District Board
- Contra Costa Library Commission
- West Contra Costa Unified School District Bond Oversight Committee

The City government is organized into several departments and related agencies:

• Administration: The Administration Department is responsible for implementing the vision of the community as directed by the City Council. The city manager has oversight over all staff functions and responsibilities, including Cable TV, City Clerk, Code Enforcement, Community Development, Finance, Fire, Information Systems, Personnel, Police, Public Works, Recreation, Redevelopment, and the Water Pollution Control Plant. The Administration Department supports the City Council efforts to engage in legislative advocacy on the local, state, and national level, advocating the City's interests and increasing the City's influence as a leader in good government.

- Finance Department: The City of Pinole Finance Department is responsible for financial reporting to other government agencies. The finance department provides a variety of services including budget development, financial analysis, bond debt administration, revenue billing and collection, accounts receivable, accounts payable, cash flow management, payroll processing, employee benefit accounting, record keeping and reporting.
- Public Works: The Public Works Department is responsible for managing capital improvement projects, operation and maintenance of the wastewater treatment plant, and maintenance of public parks, streets, sidewalks, storm drains, streetlights, trees, and public landscaping areas.
- Community Development Department: The Community Development Department (CDD) includes the Planning, Building, Redevelopment, and Enforcement Divisions. This department is responsible for implementing the goals and policies of the General Plan and Redevelopment Plan, as well as implementing and enforcing the Zoning and Building Codes. The CDD provides staff support to the Planning Commission and Economic Development and Housing Advisory Committee.
- Recreation Department: The Recreation Department coordinates and manages all recreation activities, events, and facilities in the City, including youth, summer camps, seniors, Tiny Tots, athletics, community events, community theatre, aquatics, and parks. Recreation facilities include Youth Center, Senior Center, Tiny Tots, Pinole Community Playhouse, Swim Center, tennis courts, athletic fields (primarily soccer and baseball), and parks. The Recreation Department works in collaboration with the Public Works Department on park and athletic facility maintenance repair and upgrades. The Recreation Department also staffs the Youth Commission, nonprofit Pinole Youth Foundation, and Senior Center Board.



### **SCHOOLS**

### **Existing Conditions**

### **Public Schools**

Pinole elementary, middle, and high school students are served by the West Contra Costa Unified School District (WCCUSD).

There are six elementary schools in Pinole and its Sphere of

Influence: Collins Elementary School, located at 1224 Pinole Valley Road in Pinole; Ellerhorst Elementary School, located at 3501 Pinole Valley Road in Pinole; Montalvin Manor Elementary School, located at 300 Christine Drive in Tara Hills (an unincorporated community); Shannon Elementary School, located at 685 Marlesta Road in Pinole; Stewart Elementary School, located at 2040 Hoke Drive in Pinole; and Tara Hills Elementary School, located at 2300 Dolan Way in Tara Hills (an unincorporated community). These elementary schools feed Pinole Junior High School, located at 1575 Mann Drive in Pinole, and Pinole Valley High School, located at 2900 Pinole Valley Road in Pinole. Together, these schools comprise the Pinole High School Attendance Area (PHSAA).

### **Enrollment and Facilities**

Enrollment and capacity of the schools in the West Contra Costa Unified School District are summarized in the table below.

Table 9.1
City of Pinole Public Schools

School	Grades	Student Enrollment 2006–2006	Student Capacity
Collins Elementary School	K-6	443	461
Ellerhorst Elementary School	K-6	443	449
Montalvin Manor Elementary School	K-6	423	387
Shannon Elementary School	K-6	379	385
Stewart Elementary School	K-8	476	504
Tara Hills Elementary School	K–6	460	468
Pinole Junior High School	7–8	710	858
Pinole Valley High School	9–12	1,763	1,837
Total Students		5,097	5,349

Source: West Contra Costa Unified School District, December 2006.

### **School Programs**

Pinole Valley High School includes the Environmental Studies Academy (Academy), a three-year "school within a school" program that is offered to students beginning their sophomore year. The Academy integrates subject matter from science, social studies, English, and technology courses largely through semester projects. Students enrolled in the Academy must take science, social studies, English, and technology courses. The Academy's intent is to provide students with an alternative high school path that enables students to combine college-prep academics and experiences for entry-level careers in the area of environmental studies.

Contra Costa College, located at 2600 Mission Bell Drive in San Pablo, is a public community college that serves the Pinole area. The college offers a variety of programs and services for eligible students.

### **Private Schools**

There are four private academic schools in Pinole serving preschool through twelfth grade students: La Casita Bilingue Montessori School, located at 592 Tennent Avenue, with preschool and kindergarten teaching in the Montessori method; St Joseph Elementary School, located at 1961 Plum Drive with kindergarten through eighth grade; Global Christian Academy, located at 725 San Pablo Avenue with kindergarten through tenth grade; and El Sobrante Christian School, with an elementary campus located at 5100 Argyle Road, and a junior high and high school campus located at 5070 Appian Way in El Sobrante. Enrollment at these schools is approximately 350 students.

### **Build-Out Conditions**

The West Contra Costa School District (WCCUSD) is not experiencing capacity issues, with the exception of Montalvin Manor Elementary School, which is operating at 109 percent capacity. WCCUSD projects a decline in student enrollment in the 2010-2011 school years with a total



Montessori School in Pinole

decrease of 377 students in Pinole, including 38 fewer students in elementary schools, 75 fewer students in middle schools, and 264 fewer students in high school.

With the exception of Collins, all elementary schools in WCCUSD have been modernized with Measure M bond funding. Collins is not scheduled for modernization, although many possible improvements have been identified. Pinole Junior High School, originally constructed as a K-6 elementary

school, is undersized for a junior high school with regard to lot size. The Pinole Junior High School is operating at 83 percent of its facility capacity and is scheduled for renovation under Measure D. Pinole Valley High School is currently operating at 96 percent capacity.

#### **PARKS**

### **Existing Conditions**

The City of Pinole maintains regional, community, and neighborhood parks varying in size and generally distributed throughout the community. Most of the parks in the City are located within walking or biking distance of residential neighborhoods.

The City has fourteen parks, including one regional park, four community parks, five neighborhood parks, and four unimproved open space public areas. The City's parks and their size and facilities are identified in the table below.

Table 9.2 City of Pinole Parks

Park Site	Location	Gross Park Area (acres)	Net Active Park Area (Acres)	Undeveloped Usable Area	Park Description and Facilities
Regional Parks					
Wilson Point East Bay Regional Park	End of Pinole Shores Drive	64	64		East Bay Regional Park District owned and managed. Extensive walking trails, a drinking fountain, and benches along the walking paths.
Community Parks					
Bayfront Park	1 Tennent Avenue	92	14		Small community park utilized for walking trails and July 4 <sup>th</sup> fireworks.  Walking paths and trails along shoreline and Pinole Creek, two picnic tables, benches, BBQ facilities, drinking fountain, and restroom facilities.

Park Site	Location	Gross Park Area (acres)	Net Active Park Area (Acres)	Undeveloped Usable Area	Park Description and Facilities
Fernandez Park	595 Tennent Avenue	7	4.75		Community Park in Old Town Pinole. Hosts City and other community events. Adjacent to Youth Center, Senior Center, and Community Playhouse.  Gazebo area, horseshoe pit, Youth and Senior Center, garden, tot lot, picnic tables, two parking lots, BBQ facilities, play area, restroom facilities, drinking fountains, baseball field, and basketball court.
Pinole Valley Park	3790 Pinole Valley Road	197	13	169.75	Second primary community park. Hosts most City athletic fields. Hiking trails connecting to El Sobrante Ridge, picnic tables, two parking lots, one playground, concession stand, restroom facilities, drinking fountains, two soccer fields, one baseball field, and one basketball court.
Pinole Valley Adobe Grove and Dog Parks	1267 Adobe Road	34	0.75	34.04	Primarily utilized for rental picnic areas plus small and large dog parks.  Large and small dog parks, four picnic areas, BBQ facilities, restroom facilities, and drinking fountains.
Neighborhood Parks	3				
Amber Swartz Park	3450 Savage Avenue	4	3.92		Small community-built memorial park. Gazebo, walking path to Pinole Valley Park, and two benches located in the gazebo.
Canyon Drive Park	1818 Canyon Drive	0.5	0.5		Small park for local neighborhood.  Picnic table, a large open play area, a small sand pad, and a small play structure.
Louis Francis Park	596 Marlesta Drive	2.75	2.75		Medium-size park for local neighborhood. Picnic tables, one small BBQ pit, benches, drinking fountain, and sand pad.

Park Site	Location	Gross Park Area (acres)	Net Active Park Area (Acres)	Undeveloped Usable Area	Park Description and Facilities
Meadow Park	1095 Nob Hill Avenue	1.58	1.58		Medium-size park for local neighborhood. Fully renovated in 2005.  Public art, mosaic mural, new playground, benches, and basketball courts.
Pinon Park	1600 Primrose Lane	0.66	0.66		Small park for local neighborhood. Fully renovated in 2002. Picnic tables, benches, BBQ facilities, and new play area.
Unimproved Open P	ublic Space Are	as			
Hugh Young Park	3760 Victor Street			7.92	Undeveloped open public area.
Sarah Drive Park	1850 Sarah Drive			10.22	Natural habitat for deer and wildlife. Undeveloped open public area.
View Park	2021 Henry Avenue			2.37	Undeveloped open public area. Includes walking trail up to reservoir.
Henry Avenue Public Open Space				10.82	

Total number of parks - 14

Net Active Park Acreage in Pinole – 41.9

Total Undeveloped Usable Acres – 235.12

Source: City of Pinole Recreation Department, November 2006; City of Pinole Recreation Park & Facility 5-year Master Plan, March 2007

### **Regional Parks in Pinole**

Wilson Point East Bay Regional Park along San Pablo Bay is owned and managed by the East Bay Regional Park District (EBRPD) and is located at the end of Pinole Shores Drive. The regional park encompasses 64.5 acres and includes extensive walking trails, a drinking fountain, and benches along the walking paths.

### **Community Parks in Pinole**

Fernandez Park is located in Old Town Pinole and hosts City and other community events adjacent to the Youth and Senior Centers. The basketball court and baseball field at Fernandez Park are lighted and heavily used year-round. Pinole Valley Park is the second primary community park in the City and contains most of the City's athletic fields. Pinole Valley Adobe Grove is primarily used by residents who rent the picnic areas. There are plans to develop a community garden at

Fernandez Park where young people can learn to grow food from Senior Center volunteers who will share their botanical knowledge with younger residents.

### **City of Pinole Trails**

The City of Pinole Recreation Department maintains both regional and local pedestrian and hiking trails for recreational use by residents and visitors. There are various hiking and pedestrian trails within Pinole's open space park preserves. Paved pedestrian trails include Pinole Creek Trail (from I-80 to Bayfront Park), portions of the Bay Trail (1/6 of a mile) located at the end of Pinole Shores Drive and (1/4 of a mile) around Bayfront Park, and Sarah Drive Trail (1/6 of a mile) leading to Hoke Court and Stewart Elementary School.

The City of Pinole Recreation Department is assessing trail resources for the purpose of creating a map that includes a brief list and description of trails with directions to each trail.

The General Plan (1995) identifies potential enhancements to the trails along Pinole Creek, which include:

- Development and patrol of Pinole Creek access near the Pinole Valley High School, and
- Development of a trail on the Bundy parcel.

In addition to the trail along Pinole Creek, the General Plan suggests that a route be developed to provide a continuous trail from the City's Bay shore to the City's ridges and the East Bay Municipal Utility District (EBMUD) watershed property. The General Plan suggests a loop that begins at Pinole Valley High School and extends to Duncan Canyon, crosses Rancho Road and alongside Galbreth Road or Pinole Valley Road to Adobe Road in Pinole Valley Park.

### The Bay Trail

Senate Bill 100, which was passed into law in 1987, directed the Association of Bay Area Governments (ABAG) to develop a plan to create a planned recreational corridor that circumnavigates the San Francisco Bay. Both the City of Pinole and the East Bay Regional Park District acquired parcels to contribute to the shoreline trail in the East Bay. This shoreline trail from Point Wilson trail and the Bayfront Park treatment plant along San Pablo Bay serves as an integral section of the Bay Trail and helps connect existing and planned Bay Trail segments.

In 2000, through a collaborative effort involving the East Bay Regional Park District and the cities of Pinole and Hercules, a half-mile segment of Bay Trail was constructed linking two regional parks

along the Bay. The project included construction of a paved pathway, bike lanes, and a bridge over Pinole Creek. These new amenities provide a connection to the existing spur trail around Bayfront Park and link downtown Pinole to the waterfront by linking to the Pinole Creek spur trail.

The General Plan identified future dedications that could extend the development of the Bay Trail further. These areas include:

- Dedications alongside Sunnyview Drive and Bay View Farm Road that could provide a connection between the shoreline and the utility easement to Pinon Park, and
- County and West Contra Costa County School District holdings near Garrity Creek, along with undeveloped private land on Tara Hills Drive.

### Park Ratio per City Residents

Pinole has a total of 41.9 acres of active community and neighborhood parks, equating to a parks ratio of 1.8 acres/1,000 City residents. This ratio is less than the National Recreation Association standard of 2.5 acres/1,000 residents and the Quimby Act standard of 3 to 5 acres/1,000 residents.

### **Build-Out Conditions**

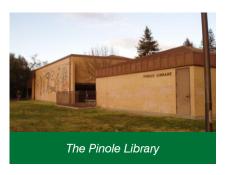
The City's Parks and Recreation Department identified major infrastructure improvement projects in the Recreation Park Facility 5-Year Master Plan. Park infrastructure improvements include Fernandez Park playground and bathroom replacement, Pinole Valley Park playground replacement, Pinole Youth and Senior Garden Walking Trail, resurface and drainage improvement of Pinole Valley Tennis Courts, Pinole Valley Park baseball field parking lot improvement, Tiny Tots facility upgrades and playground pad replacement, Fernandez Park baseball field major renovation, and Pinole Valley Park baseball field improvements.

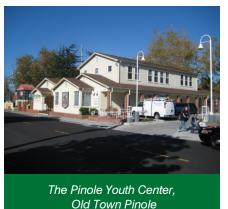
### **COMMUNITY FACILITIES**

### Libraries

### **Existing Conditions**

There is one library within the City of Pinole. The Pinole Library is a branch of the Contra Costa County Library system and is located at 2935 Pinole Valley Road. This 17,098 total square foot library sits on a 2.85-acre parcel of land and was renovated in 2002.





The library hosts a wide variety of weekly and monthly children, teen, and adult programs. The library has one meeting room that can be reserved for community use and is fully automated with free Internet access for the public.

### **Build-Out Conditions**

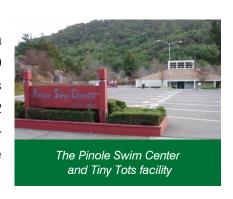
The City does not expect problems with meeting the needs of the estimated build-out population.

## YOUTH, CHILD CARE, AND RECREATIONAL FACILITIES Youth Facilities and Services

The Pinole Youth Center is located at 635 Tennent Avenue in Pinole and is overseen by the City of Pinole Recreation Department. The center's facility was recently built and is adjacent to Memorial Hall. The Youth Center offers programs for youth in sixth through twelfth grades including PY Zone, an after-school program for sixth through eighth graders; Night Out, an evening program for sixth through eighth graders; the Pinole Youth Commission for grades 6 through 12; Project ICE, a teen summer camp; workshops and classes; and field trips. The center includes a 40-person meeting room, a main recreation room, a study room, nine computer labs, a snack bar, and back patio.

#### **Child Care Facilities and Services**

The City of Pinole Recreation Department oversees Tiny Tots, a child care service and facility. Tiny Tots is located at 2450 Simas Avenue, adjacent to the Swim Center. Tiny Tots offers two preschool and pre-kindergarten programs for children 2 years 9 months to 5 years of age. Activities include age-appropriate academics, crafts, cutting with scissors, using glue and paste, painting, circle time, show and tell, music, science, and storytime.





### **Recreational Services and Facilities**

The Pinole Valley Tennis Courts are located at 2955 Pinole Valley Road and include three tennis courts which are open year-round.

The Pinole Swim Center, a City of Pinole Recreation Department facility, is located at 2450 Simas Avenue and is open April through October. The programs at the Swim Center are managed by Hilltop YMCA.

#### **Arts and Cultural Facilities**

The City of Pinole Recreation Department oversees arts and cultural facilities in the City, including the Pinole Playhouse (Memorial Hall), which is located at 601 Tennent Avenue in Pinole. The playhouse is a community theater which is utilized by the City of Pinole School of Performing Arts, the Pinole Community Players, and East Bay Improv. The playhouse facility includes a 100-person theater, a green room, storage, a snack bar, lobby, and back patio. The playhouse is connected to the Youth Center facility and is adjacent to Fernandez Park. The School of Performing Arts offers children's and youth programs including the Children's Theater Workshop and the Youth Acting Company.

The Pinole Community Players (PCP), a 30-year-old Pinole-based theater organization, performs their season at the Community Playhouse. The Pinole Young Actors Group and the East Bay Improv group also use the Pinole Playhouse facility.

Pinole Artisans is a group of artisans in the City whose purpose is to form a network, provide art education, and provide opportunities for artisans to show their work. Members of the Pinole Artisans have an opportunity to promote their artwork through the group's newsletter and website.

#### **Build-Out Conditions**

The City does not expect problems with meeting the needs of the estimated build-out population.

### Storage

### **Existing Conditions**

The City of Pinole is presently experiencing a shortage of space for storing files and materials related to governmental functions, and equipment and machinery related to capital projects and the provision of emergency services.

### **Build-Out Conditions**

The City expects this problem to be exacerbated as a result of expanded government responsibilities associated with meeting the needs of the estimated build-out population.

### 9.9 STREETS

The City of Pinole, Public Works Department, operates and maintains public streets within the City. Public streets include sidewalks, bicycle lanes, landscaping, street lighting, and other public facilities and amenities located within the right-of-way. There are two exceptions to these maintenance responsibilities, certain street lights are maintained by PG&E, and traffic signals are maintained by Contra Costa County. In addition to annual capital maintenance programs, the City periodically undertakes project to enhance these facilities in response to community objectives, and/or identified safety considerations.

### **Build-Out Conditions**

The City does not expect problems with meeting the needs of the estimated build-out population.

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### 10.1 INTRODUCTION

This Chapter describes the actions required to implement the policies of the Specific Plan in order to revitalize the San Pablo Avenue, Pinole Valley Road and Appian Way corridors, and improve the quality of life for Pinole residents, employers, employees and visitors. Implementing the Specific Plan will require investment in streets, transit, parks, plazas, and improvements to City services such as utilities, police, fire, health and human services. This investment will come from partnerships between the private sector and the City in support of new development. The Implementation Chapter identifies administrative actions, capital projects, and programs that provide the critical link between the plan vision and tangible improvements.

### 10.2 SPECIFIC PLAN ADMINISTRATION

This section describes the sequence of the key implementing steps the City should follow to effectively execute the Specific Plan. This list of steps is divided into two parts: actions completed as part of Specific Plan Adoption and actions that will be implemented after Specific Plan Adoption.

#### IMPLEMENTATION ACTIONS COMPLETED AS PART OF SPECIFIC PLAN ADOPTION

- Certify the Environmental Impact Report for the City of Pinole General Plan 2030 and Specific Plan for San Pablo Avenue, Pinole Valley Road and Appian Way;
- Adopt findings, mitigation measures, and monitoring programs as required by the California Environmental Quality Act (CEQA); and
- Adopt land use designations and Policies in the General Plan that allow for the establishment of the Specific Plan zoning districts and adopt the General Plan and Specific Plan concurrently.

#### IMPLEMENTATION ACTIONS TO BE COMPLETED AFTER SPECIFIC PLAN ADOPTION

 Adopt Zoning Code to allow for the Specific Plan to be implemented in conformance with the General Plan;

- Set up specific financing plans based upon the general financing options and strategies in the Specific Plan for major capital improvements required to support development in the Specific Plan area – San Pablo Avenue, Pinole Valley Road and Appian Way; and
- Specific Plan Administration, as identified in Chapter 6 Land Use and Development Standards, which includes review and approval of projects in accordance with the provision of the Specific Plan for San Pablo Avenue, Pinole Valley Road and Appian Way, as described in the section below.

### 10.3 PLAN ADOPTION

The Specific Plan establishes the land use and development standards for the Specific Plan area – San Pablo Avenue, Pinole Valley Road and Appian Way.

### RELATIONSHIP TO THE GENERAL PLAN

The Three Corridor Specific Plan is intended to implement the General Plan for the Specific Plan Area by establishing land use designations (Chapter 6 – Land Use Standards) and development standards for private (Chapter 7 – Private Realm Design Guidelines) and public (Chapter 8 – Public Realm Standards) properties within the Specific Plan Area..

#### RELATIONSHIP TO THE ZONING CODE

The Specific Plan is designed to supersede selected provisions of the Zoning Code. Where the Specific Plan establishes administrative practices, land uses and/or development standards, the Specific Plan shall govern. Where the Specific Plan is silent on land use or development issues, development and land use shall conform to the Zoning Code. In particular, the Specific Plan relies on the Zoning Code to govern the development review process, nonconforming uses and structures, enforcement, legal procedures, and penalties. When there is a conflict between the Specific Plan and Zoning Code, the Specific Plan shall govern.

### 10.4 PLAN ADMINISTRATION

The Community Development Director is responsible for the administration, implementation, and enforcement of this Specific Plan. The Community Development Director may delegate authority to designated staff members. In the event that the Community Development Director position is vacant, the City Manager is responsible for administering the Specific Plan, and may delegate authority to designated staff. Throughout this Chapter the term Community Development Director refers the Director or his/her designee. The Community Development Director is responsible for making consistency determinations with the Specific Plan, and for determining when an amendment to the Specific Plan text or maps is needed according to the following procedures:

#### SIMILAR USES

Recognizing that every conceivable use cannot be identified in this Specific Plan and, anticipating that new uses will evolve over time; this section gives the Community Development Director authority to approve uses when their operation, impacts, and scale are similar to those uses listed in this Specific Plan. In making a determination that a use is "similar," the Director shall make all of the following findings:

- (a) The characteristics of, and activities associated with, the proposed use are equivalent to one or more of the listed uses and will not involve a higher level of activity or population density than the uses listed in the sub-district.
- (b) The proposed use is consistent with the purposes of the applicable land use designation.
- (c) The proposed use is consistent with the General Plan and this Specific Plan.

Determinations will be made in writing and will contain the facts that support the determination. The Department will maintain a record of all such determinations for public review. All determinations will be provided to the Planning Commission, City Council, City Manager, City Attorney, and City Clerk as an information item within 30 days of the Director's determination.

#### HORIZONTAL AND VERTICAL MIXED-USE

Mixed Use Development, when it occurs, should be integrated on each individual property (vertical integration) to ensure consistency with the housing, commercial and industrial objectives of the Specific Plan. Where mixed use development may more successfully accomplish Specific Plan objectives by locating different uses on separate properties, such horizontal integration may be allowed upon finding that the horizontally integrated development:

- (a) Results in housing, commercial and/or industrial development that would create similar land use distribution to that which would occur if the development was vertically integrated; and
- (b) Would result in economic development, housing, circulation sustainability, and/or community character objectives in a manner that would not be possible through vertical integration alone.

#### **NONCONFORMING USES**

Where existing uses or structures were established legally and are principally permitted or conditionally permitted within the governing Specific Plan Sub-Area and District, they are considered legal, conforming uses and/or structures. Where existing uses or structures were established legally but are not permitted by the governing Specific Plan Sub-Area and District, they are considered legal, non-conforming uses and/or structures, and are subject to the Nonconforming Uses and Structures provisions of the Zoning Code.

### **POLICY CONSIDERATIONS**

When reviewing land use and development proposals, decision makers may encounter policy conflicts where satisfaction of one policy may interfere with full satisfaction of other policies. When working to resolve such conflicts, decision makers will assign different weight to each policy to shape decisions for the betterment of the City. Though the final determination of what relative weight to assign each policy is a decision that will be reached according to the specific circumstances associated with the proposal, the following guidelines should be used direct policy consideration:

- Land uses and development proposals must be consistent with the General Plan and Specific Plan;
- As a tool that implements the General Plan, Specific Plan Policies are subordinate to General Plan Policies; and

When these guidelines do not result in clear policy direction, such matters should be referred to the Planning Commission for resolution.

#### SPECIFIC PLAN AMENDMENT

Over time, various sections of the Specific Plan may need to be revised, as economic conditions or City needs dictate. The policies presented in the Specific Plan contain some degree of flexibility, but any Specific Plan amendments must be judged by relatively fixed criteria. The California Government Code (§ 65453) states that a Specific Plan "may be amended as often as deemed necessary by the legislative body." Amendments to this Plan may be initiated by a member of the public in accordance with any terms and conditions imposed during the original approval or in accordance with any terms and conditions pertaining to City of Pinole Zoning Code.

- Proposals to amend the Specific Plan must be accompanied by detailed information to document the change required. This information should include a revised Specific Plan text (or excerpt thereof) and revised Land Use Diagram or map amendment, where relevant, depicting the amendment requested.
- Any proposal to amend the Specific Plan must document the need for such change with a written explanation.
- The City and/or applicant must provide an analysis of the amendment's impacts relative to the Environmental Document used to adopt the Specific Plan. Depending on the nature of the amendment, supplemental environmental analysis may be necessary.
   The need for such additional analysis shall be determined by the City of Pinole in accordance with the California Environmental Quality Act (§ 15162).

The Community Development Director shall within thirty (30) days of any submittal of a request to amend this Plan, determine whether the amendment is significant or insignificant. If the amendment is determined to be significant, the application shall be reviewed and considered in the manner prescribed by the Zoning Code. If the amendment is determined to be insignificant, the Community Development Director may approve or deny the application. Any decision of the Director may be appealed to the Planning Commission and/or City Council, provided said appeal is initiated within ten (10) working days of receipt by the applicant of written notice of the Director's decision.

#### **NECESSARY FINDINGS**

- The consideration of any proposed amendment to this Plan shall include the determination of the following findings:
- Changes have occurred in the community since the approval of the original Specific Plan which warrants approving the proposed amendment.
- The proposed amendment is consistent with the General Plan for the City of Pinole.
- The proposed amendment will not be incompatible with or detrimental to adjacent properties.
- The proposed amendment will facilitate the delivery of public services and/or improve access to public facilities serving the population within the area of this Specific Plan.

The proposed amendment will enable the delivery of services and public facilities to the population within the area of this Specific Plan.

The Community Development Director will review all of the above-submitted application amendment material and provide a staff report for consideration by the Planning Commission and City Council. The Director may require submission of further amendment application material needed to make a determination with respect to the above findings. The staff report will analyze whether the proposed Specific Plan Amendment is consistent with the General Plan and whether the need to amend the Specific Plan can be supported by the application material provided.

### **PUBLIC HEARINGS**

If the amendment is considered significant, both the Planning Commission and the City Council must hold Public Hearings on the Specific Plan Amendment, in accordance with § 65453 of the State Government Code.

#### SPECIFIC PLAN REVIEW

Development standards are mandatory requirements that must be satisfied for all new projects. Qualifying building renovations may be exempt from review as a minor modification. More substantive modifications and renovations are subject to review as outlined below:

- <u>Exemptions</u>: Exemptions include minor modifications, such as painting, maintenance or repair, re-roof, and modifications that increase square footage by less than 200 square feet (within any 2-year period).
- <u>Major Modifications:</u> Major modifications are defined in this Specific Plan as any alterations of more than 10% of an existing building façade or more than 10% increase in existing building square footage.
- Renovations: For expansion or other renovation applications, the Community
  Development Director retains authority to determine major and minor modifications,
  exemptions, and give relief from Specific Plan standards based on the determination
  that they are safe and have no significant adverse impacts upon surrounding property.
  The Community Development Director may refer proposed renovations to the Planning
  Commission for review and approval.
- General Height Requirements and Exceptions: Height requirements are provided for each Specific Plan area are included in sections 6.5, 6.6 and 6.7, respectively. The Community Development Director may review and approve architectural features such as tower elements, elevator service shafts, and roof access stairwells that extend above the height limit by less than 10%. The Planning Commission shall review architectural features that extend above the height limit by 10% or more. Telecommunications antennas and service structures located on rooftops may also

exceed the maximum building height but shall be hidden to the maximum extent possible using appropriate screening and concealing technologies and shall require a use permit in each Specific Plan Area. On the recommendation of the Planning Commission, the City Council may approve buildings that exceed the maximum height adopted in any district in conjunction with a variance application request.

### 10.5 IMPLEMENTATION PLAN

Implementation tools provide strategies to the City to shape development patterns, protect natural and human infrastructure and protect the quality of life for residents. They can be separated into four distinct categories. Each category has distinct characteristics that make it suitable for specific goals and circumstances. Following are the categories that are available to the City's implementation strategies.

#### **REGULATORY ACTIONS**

City ordinances, including zoning, subdivision, environmental, and other standards, as well as the administrative approvals process.

### Regulations include:

- Performance standards that regulate development impacts.
- Administrative standards such as information or analyses required for a development.
- Application and the order and timing of approvals by regulating authorities.

### **PUBLIC IMPROVEMENTS**

Public investments and management decisions for infrastructure, public services, public lands, and public processes. Such investment or decisions can include:

- Coordinating capital improvements planning between departments to meet General Plan and Specific Plan goals.
- Water, wastewater, and transportation investments made by the City or ultimately managed by the City.

- Land acquisition, sale, or exchange for the purpose of preservation or development.
- Management decisions and expenditures for public resources such as streets, parks and property management, and deployment of public services.
- Conducting studies and making decisions on enhancing or modifying the General Plan or supporting documents.

### **EDUCATIONAL AND PROMOTIONAL PROGRAMS**

Formal programs or informal efforts undertaken by the City or in conjunction with the City to encourage voluntary actions by individuals or businesses that help fulfill the City's desired future as described in the Specific Plan and General Plan.

### INCENTIVES AND INCENTIVE REGULATION

Incentives offered by the City or in conjunction with the City to elicit actions by individuals or businesses that move the City toward its desired future. The incentives can include:

- Direct financial assistance such as cost sharing
- Indirect financial assistance such as tax breaks or infrastructure improvements
- Regulatory incentives such as density bonuses or flexible regulation
- Professional assistance incentives such as technical assistance in façade or infrastructure design, integration of historic elements, or meeting State or Federal regulatory standards

**Table 10-2** summarizes the recommended incentives program that may be adopted by City Council resolution.

Rarely will a single tool or category of tools be sufficient to achieve General Plan and Specific Plan goals. Most policies, and most of the preferred mix of land uses shown on the land use map, require the use of several tools from different categories in order to be realized and sustained. The City must take a 'comprehensive' approach in its

implementation choices, recognizing that each category of tools has unique strengths and weaknesses.

The following section presents the strategies that will be needed to implement the Specific Plan's land use recommendations. The Potential Incentives and Funding Mechanisms immediately follow.

### 10.6 IMPLEMENTATION ACTION PLAN MATRIX

The Implementation Action Matrix presents an inter-related framework for achieving the future condition represented by the land use map and the Specific Plan policies. The vision and goals presented in the Specific Plan for San Pablo Avenue, Pinole Valley Road, Appian Way are supported by the Implementation Action Plan Matrix in **Table 10-1**. The Action Plan provides a summary of Specific Plan recommendations and is presented in a table format providing a clear listing of the major programs, projects, and actions needed for implementation. The table also identifies the responsible agency or party, potential funding sources associated with each action (if warranted), and suggested timing of the actions. The table is organized to discuss regulatory actions, programs, and improvement projects for the entire Plan Area with specific direction for each corridor. The following matrix actions in Table 10-1 do not represent the entire universe of strategy options, but provide a series of priorities that support Specific Plan policies.

### **PHASING**

The Specific Plan has been designed to take advantage of development opportunities as market conditions and funding opportunities present themselves. Consequently, no corridor, sub-area or opportunity site identified in the Specific Plan has priority over any other area. Instead, the Implementation Chapter has assigned time horizons to projects and programs (e.g. short-range, mid-range, and long-range) that reflect current priorities. As economic conditions and trends change over time, the City will want to revisit and reorganize these implementation steps to optimize available opportunities. The tools and administrative procedures in this implementation plan should take into account changing market and development conditions and should be used to accommodate new

development opportunities that facilitate the revitalization of San Pablo Avenue, Pinole Valley Road and Appian Way.

Table 10-1
Implementation Action Plan Matrix

Timing: 1 = Short-Range (1-3 years), 2 = Mid-Range (4-7years), 3 = Long-Range (8+yrs), 4 = Ongoing Responsible Department: CD = Community Development, PW = Public Works, R = Recreation LAND USE AND REGULATORY ACTIONS	Timing	Responsible Department Lead	Possible Funding
Preparation of CEQA Document and Adoption of Specific Plan	1	CD	Redevelopment
Review and adoption of recommended incentives	1	CD	N/A
Create a policy for requiring reciprocal access agreements between parcels	1	CD/PW	N/A
Require all development to minimize land disturbance activities, such as vegetation clearing and grading to reduce erosion potential, sediment loss and soil over compaction that prevents water absorption.	4	CD	Developer Fees
Require all new development to preserve natural drainage features and vegetation to the maximum extent practical or to otherwise maintain predevelopment site hydrology by using site design techniques that store, infiltrate, evaporate or detain runoff. All new development shall comply, at minimum, with current municipal National Pollutant Discharge Elimination System (NPDES) requirements for peak flow, stormwater quality and runoff volume.	4	CD/PW	Developer Fees
<ul> <li>Where existing facilities are inadequate, new development shall only be approved when the following conditions are met:</li> <li>1. The developer and/or City can demonstrate that all necessary public facilities will be adequately financed and installed prior to project occupancy (through fees or other means); and</li> <li>2. The facilities improvements are consistent with applicable facility plans approved by the City or other agencies in which the City is a participant.</li> </ul>	4	CD/PW	Developer Fees/Grants

Timing: 1 = Short-Range (1-3 years), 2 = Mid-Range (4-7years), 3 = Long-Range (8+yrs), 4 = Ongoing	Timing	Responsible Department	Possible
Responsible Department: CD = Community Development, PW = Public Works, R = Recreation		Lead	Funding
ECONOMIC DEVELOPMENT			
Create a "Green Industry" program for Industrial areas of Pinole that includes:  1. Incentives and streamlined administrative review of eligible uses;  2. Support for synergistic relationships between industrial uses.	1	CD	Redevelopment/ Grants
Create a program that offers incentives and streamlined administrative review to support and encourage development: that  1. Would be located at Specific Plan Opportunity Sites; and/or  2. Furthers the objectives of the Priority Development Area.	1	CD	Redevelopment
Work with the owners of property located adjacent to Pinole Creek to ensure adequate separation between development and the Creek while promoting vertical and lateral access to the creek.	4	CD	N/A
Include projects in the Capital Improvement Program (CIP) to implement the Specific Plan streetscape improvements for Old Town. These improvements would include:  1. Uniform street furniture and amenities;  2. Construction of generous sidewalks;  3. Bus stop amenities; and  4. Bicycle Lanes.	4	CD	BID/RDA/ Grants
<ol> <li>Develop and implement a Parking Management Plan for Old Town that:</li> <li>Defines adequate parking as 15% vacancy within a two block radius of a project;</li> <li>Includes provisions for delivery vehicles, disabled parking, and other restricted parking facilities;</li> <li>Provides for parking controls (e.g. time limits, metered parking, etc.) before parking supply is expanded; and</li> <li>Identifies conditions under which shared parking should be expanded to address demand.</li> </ol>	3	CD/PW	Redevelopment/ Grants/BID
<ul> <li>Create and implement a Marketing Strategy for:</li> <li>1. The City of Pinole to identify City amenities and role in the regional market;</li> <li>2. Old Town that describes the land use and redevelopment opportunities created by the Specific Plan adoption, and communicates the vision of Old Town as a vital area of commerce.</li> </ul>	4	CD	Redevelopment

Timing: 1 = Short-Range (1-3 years), 2 = Mid-Range (4-7years), 3 = Long-Range (8+yrs), 4 = Ongoing Responsible Department: CD = Community Development, PW = Public Works, R = Recreation	Timing	Responsible Department Lead	Possible Funding
ECONOMIC DEVELOPMENT			
Continue to identify opportunity sites for the following uses in order to increase the number of people living, working and recreating in the Specific Plan Areas, esp. Old Town:  1. High-quality infill housing;  2. Office, retail and restaurant development;  3. Visitor-serving facilities; and  4. Cultural and entertainment venues.	4	CD	Private Investment/Grants/ Redevelopment
Continue to fund redevelopment projects in Old Town, including revitalization of City and RDA-owned sites identified in Specific Plan.	4	CD	Redevelopment/ Grants
Encourage and facilitate land assembly for odd-shaped and elongated parcels to provide infill opportunities that enhance civic life by defining the public realm through street and pedestrian-oriented frontages.	4	CD	Redevelopment/ Grants
Devise and implement an employment strategy to maintain the long-term economic vitality of Old Town, San Pablo Avenue, Pinole Valley Road and Appian Way by matching the range and types of jobs with the skill set and earning potential of workers and residents.	4	CD	Redevelopment/ Grants
Monitor the production and pace of new development through the issuance of building permits. The following shall be reported periodically to the City Council:  1. Production and pace of new residential units;  2. Production and pace of new commercial growth, identifying total square feet of retail, office and visitor-serving (hotel) space; and  3. Total number of planning permits approved and applications pending.	4	CD	Redevelopment
Continue to partner with non-profit organizations, private housing developers and public agencies to facilitate production of:  1. A range of affordable housing;  2. Live-work spaces;  3. Co-housing opportunities; and  4. Assisted living facilities.	4	CD	Private Investment/ Redevelopment/ Grants
Maintain an inventory of vacant and under-utilized parcels and provide the inventory to interested developers in conjunction with information on available financial assistance programs and development incentives, such as reduced parking requirements.	4	CD	Redevelopment

Timing: 1 = Short-Range (1-3 years), 2 = Mid-Range (4-7years), 3 = Long-Range (8+yrs), 4 = Ongoing	Timing	Responsible Department	Possible
Responsible Department: CD = Community Development, PW = Public Works, R = Recreation		Lead	Funding
CIRCULATION			
Prioritize capital improvements within the Specific Plan area and create a schedule of implementation activities that will provide enhanced multi-modal, pedestrian friendly access in Old Town, and along the Specific Plan Corridors. The high priority improvements should be included in the CIP	4	PW	Developer Fees, Grants/Redevelop ment
Evaluate opportunities for improving the circulation system in Old Town and San Pablo Avenue as part of the approval of new projects, including street, sidewalk, bikeway, transit and other enhancements.	4	CD/PW	Developer Fees/ BID/Redevelop ment
Construct medians along Pinole Valley Road near Pinole Valley Shopping Center & other locations as identified in the Specific Plan.	2	PW	Redevelopment/ Grants
Construct medians along Appian Way near High School & other locations as identified in the Specific Plan	2	PW	Redevelopment/ Grants
Reconstruct Appian Way from Tara Hills Drive to San Pablo Avenue.	3	PW	Redevelopment/ Grants
Construct bike and pedestrian paths and trails as identified in Pinole Creek Master Plan	4	CD/PW	Redevelopment/ Grants
Construct warning features and other safety devices to improve pedestrian safety in Old Town.	2	PW	Redevelopment/ Grants
Reconstruct the traffic signal on San Pablo Avenue at Appian	2	PW	Redevelopment/ Grants
Evaluate the feasibility and effectiveness of a shuttle or looped bus service that connects key destinations within Old Town to other areas in the City.	2	PW	Federal Grant
Encourage on-street commercial truck loading in Old Town before 11 AM to ensure daytime parking remains available for business patrons and to facilitate pedestrian movement. As necessary, provide adequate truck parking and loading areas to reduce loading activity interference with traffic flow or disruption of the general pedestrian ambience.	4	PW	General Funds
Study and implement parking structure in Old Town	3	CD/PW	Redevelopment/ Grants
Explore formation of a Parking Benefit District that dedicates parking revenue to public improvements and services within the Old Town area only. Potential net revenue uses include landscaping, trash receptacles and collection service, street cleaning, pedestrian-scaled lighting, transit and bicycle infrastructure, and management of Old Town transportation amenities and infrastructure.	1	CD	Redevelopment/ Grants

Timing: 1 = Short-Range (1-3 years), 2 = Mid-Range (4-7 years), 3 = Long-Range (8+yrs), 4 = Ongoing	Timing	Responsible Department	Possible
Responsible Department: CD = Community Development, PW = Public Works, R = Recreation		Lead	Funding
CIRCULATION			
Implement a program to maximize efficient use of existing parking lots. Such a program should initially consider improved signage and visibility, including signs that can direct overflow vehicles to alternative locations when lots are full. In part, this will be achieved through the Wayfinding signage program	2	PW	Redevelopment/ Grants
Consider a strategy and identify funding to convert under- utilized surface lots to new uses such as retail, commercial or residences.	2	CD	As funding allows
PUBLIC REALM IMPROVEMENTS			
Implement the streetscape recommendations for San Pablo Avenue, Pinole Valley Road and Appian Way (contained in Chapter 5.0 Circulation). Establish a detailed implementation program that sets timeframes given available funding and community priorities. Improvements can be phased, but priority should be given to San Pablo Avenue through Old Town to re-establish the pedestrian friendly nature of Old Town and slow traffic through Old Town.	2	CD/PW	Redevelopment/ Grants/Fee Credits
Develop designs for Gateway/Entry Monuments at Pinole Valley Road and Appian Way and designate locations for the placement of public art, interpretive signage and entry signage at the following locations:  i. Entry into Pinole at the southern end of Appian Way			
near El Sobrante  ii. Entry into Specific Plan Area on San Pablo Avenue, Appian Way and Pinole Valley Road	2	CD/PW	Redevelopment/ Grants/Fee Credits
iii. Entry into Old Town on San Pablo Avenue, Pinole Valley Road, and Tennent Avenue iv. Entry into Old Town from Hercules (City boundary)			
v. Other gateways identified by the Specific Plan			
Develop and implement wayfinding system and comprehensive signage program for Old Town and other Specific Plan Corridors, providing informational kiosks and signage oriented to pedestrians	2	CD/PW	Redevelopment/Gr ants
Provide street trees as identified in Chapter 8 – Public Realm Design Guidelines	2	PW	General or City Funds
Develop a program, permitting system and identify funding for placement and design of a consolidated network of news racks	2	PW	Redevelopment/ Grants
Require all new development to incorporate sufficient trash and recycling receptacles and enclosures to adequately serve residents and tenants.	4	CD	Redevelopment/ Grants

Timing: 1 = Short-Range (1-3 years), 2 = Mid-Range (4-7years), 3 = Long-Range (8+yrs), 4 = Ongoing Responsible Department: CD = Community Development, PW = Public Works, R = Recreation	Timing	Responsible Department Lead	Possible Funding
PUBLIC REALM IMPROVEMENTS			
Continue to provide and identify funding for public bike racks and lockers.	4	PW	Redevelopment/Gr ants
Underground utilities along Pinole Valley Road	3	PW	City Funds/Grants
Revise the Quimby Fee for Downtown projects to improve parks in the Specific Plan Areas and public plazas and provide funding for development of cultural amenities.	2	CD/R	Parkland Fees/ RDA
Enhance the visual and functional link to the Pinole Creek along Old Town and Pinole Valley Road through streetscape improvements and complementary design of new development.	4	CD/PW	Redevelopment/ Grants/Private Investment
Create plazas in Old Town and other locations along San Pablo Avenue	2	CD/PW	Redevelopment/ Grants/Private Investment
Develop and implement a strategy and identify funding to implement the pedestrian linkage improvements along Pinole Creek as recommended in the Pinole Creek Master Plan	4	PW	Redevelopment/ Grants
Lighting improvements for the walking trail along Pinole Creek.	2	PW	Redevelopment/ Grants
Create and implement a storefront and façade improvement program in the Old Town area	2	CD	Redevelopment BID
Adopt and implement Crime Prevention Through Environmental Design Guidelines (CPTED) to address public safety issues through commercial and residential project design. All new developments and tenant improvements shall be collaboratively reviewed by the Pinole Police Department, Public Works and Community Development to prevent blight and improve public safety through site design.	1	CD	N/A
Identify and implement public safety improvements, including redevelopment and enhancement of existing public parks, plazas, parking areas and restrooms.	4	PW	Redevelopment/ Grants/BID
EDUCATIONAL AND PROMOTIONAL PROGRAMS			
Develop a "Virtual Front Counter" Web page on the City's Planning and/or Redevelopment Agency Web page	1	CD	Time & Materials
Undertake priority neighborhood improvement projects	1	CD	CDBG, public- private partnerships, Redevelopment
Develop and implement art in public places program	2	CD/PW	Developer Fees/Redevelop ment

Timing: 1 = Short-Range (1-3 years), 2 = Mid-Range (4-7years), 3 = Long-Range (8+yrs), 4 = Ongoing Responsible Department: CD = Community Development, PW = Public Works, R = Recreation EDUCATIONAL AND PROMOTIONAL PROGRAMS	Timing	Responsible Department Lead	Possible Funding
Create a beautification awards recognition program for San Pablo Avenue, Pinole Valley Road and Appian Way Corridor improvements	4	CD	Redevelopment/ Grants
Explore creation of a local main street program for Old Town (private non-profit organization)	2	CD	Redevelop ment/Grants
Explore establishing a Property Owner and Business Improvement District (including evaluation of boundaries and whether to include entire Old Town area or only portions)	2	CD/PW	Redevelop ment/BID
Explore preparing a Parking Management Plan for the Old Town area to establish shared parking regulations and a parking in lieu fee program	2	CD/PW	Redevelop ment/Grants

### 10.7 POTENTIAL INCENTIVES

The City of Pinole has been positive in providing development incentives for the attraction of business and revitalization of Old Town. In order to encourage desired amenities and project characteristics, the City should explore and adopt an incentives program. The costs and benefits of each incentive will be analyzed prior to a recommendation to the City Council/Agency Board for implementation and the City Council/Agency Board will review incentives following approval of the Specific Plan.

Any incentives offered to property owners within the Specific Plan boundary will not exceed the appraised value of the improvements. If the value of the improvements is in question, the City will conduct an appraisal at the property owner's expense. All incentives and public benefits will be as adopted by City Council in separate resolution. The City Council/Agency Board may amend the incentives and public benefits at any time, but not more often than once per calendar year and not less often than once per five calendar years.

**Table 10-2** summarizes the potential components of an incentives program that may be adopted by City Council resolution. The incentives can be organized into the following categories:

- Fee/tax reductions.
- Other direct financial assistance to developers and businesses.
- Entitlement incentives.
- Other incentives.

Table 10-2
Possible Incentives

Item	Description	Recommended Actions	Funding Source
Review of Plan Check Fees (phased over life of SP)	Although the City's Plan Check fees are already modest for most projects, reducing them for projects proposed in the 16 opportunity sites in the Specific Plan Areas – San Pablo Avenue, Pinole Valley Road and Appian Way would send an important message to the development community of the City's commitment to attracting desired development to Old Town and the Planning Areas.	<ul> <li>Reduce Plan Check fees for all Plan area projects submitted for review during the first three (3) years of implementation</li> <li>Revisit this issue after the first three years and consider extending the benefits</li> </ul>	Redevelopment Funds
Review of Business License Fees	As with Plan Check fees, the City's existing Business License fees are modest (\$114/year). Nevertheless, reducing them for a specified period of time (for both existing and new businesses) would send an important message to the business community as to the City's level of commitment to Old Town and the Planning Areas.	Upon adoption of the Specific Plan, give all existing businesses in the Plan area a one-year Business License tax reduction.  Offer the same incentive to all new businesses that open in the Plan area during the first three (3) years of implementation  Revisit this issue after the first three years and consider extending the benefits	Redevelopment Funds

Item FEE/TAX REDUC	Description	Description Recommended Actions	
Review of Development Impact Fees	As means of improving the Old Town's economic attractiveness, the City could revise its "Growth Mitigation Fee" schedule to provide lower fees in the Plan area. In order to be consistent with AB 1600 requirements, these fee differentials would need to be based on the cost and availability of infrastructure.	As part of the upcoming study to review and update the City's schedule of impact fees, evaluate the feasibility of adopting different fees for Old Town and other Specific Plan Areas- San Pablo Avenue, Pinole Valley Road and apian Way). Whereas it conceptually makes sense that impacts are lower (and therefore fees should be lower) in Old Town, this premise would need to be tested as part of the formal study process (as required by AB 1600).	Redevelopment Funds
OTHER DIRECT F	FINANCIAL ASSISTANCE TO DEVELOPER	RS AND BUSINESSES	
Storefront/ Façade/ Parking Lot Improvement Reimbursement/ Loan Program	This program should continue to be available to enhance Old Town and businesses along San Pablo Avenue, Pinole Valley Road and Appian Way.	Aggressively promote this program as part of the marketing process for the Specific Plan Areas esp. Old Town	Redevelopment Funds
Revolving Loan Program	The City should develop specific program guidelines for this proposed revolving loan program. The purpose of the this Revolving Loan Fund is to ultimately aid in the revitalization of Old Town, by fostering growth of new businesses and assisting expanding businesses. Its objectives are to improve the visual quality of commercial buildings and mixed uses, increase the City's tax base, and provide needed services to local residents. The program will also help preserve historically architecturally significant structures and address safety issues by eliminating code deficiencies. This loan must be used for non-residential projects only.	Periodically reevaluate     the program guidelines     to ensure the objectives     are met.	RDA funds and/or other City revenues

Item	Description	Recommended Actions Funding Source	
ENTITLEMENT INCENTIVES			
Expedited Plan Checks	This incentive will provide certainty of the City-controlled review process, and provide priority processing. Project applications will be expedited as there is no triggered EIR if the project meets the requirements of the Specific Plan.	For projects within the Specific Plan area, offer expedited processing  No cost associated with this item (cost recovery)	
City sponsored Environmental Review	The City may undertake environmental review for projects at their discretion in order to assist in expediting the development process.	RDA Funds	
Streamlined Entitlement and Environmental Documentation	The intent of this incentive is to remove – to the maximum degree possible – the need for discretionary approvals for projects that fall within the "envelope" of development established by the Specific Plan. In many communities, this is achieved through a "Program" or "Master" EIR, which essentially serves as an environmental pre-clearance document for all future projects that are consistent with the Specific Plan land uses.	<ul> <li>Investigate the various administrative options for minimizing the need for CEQA documentation for future projects proposed within the Specific Plan Area.</li> <li>As appropriate (based on the above investigations), prepare a Program EIR for the Plan area.</li> </ul>	
Parking Incentives	There is currently "surplus" parking in Old Town. This extra capacity can enable the City to lower the required parking ratios for new development, thereby substantially reducing the costs of development and giving Old Town an "edge" relative to other Planning Areas where parking requirements are higher.	The Zoning provides for reduced parking requirements as a key incentive for selected sub-areas/districts to account for an existing "reserve" of public parking.  To ensure that the lowered requirements do not result in a long-term parking shortage, this issue should be periodically reviewed.	
Height, Setback and Density Incentives	Within areas such as San Pablo Avenue, Pinole valley Road and Appian Way, that is largely built out with existing uses, land values tend to be relatively high. As a consequence, increased densities are often necessary to make new development (or redevelopment of existing uses) financially feasible. In addition to increasing allowable densities, zoning should allow the flexibility to develop desired land uses for which the future market is uncertain (e.g., live/work space).	<ul> <li>The Specific Plan provides flexibility and increased density in the land use and developments standards section.</li> <li>Monitor the effectiveness of this provision by revisiting this issue periodically.</li> </ul>	

Item OTHER INCENTIV	Description /ES	Recommended Actions	Funding Source
3-Tiered Marketing Program	The marketing effort for the Specific Plan Areas should include components:  Promoting the Old Town District as a place to shop (i.e., marketing to consumers through advertising, special events, etc.).  Promoting the opportunity sites in Old Town to specific retail tenants (i.e., tenant recruitment).  "Branding" the opportunity sites as places to live, work, shop, and recreate.	The Redevelopment Agency will lead this effort to promote the Old Town to the general public and the Development Community as a place to invest and gather.	RDA or Business Improvement District (BID).
Area-Wide Infrastructure/A menity Investments	Area-wide infrastructure and "amenity" (streetscape) investments will serve to "set the table" creating an upgraded urban environment to encourage new private investment.	(See Chapter 5.     Circulation and 8. Public     Realm Public Realm -     Mobility and VII. Public     Realm Standards and     DGs – Gateways and     Streetscaping of the     Plan)	Redevelopment Funds
Project-Specific Infrastructure Assistance	In addition to area-wide infrastructure improvements, there may be specific development projects for which it is advantageous for the City to provide financial assistance for the development of on- or off-site or project-specific infrastructure. This type of assistance is typically negotiated on a case-by-case basis (reflecting the specific public benefits that can justify the public investment) and subject to a development agreement.	Ensure that guidelines for revolving loan fund are flexible enough to allow for funding of infrastructure related to key "catalyst" projects.	RDA funds
City/Redevelopm ent-Owned Opportunity Sites	Site assembly – the process of making sizeable development sites available to private developers – is often an important municipal tool in a redevelopment process.	Maintain an inventory of existing     City/Redevelopment-owned sites in the Plan Area, noting location, acreage and existing use.      Prioritize sites in terms of size, location, and feasibility of relocating existing uses.      Market selected site(s) via a developer request for qualifications/proposals process	Redevelopment Funds

### 10.8 FINANCING PLAN

The Specific Plan for San Pablo Avenue, Pinole Valley Road and Appian Way contains comprehensive recommendations for both physical improvements and programs. As such, a variety of funding sources will be required to implement many of these recommendations. In some cases, public/private partnerships will need to be formed to implement desired programs. The following describes various funding mechanisms that are available at the local, state and federal level that may be pursued by the City to help implement the planned public improvements. The discussion below includes examples of potential funding sources for the Specific Plan areas.

#### LOCAL FUNDING SOURCES

### **City General Fund**

This is the most accessible and flexible funding source available to local agencies. Local revenues are collected in the City General Fund from property tax, sales tax, and transient occupancy tax and expended on projects and programs as defined in the City's adopted budget. Projects and programs that may be funded by this source of money generally include those items which cannot be paid for by other funding sources and which provide a direct community-wide benefit for the residences or businesses in town. However, since this funding source is the City's primary operating capital and highly competitive, it should be looked at as a secondary source to fund most projects.

### Redevelopment Area Tax Increment Financing

The Specific Plan areas lies within a City Redevelopment Area, thus, is eligible for expenditure of bond proceeds levied against the potential increased tax increment for the area. The Pinole Redevelopment Agency has significant resources available for infrastructure projects, housing projects, planning and development studies and public/private partnerships. These projects focus on the economic development of blighted areas within the Specific Plan Areas. Most of these monies have been allocated to upgrade infrastructure facilities (storm drains, new curb and gutter), and streets (widening, bulbouts, sidewalks, lane and parking space striping). The remaining funds

should be carefully used to implement some of the higher priority programs identified in this Specific Plan.

### **Municipal Bond Financing**

Federal and State laws allow cities to issue bonds with interest payment to investors that are exempt from Federal and State income, thus allowing cities to sell the bonds at below market interest rates. Cities in turn can utilize funds for certain projects that serve a public purpose. Provided the public purpose is well defined, such bonds give cities a powerful vehicle for financing capital improvements. Bond payments for improvements within the Specific Plan areas would be secured by the formation of an assessment district. An assessment district such as a landscape, lighting, or street improvement can be formed to fund public improvements that will benefit a localized area. The City issues bonds to pay for such improvements, and the debt is paid by assessing property owners who will be served by this improvement. The individual property owner portion of the debt is based on the owner's proportion of benefit. Any method that reasonably measures these benefits can be used to spread the debt among property owners. One drawback of municipal bond financing is that it requires approval of two-thirds of the voters in a local election. Also, since the project is within the City's Redevelopment area, additional property obligations are not warranted.

### **Development Impact Fees**

While most of the Specific Plan area is built out, some of the planned improvements could be necessary because of the pressures for either new development or recent growth in the surrounding area. Therefore, it could be logical to attach an appropriate portion of the financial responsibility of these improvements to new development. A mechanism commonly utilized for funding various roadway improvements is development impact fees. Impact fees collected through this mechanism are based on the proportion of impact relative to the improvements necessary, providing a clear connection or "nexus" between development and particular improvement. Since most of the project area is built out, development impact fees will not likely be one of the primary mechanisms for paying for improvements.

#### OTHER GOVERNMENT SOURCES OF FUNDING

### **Transportation for Livable Communities Program**

The Transportation for Livable Communities (TLC) Program provides planning grants, technical assistance, a Housing Incentive Program and capital grants to help cities and nonprofit agencies develop transportation related projects.

The Transportation for Livable Communities Capital Program funds transportation infrastructure improvements for pedestrian, bicycle and transit facilities. The Metropolitan Transportation Commission (MTC) allocates federal Surface Transportation Program or Congestion Mitigation and Air Quality Improvements Program Funds toward the projects. Grants typically range from \$500,000 to \$3 million per project. A local match of 11.5 percent of the total TLC project cost is required. Project activities eligible for funding include bicycle and pedestrian paths and bridges; on street bike lanes; pedestrian plazas; pedestrian street crossings; streetscaping such as trees, lighting, furniture; traffic calming design features; signage and gateway features.

The Community Design Planning Program funds community design and planning processes to revitalize existing neighborhoods, Downtowns, commercial cores, and transit station areas and stops. The community design planning processes often lead to the development of capital projects. MTC allocates Transportation Development Act or Surface Transportation Program funds to this program. Up to \$75,000 is available per project and a 20 percent local match is required.

TLC provides funding for projects that are developed through an inclusive community planning effort. Applications are available online and there are also regional workshops to provide an overview of TLC, federal-aid process for delivery TLC projects, and TLC application process.

### Contra Costa County Measures C & J Discretionary Funds

Measures C & J are transportation sales tax initiatives that were approved by Contra Costa County voters. These Measures have two main elements:

- The Expenditure Plan administers the distribution of sales tax revenues to transportation programs and projects in the county.
- The Growth Management Program attempts to sustain the expenditure plan's investments by laying out certain requirements that cities and the county must meet in order to receive their share of "Local Street Maintenance and Improvement" funding.

The sales tax is expected to be distributed to specific transportation programs and projects in the county as follows:

- 41% on highways
- 28% on transit
- 0.4% on trails
- 30% on other programs

The City of Pinole could use Measure C funds to improve pedestrian and bicycle paths, highways and transit.

### **Quimby Act and Parks Reimbursement**

Cities and Counties have been authorized since the passage of 1975 Quimby Act to pass ordinances requiring developers set aside land, donate conservation easements, or pay fees for park land and improvements. Revenues generated through the Quimby Act cannot be used for the operation and maintenance of park facilities. The act ensures open space acreage in jurisdictions adopting Quimby Act standards of 3-5 acres per 1,000 residents.

#### **PRIVATE INVOLVEMENT**

### **Private Financing**

Private developers may construct many of the public facilities that are required to serve the Area Plan, using cash, funds from private investors, lines of credit, conventional lending sources, and other sources of private financing.

### **Private Cost Sharing/Reimbursement Agreement**

Public facilities that directly benefit other properties within the Area Plan pay their proportionate share of costs upon some equitable factor. In the case where one property would develop first and construct public facilities benefiting additional properties, the provider of shared infrastructure will be entitled to reimbursement of costs.

### In-Lieu Development Fee

In lieu of payment of all or a portion of development fees, developers may dedicate land to the City of Pinole for other purposes such as parks or affordable housing. Whenever a developer determines to dedicate land in lieu of payment, a written application must be made describing the property to be dedicated and the development to receive credit for the development fee. If the land is used for a park, City staff prepares a report to the City Council regarding the proposed dedication. The value of the property to be dedicated must be determined in the same manner as the then current calculation of the average cost of parkland for the Local Park Development Fee.

### Impact Fees and Exactions:

Where new development could create impacts from increased population or demand on services, dedications of land and impact fees are exactions which are used to reduce the impact. Such exactions must (a) "advance a legitimate state interest" (such as protection of the public health, safety, and welfare) and (b) mitigate the adverse impacts that would otherwise result from a project. Impact fees must also identify the purpose of the fee and the use to which it will be put. The local agency must also specify the nexus between the development project and the improvement being financed, and must establish that the amount of funds being collected will not exceed that needed to pay for the improvement.

#### **ASSESSMENT DISTRICTS**

Assessment Districts are most commonly established to finance the construction of public capital improvements and where authorized, to operate and maintain costs of certain public facilities. Assessment districts are formed in two different ways: (1) Property owners petition the appropriate public agency to form a district and provide a needed public

improvement, or (2) A public agency foresees the need for an improvement and approaches the affected property owners with an assessment district proposal.

### **Property and Business Improvement Districts**

A property and business improvement district (PBID) is a newly established benefit assessment district that can be formed based upon state legislation known as AB3754. PBID districts are geographically defined business areas in which private property owners band together to gain legal standing and generate sufficient revenue to realize common goals for improving the service and facilities of an area. All types of businesses and some residential zones can be included within this district including commercial, professional office, finance institutions, and high density residential. The PBID can perform a number of activities designed to supplement existing City services; such as marketing district businesses and activities, promotion of public events, street and sidewalk cleaning, graffiti removal, promotion of tourism, sanitation, retail retention and recruitment. Physical improvements such as benches, kiosks, pedestrian shelters, signs, lighting, restrooms, trash receptacles, planting area, fountains, plazas, etc., are often also funded by a PBID.

Within areas of the Specific Plan area, the following activities would be ideally suited for BID funding:

- Marketing activities (including planning, media/banner development, special events organization, and ongoing advertising);
- Landscape and streetscape improvements; and
- Parking improvements (see additional discussion below).

There are several legal forms of BIDs authorized by California law. The most common are districts formed under the Parking and Business Improvement Act of 1989. Business Improvement Areas (BIAs) formed under the 1989 law impose a fee on the business licenses of the businesses (rather than the property owners) operating in the area. The collected funds are used to pay for the improvements and activities specified in the formation documents.

A similar assessment procedure was authorized by the Property and Business Improvement District (PBID) Law of 1994. The distinction is that the PBID makes the assessment on the real property and not on the business itself.

This is a potential funding source that may be appropriately utilized in Old Town. Establishing a PBID in this area may be difficult until more properties develop and Old Town business sales improve, and should be reconsidered as a likely future source of revenue.

Given the size and diversity of the Specific Plan area, it may be appropriate for separate BIDs or PBIDs to be explored for different sub-areas in the Specific Plan area. In this way, the collected funding could be more specifically targeted to the unique improvement and programmatic needs of each sub-area.

### **Landscape and Lighting District**

The City of Pinole can establish landscape and lighting districts to maintain landscape and lighting in sub-areas of the city, the maintenance cost being paid for by assessments on property owners within each district. These districts are based on land use type and are used to supplement maintenance costs. The Landscaping and Lighting Act of 1972 (Streets and Highway Code section 22500) enables assessments to be imposed in order to finance:

- Acquisition of land for parks, recreation, and open space
- Installation or construction of planting and landscaping, street lighting facilities, ornamental structures, and park and recreational improvements (including playground equipment, restrooms and lighting)
- Maintenance and servicing any of the above.

However, creation of new districts is highly unlikely since the passage of proposition 218, which requires a 2/3-majority voter/landowner approval for formation. Today, the existing residential neighborhoods surrounding Old Town are generally comprised of moderately priced homes. It is unlikely that these owners would be willing to support additional tax assessments.

#### **Maintenance Assessment District**

Maintenance Assessment Districts (MADs) are authorized in the "Landscape and Lighting Act of 1972." MADs usually fund:

- Maintenance services, construction and installation.
- Open space and mini-parks
- Street medians and street lighting,
- Security
- Flood control and drainage.

### **Parking Assessment District**

The Parking District Law of 1943 (Streets and Highways Code section 31500) authorizes a city or county to finance the following acts:

- Acquisition of land for parking facilities (including the power of eminent domain)
- Improvement and construction of parking lots and facilities
- Issuance of bonds
- Employee salaries

### Infrastructure Financing District (Government Code 5395 et seq.)

This is a new mechanism. It allows the use of tax increment funds to help pay for infrastructure (may not include any area in a redevelopment project area). This can increase the funds available for infrastructure without additional burden on the property, but at the expense of the general fund. It may have limited potential, as each agency sharing in the property tax revenues may veto the use of its portion of the tax increment. Even motivated agencies would be taking what would otherwise be general fund revenues and spending them on infrastructure.

# 11.0 DEFINITIONS

### 11.1 DEFINITIONS

The following definitions describe unique terms used throughout this document for the purpose of establishing a clear and common understanding.

**Accent Trees.** Trees used to supplement the required street trees.

**Adult Day Care Home.** Pursuant to definitions of State law, an adult day care home is a home which provides supervision and non-medical care to six or fewer adults, including elderly persons, in the provider's own home, on a less than 24-hour basis.

**Adult-Related Uses.** Those uses defined and regulated by Title 17, Chapter 17.56 of the City's Zoning Code.

**Alcoholic Beverage Sales.** The retail sale of beer, wine, and/or other alcoholic beverages for on- or off premise consumption.

**Alley.** Alleys are narrow private drives serving commercial and residential development.

Alley Access Parking. Residential or commercial parking that is accessible from an alley.



Arcade Frontage. An Arcade frontage is nearly identical in character to the Gallery frontage except that the upper stories of the building may project over the public sidewalk and encroach into the public right-of-way. The sidewalk must be fully absorbed within the colonnade so that a pedestrian may access it. This frontage is typically for retail use. An encroachment permit is needed to construct this frontage type, but can be approved as part of Design Review.



**Articulation.** The manner in which portions of a building form are expressed (materials, color, texture, pattern, modulation, etc).

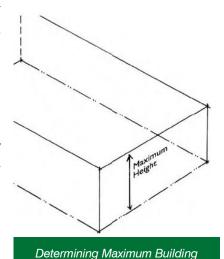
Arterials. Arterials provide primary connections between major areas within the City of Pinole and also distribute traffic between adjacent communities. In addition, arterials provide considerable statewide and interstate circulation. Speed limits often range from 30 to 50 mph.

Auto Parts Sales. Stores selling new and used automobile parts, tires, and accessories. May also include minor parts installation (see "Vehicle Services"). Does not include tire recapping establishments, which are found under "Vehicle Services" or businesses dealing exclusively in used parts, which are included under "Recycling - Processing."

Banks and Financial Services. Financial institutions such as banks and trust companies, credit agencies holding (but not primarily operating) companies, lending and thrift institutions, and investment companies.

Also includes Automated Teller Machines (ATM).

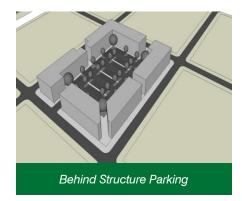
Bars and Nightclubs. Any bar, cocktail lounge, discotheque, or similar establishment, which may also provide live entertainment (e.g. music and/or dancing, comedy) in conjunction with alcoholic beverage sales. These facilities do not include bars that are part of a larger restaurant. Includes bars, taverns, pubs, and similar establishments where any food service is subordinate to the sale of alcoholic beverages. May also include the brewing of beer as part of a brew pub or microbrewery. Bars and nightclubs may include outdoor food and beverage areas.



Bed and Breakfast. Residential structures with one family in permanent residence with up to five bedrooms rented for overnight lodging, where meals may be provided subject to

Height

overnight lodging, where meals may be provided subject to applicable Health Department regulations. A Bed and Breakfast Inn with more than five guest rooms is considered a hotel or motel and is included under the definition of "Hotels and Motels."



Behind Structure Parking. Ground level public or private parking lot located in the rear yard setback behind a building. If possible, access to the parking should be taken from an alley.

**Building Height.** The vertical extent of a building measured in feet from the grade to the highest point of the coping of a flat roof or to the highest point of the highest gable of a pitch or hip roof.

**Building Materials Stores and Yards.** Retail establishments selling lumber and other large building materials, where most display and sales occur indoors. Includes stores selling to the general public, even if contractor sales account for a major proportion of total sales. Includes incidental retail ready-mix concrete operations, except where excluded by a specific zoning district.

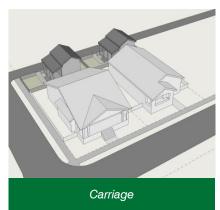
Building Type. Defines the type of structure based on massing, layout, and use.

**Build-to Line (BTL).** An urban setback dimension that delineates the maximum distance from the property line a front building façade can be placed.

Business Support Services. Establishments primarily within buildings, providing other businesses with services such as maintenance, repair and service, testing, rental, etc. Other support services include but are not limited to equipment repair services (except vehicle repair, see "Vehicle Services"), commercial art and design (production), computer-related services (rental, repair), copying, quick printing, and blueprinting services (other than those defined as "Printing and Publishing"), equipment rental businesses within buildings (rental yards are "Storage Yards"), film processing laboratories, heavy equipment repair services where repair occurs on the client site,

janitorial services, mail advertising services (reproduction and shipping); mail box services other "heavy service" business services, outdoor advertising services, photocopying and photofinishing.

Carriage House. A building type consisting of a dwelling unit on top of a detached garage. Carriage Houses typically abut an alley at the rear of a lot that also includes a Front Yard or Side Yard House.



Car Wash and Detailing. Permanent, drive-through, self-service and/or attended car washing establishments, including fully mechanized facilities. May include detailing services. Temporary car washes are typically fundraising activities generally conducted at a service station or other automotive-related business, where volunteers wash vehicles by hand, and the duration of the event is limited to one day and are not part of this use classification and not regulated by this title.

Child Day Care Facility. A State licensed facility which provides non-medical, care, protection, and supervision to more than 14 children under 18 years of age on a less than 24-hour basis. Commercial or nonprofit child day care facilities include infant centers, preschools, sick-child centers, and school-age day care facilities. These may be operated in conjunction with a school or church facility or as an independent land use. Also see "Family Day Care Home, Small" and "Family Day Care Home, Large."

Class I Multi-Use Path. Provides for pedestrian and bicycle use on a paved right-of-way separate from any street or highway. Under Caltrans standards, it must be at least eight feet wide for a two-way path.

Class II Bike Lanes. Provides for a striped and stenciled lane for one-way travel on a street or highway. A Bike Lane has a minimum standard width of four feet.

Class III Bike Route. Provides for shared use of a street with motor vehicle traffic, and may be identified only by signing and/or pavement legends. They usually are used and posted to connect other bike lane segments.

Clubs, Lodges, and Private Meeting Halls. Permanent meeting facilities for organizations operating on a membership basis for the promotion of the interests of the members, (e.g. business associations, civic, social and fraternal organizations, labor unions and similar organizations, political organizations, professional membership organizations).

**Collectors.** Collectors typically serve intra-city rather than regional circulation needs. Their primary function is to provide access to adjacent properties and connections between local roads and other roadways that are higher in the hierarchy of classification. Travel speeds on collectors often range between 25 mph and 45 mph.

Cottage Industry. Establishments manufacturing and/or assembling small products primarily by hand, including but not limited to jewelry, pottery and other ceramics, as well as small glass and

metal art and craft products. Also see "Manufacturing – Major" and "Manufacturing – Minor."

Courtyard Housing. A group of dwelling units arranged to share one or more common courtyards. Dwellings take access from the street or the courtyard(s). Dwelling configuration occurs as townhouses, apartments, or apartments located over or under townhouses. The Courtyard is intended to be a semi-public space that can be an extension of the public realm.



Door Yard Frontage. Is characterized by a façade that is set back from the street property line. This buffers residential uses from the sidewalk. Door Yard/Terrace is a variation on the Stoop frontage, but it provides opportunities for multiple levels of commercial/retail easily accessible from the street. Could also be used for a lower-level commercial use with office or residential on the second level.



**Drive-in and Drive-through sales and services.** Facilities where food or other products and services may be obtained by motorists without leaving their vehicles. These facilities include but are not limited to fastfood restaurants, drive through coffee, dairy product, photo stores, drive-up bank teller windows, and dry cleaners. Does not include automatic teller machines (ATMs), automobile service stations, or car washes, which are separately defined.

**Du/Tri/Quadplex.** A du/tri/quadplex is a building type that contains two, three, or four dwelling units. Each unit is individually accessed directly from the street.

**Dwelling Unit.** Any room or group of connected rooms that have sleeping, cooking, eating, and bathroom facilities, and are intended for long term occupation.



Duplex, Triplex and Quadplex

**Dwelling, Multifamily.** A building designed and intended for occupancy by three or more families living independently of each other, each in a separate dwelling unit, which may be owned individually or by a single landlord (e.g., apartment, apartment house, townhouse, condominium).

**Dwelling, Second Unit.** An attached or detached dwelling unit which provides complete independent living facilities for one or more persons, with permanent provisions for living, sleeping, eating, cooking and sanitation sited on the same parcel as the primary dwelling unit. This definition includes granny flats.

**Dwelling, Single-family.** A building designed exclusively for occupancy by one family on one or more lots. This classification includes manufactured homes (defined in California Health and Safety Code Section 18007) and model homes for the first sale of homes within the subdivision.

**Dwelling, Two-family.** An attached building (e.g. duplex) designed for occupancy by two families living independently of each other, where both dwellings are located on a single lot. For the purposes of this Title, this definition also includes halfplexes (two attached units, each with a separate lot). Does not include "Second Dwelling Units."

**Elevation.** A fully dimensioned drawing of the front, rear, or side of a building showing features such as windows, doors, and relationship of grade to floor level.

**Emergency Shelter.** Any facility, the primary purpose of which is to provide temporary or transitional shelter for the homeless in general or for specific populations of the homeless.

**Equipment Sales and Rental.** Service establishments with outdoor storage/rental yards, which may offer a wide variety of materials and equipment for rental (e.g. construction equipment).

**Façade.** Any vertical, exterior face of wall of a building, usually the front, often distinguished from other faces by architectural details.

Family Day Care Home – Large. State licensed facilities that provide non-medical care and supervision of minor children for periods of less than 24 hours within a single family residence. The occupant of the residence provides care and supervision generally for seven to 14 children. As described in the California Health and Safety Code, large day care homes may provide services for up to 16 children when specific conditions are met.

Family Day Care Home – Small. State licensed facilities that provide non-medical care and supervision of minor children for periods of less than 24 hours within a single family residence. The occupant of the residence provides care and supervision generally to six or fewer children. As described in the California Health and Safety Code, small day care homes for children may provide services for up to eight children when specific conditions are met.

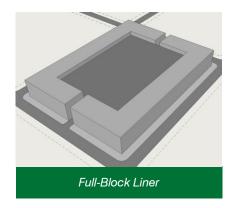
Frontage Line. A lot line fronting a street, public right-of-way, paseo, plaza, or park.

**Frontage Type.** Refers to the architectural composition of the front façade of a building; particularly concerning how it relates and ties into the surrounding public realm.

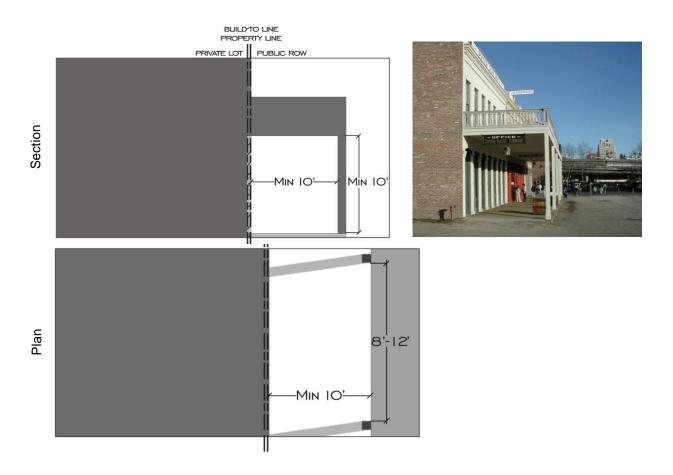
Front Yard Housing Building Type. A detached building designed as a single-family residence, duplex, triplex, or quadplex. Front Yard Housing is accessed from the sidewalk adjacent to the street build-to line.

Full Block Liner. An attached building with a frontage that spans the length of a Downtown block, and has zero side yard setbacks. It is used for mixed-use, residential, and commercial development.





Gallery Frontage. This is characterized by a façade which is aligned close to or directly on the right-of-way line with the building entrance at sidewalk grade, and with an attached colonnade that projects over the public sidewalk and encroaches into the public right-of-way. The sidewalk must be fully absorbed within the colonnade so that a pedestrian may access it. This frontage is typically for retail use. An encroachment permit is needed to construct this frontage type, but can be approved as part of Design Review.



**Green Building.** Green building is a whole systems approach to the design, construction and operation of buildings that employs materials and methods that promote natural resource conservation, energy efficiency, and a good indoor air quality.

Green Industry. Agricultural, manufacturing, research and development (R&D), administrative, and service activities that contribute(s) substantially to preserving or restoring environmental quality. Specifically, but not exclusively, this includes industries that help to protect ecosystems and biodiversity; reduce energy, materials, and water consumption through high-efficiency strategies; de-carbonize the economy; and minimize or altogether avoid generation of all forms of waste and pollution.

**Green Roof.** A green roof is a roof of a building that is partially or completely covered with vegetation and soil, or a growing medium, planted over a waterproofing membrane.

Greenhouse Gas Emissions. Gasses such as carbon dioxide, methane, and CFC's that are relatively transparent to the higher energy sunlight, but trap lower energy infrared radiation.

**Half Block Liner.** An attached building with a frontage of approximately one-third to one-half the length of a Downtown block, and zero side yard setbacks. It is used for mixed-use, residential, and commercial development.

**Hardscape**. The use of hardened surfacing materials to create unique patterns of color, design, and texture in order to create visual interest; also used to mean those areas that have received such improvements.

Heat Island Effect. Occurs when warmer temperatures are experienced in urban landscapes compared to adjacent rural areas as a result of solar energy retention on constructed surfaces. Principle surfaces that contribute to the heat island effect include streets, sidewalks, parking lots, and buildings.

**Home Occupation.** The conduct of business within a dwelling unit or residential site, employing occupants of the dwelling, with the business activity being subordinate to the residential use of the property.

Heliports. A designated, marked area on the ground or the top of a structure where helicopters may land at any time.

Home Improvement Supplies. Establishments (retail or wholesale) that sell kitchen, bath, furnishings, carpeting, and other home oriented supplies. Other retail uses are permitted if accessory to the primary use. These use may include an expansive showroom. This category does not include the sale of lumber and does not permit the outdoor display of merchandise. This use classification is a sub category of the larger Building Materials Stores and Yards use classification and may be combined with or separate from such uses.

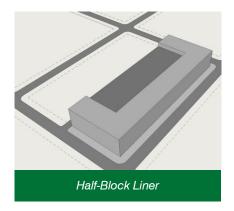
Hotels/Motels. Facilities with guest rooms or suites, provided with or without kitchen facilities, rented to the general public for transient lodging (less than 30 days). Hotels provide access to most guest rooms from an interior walkway, and typically include a variety of services in addition to lodging (e.g. restaurants, meeting facilities, personal services). Motels provide access to most guest rooms from an exterior walkway. Also includes accessory guest facilities such as swimming pools, tennis courts, indoor athletic facilities, accessory retail uses, etc.

**Impervious Surface**. Any material that prevents absorption of stormwater into the ground.

Indoor Amusement/Entertainment Facilities. Establishments providing indoor amusement and entertainment services for a fee or admission charge (e.g. dance halls and ballrooms, electronic game arcades). Four or more electronic games or coin-operated amusements in any establishment, or a premises where 50 percent or more of the floor area is occupied by amusement devices, are considered an electronic game arcade as described above; three or less machines are considered accessory to the primary use of the site.

Indoor Fitness and Sports Facilities. Predominantly participant sports and health activities conducted entirely within an enclosed building. Typical uses include bowling alley, billiard parlor, ice/roller skating rinks, indoor racquetball courts, indoor climbing facilities, soccer areas, athletic clubs and health clubs. This use does not include special studios not a part of an athletic or health club (e.g. karate studio, dance studio, etc.). Also see "Schools – Private and Special/Studio."

**Infill Housing/ Quarter Block Liner.** An attached building with a frontage that is less than one-third the length of a Downtown block, and zero side yard setbacks. It is used for mixed-use, residential, and commercial development.





**Kennels.** These facilities provide boarding of animals. May also include day-time boarding and activity for animals (e.g. "doggie day care").

Libraries, Museums, and Similar Facilities. Public or quasi-public facilities such as aquariums, arboretums, art exhibitions, botanical gardens, historic sites and exhibits, libraries, museums, and planetariums, which are generally non-commercial in nature.

Live-Work Facility. Rowhouse or conventional house with the first story or the front of the home available as a commercial space. The commercial space may be independently leased or used by an occupant of the residential unit. The building construction must meet the building and fire code based on the use type. The work space shall contain an active use and not be used exclusively for storage or warehousing. For example, the unit may include:

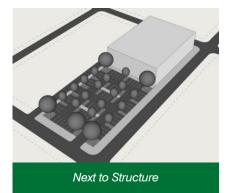
- First floor storefront for retail or service business activity;
- Restaurant, café, or catering establishment with a complete kitchen and sanitary facilities;
- Minor fabrication activities (e.g. carpentry, light welding, print shop, artist studio); and
- Office space.

Local Streets. Local roads provide access to adjacent properties, primarily residential uses, and distribute traffic to collectors. Travel speeds on local streets typically range from 25 to 35 mph.

Manufacturing – Major. Manufacturing, fabrication, processing, and assembly of materials in a raw form. Uses in this category typically create greater than usual amounts of smoke, gas, odor, dust, sound, or other objectionable influences that might be obnoxious to persons conducting business on-site or on an adjacent site. Uses include but are not limited to batch plants, rendering plants, aggregate processing facilities, plastics and rubber products manufacturing. Also see "Manufacturing – Minor" and "Cottage Industry."

Manufacturing – Minor. Manufacturing, fabrication, processing, and assembly of materials from parts that are already in processed form and that, in their maintenance, assembly, manufacture, or plant operation, do not create excessive amounts of smoke, gas, odor, dust, sound, or other objectionable influences that might be obnoxious to persons conducting business on-site or on an adjacent site. Uses include but are not limited to furniture manufacturing and cabinet shops, laundry and dry cleaning plants, metal products fabrication, food and beverage manufacturing, etc. Also see "Manufacturing – major" and "Cottage Industry."

**Medical Services – General.** Facilities primarily engaged in furnishing outpatient medical, mental health, surgical and other personal health services, but which are separate from hospitals (e.g. medical and dental laboratories, medical, dental and psychiatric offices, out-patient care facilities, allied health service).



**Medical Services - Hospitals.** Hospitals and similar facilities engaged primarily in providing diagnostic services, and

extensive medical treatment, including surgical and other hospital services. These establishments have an organized medical staff, inpatient beds, and equipment and facilities to provide complete health care. May include on-site accessory clinics and laboratories, accessory retail uses and emergency heliports.

**Mixed Use.** The development of land, buildings, or structures with a variety of complementary and integrated uses.

Mullion. A mullion is a structural element which divides adjacent window units.

**Multi-Family Faux House.** A detached building that has a street appearance of a single large house which contains more than four dwellings. Each dwelling is individually accessed from a central lobby, which in turn is accessed directly from the street.

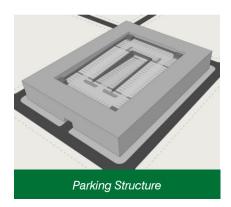
**Multi-Family Residential.** A building in which two or more families live in separate but attached dwellings, such as apartment houses, townhomes and condominiums.

**Next to Structure Parking.** Ground level public or private parking lot located in the side yard setback next to a building. If possible, access to the parking should be taken from an alley.

**Outdoor Commercial Recreation.** Facility for various outdoor participant sports and types of recreation where a fee is charged for use (e.g. amphitheaters, amusement and theme parks, golf driving ranges, health and athletic club with outdoor facilities, miniature golf courses, skateboard parks, stadiums and coliseums, swim and tennis clubs, water slides, zoos).

Office – Business and Professional. This use listing includes offices of administrative businesses providing direct services to consumers (e.g. insurance companies, utility companies), government agency and service facilities (e.g. post office, civic center), professional offices (e.g. accounting,

attorneys, employment, public relations), and offices engaged in the production of intellectual property (e.g. advertising, architectural, computer programming, photography studios). This use does not include medical offices (see "Medical Services") Outdoor storage of materials is prohibited.



**Parapet.** The extension of the main walls of a building above the roof level.

Parks and Public Plazas. Public parks including playgrounds and athletic fields/courts and public plazas and outdoor gathering places for community use. If privately-owned and restricted to the public (e.g., require payment of fee), the same facilities are included under the definition of "Outdoor Commercial Recreation."

Park and Ride Facility. A designated area where a vehicle may be left in order to carpool with other commuters or to ride public transit.

**Parking Structure.** A privately owned and operated multi-story structure that provides public parking spaces for a fee.

**Parking Type.** Refers to the type of parking allowed for motorized vehicles including automobiles, trucks, and motorcycles.

PDA. Priority Development Area

**Pedestrian Refuge Island**. A pedestrian refuge island is a small concrete or paved island in the middle of a road that allows people to cross the road in stages.

**Pedestrian Scale**. The proportional relationship between an individual and his or her environment.

Personal Services. Establishments providing non-medical services as a primary use, including, but not limited to barber and beauty shops, clothing rental, dry cleaning pick-up stores with limited equipment, home electronics and small appliance repair, Laundromats (self-service laundries), shoe repair shops, and tailors. These uses may also include accessory retail sales of products related to the services provided, spas and hot tubs for rent, and tanning salons.

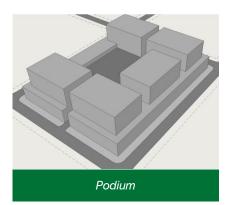
**Personal Services, Restricted.** Personal service establishments (e.g. check cashing services, fortune tellers, psychics, palm readers, and similar services, tattooing, piercing, massage parlors and similar services) tending to have a blighting and/or deteriorating effect upon surrounding areas and which may need to be dispersed to minimize their adverse impacts. These uses may also include accessory retail sales of products related to the services provided.

Personal Storage Facility. A structure or group of structures containing generally small, individual, compartmentalized stalls or lockers rented as individual storage spaces and characterized by low parking demand.

**Pervious Surface.** Any material that permits full or partial absorption of storm water into previously unimproved land.

**Podium.** This building type refers to a commercial building up to four stories tall that has a larger first floor area compared to the other floors.

**Porch Frontage.** This frontage type is characterized by a façade which is set back from the property line with a front yard, and by a porch which is appended to the front façade (the porch may encroach into the front setback).





**Printing and Publishing.** Establishments engaged in printing by letterpress, lithography, gravure, screen, offset, or electrostatic (xerographic) copying, and other establishments serving the printing trade including bookbinding, typesetting, engraving, photoengraving, and electrotyping. This use also includes establishments that publish newspapers, books and periodicals, and establishments manufacturing business forms and binding devices. Does not include "quick printing" services or desktop publishing which are included in "Business Support Services.

Priority Development Area (PDA). Locally-identified, infill development opportunity areas within existing communities. PDAs are generally areas of at least 100 acres where there is local commitment to developing more housing along with amenities and services to meet the day-to-day needs of residents in a pedestrian-friendly environment served by transit. To be eligible to become a PDA, an area had to be within an existing community, near existing or planned fixed transit or served by comparable bus service, and planned for more housing.

Recycling Facility – Collection. A recycling facility used for the acceptance by donation, redemption, or purchase of recyclable materials from the public that does not occupy more than 500 square feet. This classification may include a mobile unit, kiosk-type units that may include permanent structures and unattended containers placed for the donation of recyclable materials. This also includes so-called "reverse vending machines" - automated mechanical device that accepts one or more types of empty beverage containers including, but not limited to, aluminum cans, glass bottles and plastic bottles, and issues a cash refund or a redeemable credit clip with value of not less than the container's redemption value as determined by the State.

Recycling Facility – Processing. A recycling facility located in a building or enclosed space and used for the collection and processing of recyclable materials. Processing means the preparation of material for efficient shipment or to an end-user's specifications by such means as baling, briquetting, compacting, flattening, grinding, crushing, mechanical sorting, shredding, cleaning and remanufacturing.

Recycling Facility – Scrap and Dismantling. Uses engaged in the assembling, breaking up, sorting, temporary storage, and distribution of recyclable or reusable scrap and waste materials, including the dismantling or wrecking of automobiles or other motor vehicles, and/or the storage or keeping for sale of parts and equipment resulting from such dismantling or wrecking. The presence on any lot or parcel of land of five or more inoperable vehicles from which parts have been or are to be removed for reuse or sale shall constitute prima facie evidence of an automobile wrecking yard. This use does not include landfills or other terminal waste disposal sites.

Religious Institutions. Facility operated by religious organizations for worship, or the promotion of religious activities (e.g. churches, mosques, synagogues, temples) and accessory uses on the same site (e.g. living quarters for ministers and staff, child day care facilities which where authorized in conjunction with the primary use. Other establishments maintained by religious organizations, such as full-time educational institutions, hospitals and other potentially related operations (e.g. recreational camp) are classified according to their respective activities.

Residential Care Home. Consistent with the definitions of State law, a residential care home is a home that provides 24-hour non-medical care for six or fewer persons 18 years of age or older, or emancipated minors, with chronic, life-threatening illness in need of personal services, protection, supervision, assistance, guidance, or training essential for sustaining the activities of daily living, or for the protection of the individual. This classification includes group homes, rest homes, residential care facilities for the elderly, adult residential facilities, wards of the juvenile court, and

other facilities licensed by the State of California. Convalescent homes, nursing homes and similar facilities providing medical care are included under the definition of "Medical Services - Extended Care."

Restaurants. A retail business selling food and beverages prepared and/or served on the site, for on- or off premise consumption. These include eating establishments where customers are served from a walk-up ordering counter for either on- or off-premise consumption, and establishments where most customers are served food at tables for on-premise consumption, but may include providing food for take-out. Also includes coffee houses and accessory cafeterias as part of office and industrial uses.

Retail. Stores and shops selling multiple lines of merchandise. These stores and lines of merchandise include but are not limited to art galleries, bakeries (all production in support of onsite, sales), clothing and accessories, collectibles, department stores, drug and discount stores, dry goods, fabrics and sewing supplies, florists and houseplant stores (indoor sales, only; outdoor sales are "Plant Nurseries"), furniture, home furnishings and equipment, general stores, gift shops, hardware, hobby materials, musical instruments, parts and accessories, newsstands, pet supplies specialty shops, sporting goods and equipment, stationery, and variety stores. Also includes Retail – Accessory. The retail sales of various products (including food service) in a store or similar facility that is located within a health care, hotel, office, or industrial complex. These uses include but are not limited to pharmacies, gift shops, and food service establishments within hospitals, and convenience stores and food service establishments within hotel, office and industrial complexes. This use category also includes retail associated with industrial uses for the products sold, distributed or manufactured on site. Such retail area shall not exceed 25 percent of the total square footage of the tenant space of a single use development or the combined floor area of an integrated development in a mixed use project.

**Rowhouse.** Two or more detached two- or three-story dwellings with zero side yard setbacks. A Rowhouse may be used for non-residential purposes

Schools – Private and Special/Studio. Includes private educational institutions (e.g. boarding schools, business, secretarial, and vocational schools, colleges and universities, establishments providing for courses by mail or on-line) and special schools/studios (e.g. art, ballet and other dance, computers and electronics, drama, driver education, language, music, photography). Also includes facilities, institutions and conference centers that offer specialized programs in personal growth and development (e.g. fitness training studios, gymnastics instructions and aerobics and

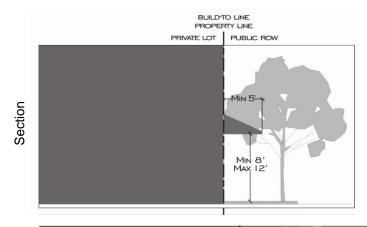
gymnastics studios, environmental awareness, arts, communications, management). Also see "Indoor Fitness and Sports Facilities."

Schools – Public. Public educational institutions such as community colleges, universities, elementary, middle/junior high schools, high schools, and military academies.

Service Station. A retail business selling gasoline or other motor vehicle fuels.

**Setback**. The required distance between a property line and a structure. The setback is measured from the property line and/or right of way line.

Shopfront and Awning Frontage. This frontage is characterized by a façade which is aligned close to or directly on the right-of-way line with the building entrance at sidewalk grade. A shopfront and awning frontage has substantial glazing on the ground floor. Shopfront and awning frontages provide awnings or canopies cantilevered over the sidewalk.





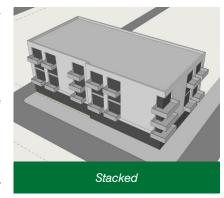




Building entrances may either provide a canopy or awning, or alternatively, may be recessed behind the front building façade.

**Stacked.** A residential building comprised of vertically stacked dwelling units. The main entrance to the building shall be through a street level lobby.

**Stoop Frontage.** This frontage is characterized by a façade which is aligned close to the frontage line with the ground story

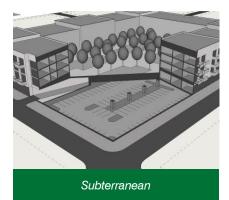


elevated from the sidewalk to provide privacy for the ground floor uses. The entrance is usually an exterior stair or landing which may be combined with a small porch or roof. The Stoop frontage type is suitable for ground floor residential uses with short setbacks.



**Streetscape**. The visual character of a street as determined by elements such as structures, access, greenery, open space, view, etc. The scene as may be observed along a public street composed of natural and man-made components, including buildings, paving, planting, street

hardware, and miscellaneous structures.



**Subterranean Parking.** Underground public or private parking structure typically located directly underneath the building it is serving.

**Sustainability.** Balancing economic, environmental and social interests and responsibilities to promote development that meets the needs of the community, now and into the future.

**Swales**. An open drainage channel that has been explicitly designed to detain or infiltrate the entire runoff volume associated with a water quality storm event.

**Telecommunications Facility.** Facility designed and/or used for the purpose of transmitting, receiving, or relaying voice and/or data signals from various wireless communication devices, including transmission tower, antenna, and or other facility designed or used for that purpose. Amateur radio transmission facilities, facilities operated exclusively as part of a public safety

network, and facilities used exclusively for the transmission of television and/or radio broadcasts are not "telecommunication facilities".

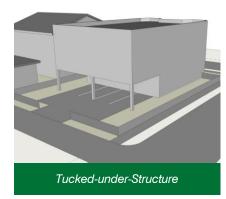
**Terraced.** A mixed-use, residential, or commercial building characterized by individual units that are accessed via multi-leveled outdoor terraces. The terraces are intended to be semi-public spaces that are extensions of the public realm.



Theatres and Auditoriums. Indoor facilities for public assembly and group entertainment, other than sporting events (e.g. civic theaters, facilities for "live" theater and concerts, exhibition and convention halls, motion picture theaters, auditoriums). Does not include outdoor theaters, concert and similar entertainment facilities, and indoor and outdoor facilities for sporting events; see "Outdoor Commercial Recreation."

**Traffic Calming.** Reducing motorist speed, decreasing motor vehicle volumes, and increasing safety for pedestrians and non-motorized vehicles

**Transit Stations and Terminals.** Passenger stations for vehicular and rail mass transit systems; also terminal facilities providing maintenance and service for the vehicles operated in the transit system. Includes buses, taxis, railway, etc.



**Tucked-Under-Structure.** Ground level covered parking lot located directly beneath the second floor of building. Tucked under parking should not be visible from the public right-of-way unless the right-of-way is a City designated alley.

Vehicle Services. The repair, alteration, restoration, towing, painting, cleaning (e.g. self-service and attended car washes), or finishing of automobiles, trucks, recreational vehicles, boats and other vehicles as a primary use, including the incidental

wholesale and retail sale of vehicle parts as an accessory use. This use includes major repair and body work - repair facilities dealing with entire vehicles; such establishments typically provide towing, collision repair, other body work, and painting services and may also include tire recapping establishments. Also includes Minor facilities specializing in limited aspects of repair and maintenance (e.g., muffler and radiator shops, quick-lube, smog check). Does not include repair shops that are part of a vehicle dealership on the same site.

**Veterinary Facility.** Veterinary facility that is primarily enclosed, containing only enough cage arrangements as necessary to provide services for domestic and exotic animals requiring acute medical or surgical care with accessory outdoor use that provides long term medical care. Grooming and boarding of animals is allowed only if accessory to the facility use.

### **DEVELOPMENT POTENTIAL**

The Specific Plan modifies development potential within the San Pablo Avenue, Pinole Valley Road, and Appian Way corridors. The General Plan reduces the development potential on several properties outside of the Specific Plan boundaries by assigning a Rural Land Use designation to some properties that had previously been designated for residential use. The change to a Rural land use designation would result in a small reduction in development potential due to environmental constraints on these sites. Accordingly, these projections focus on the land use changes that increase development potential, primarily within the Specific Plan Areas.

Predicting changes to commercial square footage and residential density under the Specific Plan over the next 20 – 30 years is difficult because the land use designations provide a great deal of flexibility in order to accommodate a broad range of market conditions. The percentage of commercial and residential use within the mixed use zones could vary broadly, lot coverage will be higher in locations where off-site parking facilities (e.g. a parking garage in Old Town) or improved transit service obviate the need for parking on-site, and lower for some commercial uses (e.g. industry) that are not located entirely within a structure.

It is important, however, to develop land use projections so that the effectiveness of the Specific Plan can be measured and evaluated. By applying assumptions related to site utilization, and building efficiency, the Specific Plan creates the following potential for development above what is allowable under the existing land use designations:

Residential: 1,076 Units
Retail: 431,088 s.f.
Office: 512,466 s.f.
Industrial: 0 s.f.

As recent events (e.g. changes in the housing market, a gasoline price spike, and economic uncertainty) demonstrate, economic conditions during the life of the Specific Plan may dramatically influence housing needs, investment decisions, and travel behavior. Because of this potential for change, the economic analysis does not attempt to project conditions beyond a 15 year period. Never-the-less, the projections contained herein attempt to estimate new development over the next 20 to 30 years. The following text describes the factors and assumptions that were used to estimate the development potential for the Specific Plan.

#### LAND USE COMPARISON

In order to calculate long-range development projections for the Specific Plan area, it is important to understand projected demand, and to assign realistic assumptions to development potential within land use categories that reflect demand and physical and regulatory constraints. The following discussion describes the market conditions that are anticipated over the next 15 years, and provides a detailed description of the assumptions that are applied to land use types.

### **Projected Demand**

Population and employment growth in Pinole will likely be slower than the County and the region due to the largely built-out nature of the City. ABAG projections 2007 estimate that by the year 2035, the population of Pinole will increase by 14% and jobs in Pinole will increase by 38%, compared to projected population and job increases in the County of 27% and 56% respectively. The majority of households in Pinole own their home (74%), while the majority of jobs in Pinole are in health, education, and recreational services (52%), with the retail sector (23%) and financial and professional services (11%) providing the next highest concentration of jobs. Countywide, the highest rate of growth is expected to occur in the financial and professional services, and health and education sector, with a projected 61% increase in each. Given these population and employment sector projections, and based on ABAG estimates, BAE projects that there will be demand for the following development within the next 15 years:

Office: 75,000 – 100,000 square feet
Retail: 125,000 square feet
Industrial: Existing Supply is Adequate
Lodging: Undetermined

ABAG Projections 2007 reflect a Regional Housing Need Allocation for the City of Pinole by the year 2014 as follows:

Income Category	Number of Units
Very Low Income	83 Units
Low Income	49 Units
Moderate Income	48 Units
Market Rate	143 Units
TOTAL	323 Units

ABAG Projections 2007 anticipate that residential demand will continue to grow. ABAG seeks to identify the number of housing units each City would need to plan for in order to contribute their fare share toward satisfying regional demand. By year 2030, Pinole would have to be able to provide 1,892 more units than presently exist in order to satisfy its share of the regions demand.

### **Specific Plan Capacity**

The Specific Plan areas contain approximately 300 acres of predominantly developed land. In order to accommodate the projected demand for development and invite further capital investment within Pinole, the corridor land uses have been changed to:

- Replace single use commercial zoning with various mixed use zones;
- Eliminate commercial FAR as a development constraint;
- Increase opportunities for residential development; and
- Increase residential density.

The following table provides a breakdown, by corridor, of the gross acreages within the 2009 land use designations.

Table 1 2009 Land Use Designations

2000 Land Has		A			
2008 Land Use	San Pablo Ave.	Appian Way	Pinole Valley Rd.	Acres	
(R1) Low Density Res.*	8.5	13.6	37.3	59.4	
(R2) Moderate Density Res.	0.2	0	3.0	3.2	
(R3) Medium Density Res.	3.5	0	2.3	5.8	
(R4) High Density Res.	8.7	0.4	2.8	11.9	
(C1) Neighborhood Business	9.6	14.6	20.5	44.7	
(C2) Central Business	15.8	10.9	1.2	27.9	
(C3) General Commercial	14.6	45.6	10.8	71.0	
(M1) Light Industrial	12.4	0	0	12.4	
(M2) General Industrial	7.3	0	0	7.3	
(PD) Planned Development	27.9	14.8	0.3	43.0	

2009 Land Haa		A 2 4 2 2		
2008 Land Use	San Pablo Ave.	Appian Way	Pinole Valley Rd.	Acres
(PA) Professional/Admin.	0	10.8	0	10.8
(OS) Open Space	6.2	0	0	6.2
(MU) Mixed-Use	8.6	0	0	8.6
	123.3	110.7	78.2	312.2

<sup>\*</sup>Includes School Pinole Valley High, Collins Elementary, and Pinole Middle School Properties

Table 2
Specific Plan Land Use Designations

Drawand Zaning		A ====			
Proposed Zoning	San Pablo Ave.	Appian Way	Pinole Valley Rd.	Acres	
(MDR) Medium Density Res.	13.0	0	6.8	19.8	
(HDR) High Density Res.	0	1.9	1.7	3.6	
(VHR) Very High Density Res.	6.8	0	0	6.8	
(RMU) Residential MU	15.9	18.6	0	34.5	
(CMU) Commercial MU	38.6	70.3	19.4	128.3	
(OPMU) Office/Professional MU	0	8.4	11.3	19.7	
(OIMU) Office Industrial MU	43.4	0	0	43.4	
(P/QP/I) Public/Institutional	4.4	11.5	35.6	51.5	
(OS) Open Space	1.2	0	3.4	4.6	
	123.3	110.7	78.2	312.2	

#### **DEVELOPMENT CONSTRAINTS**

Estimating the redevelopment potential of property within the Specific Plan area is complicated by the fact that ownership is fragmented and, with a few exceptions, parcel sizes are small. These factors combine to make redevelopment less efficient from a land utilization perspective.

A reasonable estimate of development potential must also account for the fact that most of the properties within the Specific Plan area are already developed. Even if underutilized, developed properties generate income for property owners and make it less likely that the site will redevelop unless there is a substantial incentive to do so. This condition tends to limit the financial feasibility of redeveloping parcels unless new development intensity is high enough to justify the costs

The following table provides a breakdown, by corridor, of the gross acreages within the Specific Plan land use designations.

associated with buying and clearing land that is presently occupied with income-generating uses. This condition will likely result in slow and incremental property development patterns.

#### **DEVELOPMENT POTENTIAL**

It is important to identify some of the considerations that will influence development projections within the Specific Plan boundaries. The assumptions that have been generated for these projections have been formulated to reflect the following conditions:

- <u>Development Constraints</u>: Regulatory, physical, and market constraints will have an effect on build-out conditions:
  - <u>Development Intensity</u>: Residential density and commercial floor area limitations establish the maximum potential development within the Specific Plan area;
  - <u>Topography</u>: Steep slopes will reduce the development potential on some properties;
  - Market Demand: Limited demand for certain development types, particularly retail, will further limit development potential.
- <u>Large Lot</u>: Development of properties that are over an acre in size will typically result in more efficient parking and circulation layouts.
- <u>Small Lot</u>: Smaller scale properties typically require a higher percentage of the property to be utilized for parking and circulation.
- Old Town: Old Town parking conditions and standards promote shared parking and create an opportunity for more compact development than would be anticipated in other areas of the Specific Plan.
- Redevelopment: Because most of the properties within the Specific Plan boundaries are already developed, redevelopment of these sites will likely occur slowly and in an incremental manner.

The land use assumptions account for these conditions and establish a realistic development scenario that could be compared with potential development under the 1995 General Plan and Zoning Code. This section describes the assumptions used to evaluate existing conditions, and changes that would result under the Specific Plan. Due to the fact that opportunity sites tend to be

larger than other properties within the Specific Plan area, separate assumptions have been applied to these properties.

#### LAND USE ASSUMPTIONS

### **General Assumptions**

To estimate development potential within the Specific Plan Corridors, it is important to make assumptions related to the following:

- Percent of development type by sector (residential, retail, office, industrial, public, other).
- Square footage by sector the average dwelling unit size per development type or average number of square feet of building area per employee (square feet per employee ratios) have been based on projections provided by Bay Area Economics.
- Landscaping, circulation, and setback requirements are deducted from the gross lot area to create a site utilization percentage. Depending on the type and location of development, site utilization is expected to vary from 40% for industrial uses to 85% for mixed use and high density projects.
- Maximum height in stories.
- Depending on development type (e.g. apartment v. townhouse), a portion of the building area
  may be taken up with hallways and common areas. Where this occurs, a building efficiency
  adjustment is made. Based on analysis of selected opportunity sites, and input from industry
  professionals, it is estimated that 15% of the building area would be unavailable for residential
  use.

These assumptions are used to calculate development potential for a land use. For parcels where more than one development type is possible, a blended development type has been created. Blended development types contain percentages of land uses (e.g. 10% Retail, 60% Medium Density Residential, 30% Office) that would be anticipated given site conditions. Actual development proposals are not expected to follow these estimates and will likely propose substantially different use ratios, these estimates attempt to predict the average development ratios throughout the Specific Plan area over a 30-year period.

Parcels within the Specific Plan Area are in-fill sites that are presently served by infrastructure and will not require substantial land dedication for public improvements. Accordingly, it is assumed that 100% of the sites are available for development.

### **Existing Conditions Assumptions**

The following assumptions were used to evaluate development potential under the 1995 General Plan and Zoning Code:

- Low Density Residential: With potential densities of between 1 and 7 units per acre, these zones are predominantly developed and are not expected to experience further subdivision. Where Low Density Residential property contains public facilities (e.g. schools), these sites are expected to continue operating as a public facility. While Low Density Residential properties may experience some increase in residential use through the creation of second units, the potential for additional development is not substantial, and would not change under the Specific Plan. Projections assume that existing conditions represent build-out.
- Moderate Density Residential: An inconsequential amount of land within the project area has been designated with a moderate residential density. No further development potential is calculated for this area. Projections assume that existing conditions represent build-out.
- Medium Density Residential: These zones have not developed to the maximum allowed (8 to 15 dwelling units per acre) and contain single-family residences that may be converted into duplex or triplex units in response to the rising cost of housing. While such conversion would likely occur slowly and incrementally, the development projections assume that such conversion would occur, but that the actual number of units would be limited by smaller lot sizes. Accordingly, development potential is calculated:
  - With at a density of 12 units per acre;
  - 65% site utilization; and
  - 85% building efficiency.
- High Density Residential: These properties have, in some cases, developed to the maximum potential density (25 dwelling units per acre), while others remain underdeveloped. The development projections assume housing costs will rise over 20 30 years and result in

development at the maximum permitted density and employ the following development projection assumptions:

- A site utilization of 60%;
- A unit efficiency of 85%; and
- A density of 25 units per acre.
- Neighborhood Commercial: The Pinole Valley and Appian 80 shopping centers are located in this zone, together with a mix of office and retail uses on other sites on Pinole Valley Road and scattered throughout the Specific Plan area. Due to the amount of land occupied by lower intensity retail, these properties:
  - Were assigned a site utilization of 30%; and
  - Were expected to contain primarily retail (75%) with the balance (25%) used for office space.
- <u>Central Business</u>: These zones are located within the Old Town area and are dispersed along San Pablo Avenue and Appian Way. In Old Town, development on these properties tends to occupy a higher percentage of the site and to contain a balanced mix of office and retail, while outside of Old Town, development occupies a smaller percentage of the site. Using a recent development proposal near the Appian/Dalessi intersection as a model for mixed use development, these properties:
  - Were assigned a site utilization of 50%;
  - Were expected to contain an equal distribution of retail (50%) and office (50%) uses; and
  - Were expected to develop at no more than 2 stories.
- General Commercial: This zone includes the Del Monte shopping center, the Kaiser Medical Office, and a variety of other uses that tend toward retail activity. Because these sites have developed at lower intensity levels and seem to favor retail use, they:
  - Were assigned a development percentage of 40%;

- Were expected to contain more retail (65%) than office (35%);
- Retail would be located within single-story structures; and
- Office would be located within two story structures.
- Light Industry: Industrial land use, unlike commercial development, has varied demand for structured space depending on the individual use. This zone contains a corporation yard and RV storage that require most of the site for outdoor storage and circulation, and a self-storage facility that contains structures on much of the site. Because future redevelopment of these sites could result in higher percentage of structured space, the development projections assume:
  - Site utilization of 40%; and
  - Single story development.
- General Industry: This zone contains Sugar City (a construction/landscape supply business) and the wastewater treatment plant. These types of uses have need for office, administrative and retail space, but also require large areas for outdoor storage and materials processing. Though future uses in these locations could generate demand for more structured space, the development projections anticipate that the existing conditions are likely to prevail and assume:
  - Site utilization of 30%; and
  - Single story development.
- Planned Development: While the majority of property within this zone is residential, the Planned Development District also contains light industrial uses at the Pinole shores Business Park site, and medical office facilities on Appian Way across the street from Doctor's Hospital medical building. In general, these sites support a development intensity that is unlikely to change, and development projections assume that existing development reflects build out under current conditions. Development projections assume:
  - Site utilization of 50%;

- 70% of structured space will be used for residential development at density of 15 units/acre;
- 15% of structured space will contain light industrial uses; and
- 15% of structured space will contain offices.
- <u>Professional and Administrative</u>: Created primarily to support medical facilities and offices, this
  zone contains Doctors Hospital and adjoining sites that house medical and other offices.
  Development projections assume:
  - 40% site utilization
  - 100% of structured space will be used for offices; and
  - 2 story construction.
- Mixed Use: This zone contains land that is used for single use residential and retail activity, is vacant, or contains mixed use residential, retail and office space. The largest of these properties contains the Collins House and the Heritage Park project site. This site is not expected to develop at a greater intensity than already exists or that has been approved. The other properties are located on San Pablo Avenue and would expect to develop or redevelop at higher densities than currently exist. To account for the wide variation between anticipated development for these sites, they:
  - Were assigned a site utilization of 40%;
  - 40% of structured space would contain residential uses with the balance equally distributed between office and retail (30% each); and
  - Residential development assumed 85% unit efficiency.
- Open Space: This land use designation contains active and passive recreational facilities.
   Development projections assume that existing development reflects build out under current conditions.

### **Specific Plan Assumptions**

The following assumptions have been used to project development potential on sites located within the Specific Plan Boundaries.

<u>Medium Density Residential</u> (10.1 to 20 dwelling units per acre): Project economics are such that structured parking will not be feasible and the majority of development would be townhouse products.

- 75% site utilization
- 70% of anticipated development would be Townhouse product type with:
  - A density of 15 units/acre; and
  - 100% building efficiency.
- 30% of anticipated development would be attached units with:
  - A density of 20 units/acre; and
  - 85% building efficiency.

<u>High Density Residential</u> (20.1 to 35 dwelling units per acre): The small lot sizes of these in-fill sites will require a comparatively high percentage of the site to be dedicated to circulation and parking, thereby reducing residential densities. Accordingly, development projections assume

- 75% site utilization
- Residential density of 30 units/acre; and
- 85% building efficiency.

<u>Very High Density Residential</u> (35.1 to 50 dwelling units per acre): At higher densities, the project economics are such that structured parking becomes a viable development type. Consequently, development projections assume:

85% site utilization

- Residential density of 50 units/acre; and
- 85% building efficiency.

<u>Residential Mixed –Use Designation</u>: While commercial uses are allowed on these properties, it is expected that any such commercial development would be accessory to the principal residential use at development densities between 10.1 and 30 Units/Acre. With the exception of opportunity sites, project economics are likely to result in townhouse development prototypes with densities of 15 – 20 Units/Acre. Accordingly, development projections:

- 85% site utilization
- Estimate that no more than 10% of development will be commercial
- Commercial development will only include retail uses
- Residential development would occur at 20 Units/Acre

<u>Commercial Mixed-Use Designation</u>: Though intended to support commercial and mixed-use development, this land use designation allows residential development at 10 – 18 units per acre. This land use designation exists within the Service, Old Town, and Mixed-Use Sub-Areas and is expected to exhibit different development patterns for each as described below:

- 85% site utilization
- Service Sub-Area: Commercial-mixed use properties are expected to continue supporting retail and service centers, but will experience modest development of residential uses:
  - Existing commercial to remain
  - 15 units/acre residential
- Old Town and Mixed-Use Area: Commercial mixed-use property is expected to be more aggressively used for mixed uses, with an emphasis on retail activity
  - 50% Residential at 15 units/acre
  - 30 % Retail

20% Office

Office Professional Mixed-Use Designation: While these sites have the potential for mixed-use, it is assumed that any retail or residential development will be accessory to the principal use of these sites for office facilities.

- 75% site utilization
- 80% Office
- 15% Residential at 15 Units/Acre
- 5% Retail

Office Industrial Mixed-Uses: It is anticipated that office flex, and light industrial development would occur on these properties, and would follow the Pinole Shores Business Park model.

- 30% site utilization;
- 80% industrial
- 10% retail
- 10% office

<u>Public/Quasi-Public/Institutional Designation</u>: City Hall, the Senior and Youth Centers, and other similar uses are not expected to change. Accordingly, development projections assume existing conditions represent build-out.

Open Space Designation: There is existing Open Space at Fernandez Park, Collins Elementary School, and Pinole Valley High School. No additional open space is contemplated for the Specific Plan area

#### LAND USE COMPARISON

By applying the above assumptions to the Specific Plan Area, this analysis estimates the development potential under the 1995 General Plan and governing Zoning Code as compared to the development potential under the Specific Plan. If all of the properties within the Specific Plan corridors were to develop according to the provisions of the land use and development standards, in accordance with the above assumptions, Pinole would expect to realize increased development in the course of the next 20 to 30 years as indicated below.

Table 3
Development Projections

Corridor	Residential (Units)	Retail (s.f.)	Office (s.f.)	Industrial (s.f.)
San Pablo Ave.	885	322,172	336,253	426,692
Pinole Valley Rd.	141	238,708	105,038	1,239
Appian Way	244	561.260	468,449	48,352
Existing Total	1,270	1,122,140	909,740	476,283
San Pablo Ave.	1,119	552,927	307,233	472,578
Pinole Valley Rd.	351	192,603	386,843	0
Appian Way	877	807,698	728,129	0
Proposed Total	2,346	1,553,228	1,422,206	472,578
Difference	1,076	431,088	512,466	(3,705)

In the next 15 years, BAE projects that demand for commercial development will be limited as follows:

Office: 75,000 – 100,000 square feet
Retail: 125,000 square feet
Industrial: Existing Supply is Adequate
Lodging: Undetermined

Because the proposed land use designations would accommodate more commercial development than is expected to occur in during the General Plan cycle, these development projections limit growth in the commercial and office sectors to 150,000 square feet above the existing land use capacity by the year 2030.

Table 4
Pinole Specific Plan Land Use Types – Existing Conditions

Land Use Type Name	Total Acreage	Lot Coverage	Building Footprint	Residential	Commercial Retail	Commercial Office	Industrial	Other	Maximum Du/Acre	# Stories	Residential Units	Retail Area	Office Area	Industrial Area	Other
San Pablo Avenue															
(R1) Low Density Residential	8.50	65%	5.53	5.53	0.00	0.00	0.00	0	7.00	2	60	0	0	0	0
(R2) Moderate Density Residential	0.20	60%	0.12	0.12	0.00	0.00	0.00	0	12.00	2	2	0	0	0	0
(R3) Medium Density Residential	3.50	65%	2.28	2.28	0.00	0.00	0.00	0	12.00	2	42	0	0	0	0
(R4) High Density Residential	8.70	60%	5.22	5.22	0.00	0.00	0.00	0	25.00	3	165	0	0	0	0
(C1) Neighborhood Business	9.60	30%	2.88	0.00	2.16	0.72	0.00	0	0.00	1	0	64,571	21,524	0	0
(C2) Central Business	15.80	50%	7.90	0.00	3.95	3.95	0.00	0	0.00	2	0	101,213	101,213	0	0
(C3) General Commercial	14.60	40%	5.84	0.00	3.80	2.04	0.00	0	0.00	1.5	0	125,904	67,794	0	0
(M1) Light Industrial	12.40	40%	4.96	0.00	0.00	0.00	4.96	0	0.00	1	0	0	0	216,058	0
(M2) General Industrial	7.30	30%	2.19	0.00	0.00	0.00	2.19	0	0.00	1	0	0	0	95,396	0
(PD) Planned Development	27.90	50%	13.95	9.77	0.00	2.09	2.09	0	15.00	2	529	0	115,238	115,238	0
(PA) Professional and Administrative	0.00	40%	0.00	0.00	0.00	0.00	0.00	0	0.00	2	0	0	0	0	0
(MU) Mixed-Use	8.60	40%	3.44	1.38	1.03	1.03	0.00	0	15.00	1.5	87	30,484	30,484	0	0
(OS) Open Space	6.20	20%	1.24	0.00	0.00	0.00	0.00	1.24	0.00	1	0	0	0	0	54,014
	123.30										885	322,172	336,253	426,692	54,014
Pinole Valley Road															
(R1) Low Density Residential	1.70	65%	1.11	1.11	0.00	0.00	0.00	0	7.00	2	12	0	0	0	0
(R2) Moderate Density Residential	3.00	60%	1.80	1.80	0.00	0.00	0.00	0	12.00	2	36	0	0	0	0
(R3) Medium Density Residential	2.30	65%	1.50	1.50	0.00	0.00	0.00	0	15.00	2	35	0	0	0	0
(R4) High Density Residential	2.80	60%	1.68	1.68	0.00	0.00	0.00	0	25.00	3	53	0	0	0	0
(C1) Neighborhood Business	20.50	30%	6.15	0.00	4.61	1.54	0.00	0	0.00	1	0	137,887	45,962	0	0
(C2) Central Business	1.20	50%	0.60	0.00	0.30	0.30	0.00	0	0.00	2	0	7,687	7,687	0	0
(C3) General Commercial	10.80	40%	4.32	0.00	2.81	1.51	0.00	0	0.00	1.5	0	93,134	50,149	0	0
(M1) Light Industrial	0.00	40%	0.00	0.00	0.00	0.00	0.00	0	0.00	1	0	0	0	0	0
(M2) General Industrial	0.00	30%	0.00	0.00	0.00	0.00	0.00	0	0.00	1	0	0	0	0	0
(PD) Planned Development	0.30	50%	0.15	0.11	0.00	0.02	0.02	0	15.00	2	6	0	1,239	1,239	0
(PA) Professional and Administrative	0.00	40%	0.00	0.00	0.00	0.00	0.00	0	0.00	2	0	0	0	0	0
(MU) Mixed-Use	0.00	40%	0.00	0.00	0.00	0.00	0.00	0	15.00	2	0	0	0	0	0
(PF) Public Facilities	35.60	20%	7.12	0.00	0.00	0.00	0.00	7.12	0.00	1	0	0	0	0	62,029
	78.20										141	238,708	105,038	1,239	62,029

Land Use Type Name	Total Acreage	Lot Coverage	Building Footprint	Residential	Commercial Retail	Commercial Office	Industrial	Other	Maximum Du/Acre	# Stories	Residential Units	Retail Area	Office Area	Industrial Area	Other
Appian Way															
(R1) Low Density Residential	2.10	65%	1.37	1.37	0.00	0.00	0.00	0	7.00	2	15	0	0	0	0
(R2) Moderate Density Residential	0.00	60%	0.00	0.00	0.00	0.00	0.00	0	12.00	2	0	0	0	0	0
(R3) Medium Density Residential	0.00	65%	0.00	0.00	0.00	0.00	0.00	0	15.00	2	0	0	0	0	0
(R4) High Density Residential	0.40	60%	0.24	0.24	0.00	0.00	0.00	0	25.00	3	8	0	0	0	0
(C1) Neighborhood Business	14.60	30%	4.38	0.00	3.29	1.10	0.00	0	0.00	1	0	98,202	32,734	0	0
(C2) Central Business	10.90	50%	5.45	0.00	2.73	2.73	0.00	0	0.00	2	0	69,824	69,824	0	0
(C3) General Commercial	45.60	40%	18.24	0.00	11.86	6.38	0.00	0	0.00	1.5	0	393,234	211,741	0	0
(M1) Light Industrial	0.00	40%	0.00	0.00	0.00	0.00	0.00	0	0.00	1	0	0	0	0	0
(M2) General Industrial	0.00	30%	0.00	0.00	0.00	0.00	0.00	0	0.00	1	0	0	0	0	0
(PD) Planned Development	14.80	50%	7.40	5.18	0.00	1.11	1.11	0	15.00	2	222	0	48,352	48,352	0
(PA) Professional and Administrative	10.80	40%	4.32	0.00	0.00	4.32	0.00	0	0.00	2	0	0	105,798	0	0
(MU) Mixed-Use	0.00	40%	0.00	0.00	0.00	0.00	0.00	0	15.00	2	0	0	0	0	0
(PF) Public Facilities	11.50	20%	2.30	0.00	0.00	0.00	0.00	2.3	0.00	1	0	0	0	0	20,038
	110.70										244	561,260	468,449	48,352	20,038
	312.20										1,271	1,122,140	909,740	476,283	136,081

Table 5
Pinole Specific Plan Land Use Types – Specific Plan Assumptions

Land Use Type Name	Total Acreage	Lot Coverage	Building Footprint	Residential	Commercial Retail	Commercial Office	Industrial	Other	Maximum Du/Acre	# Stories	Residential Units	Retail Area	Office Area	Industrial Area	Other
San Pablo Avenue															
(LDR) Low Density Residential	0.00	65%	0.00	0.00	0.00	0.00	0.00	0.00	7.00	2	0	0	0	0	0
(MDR) Medium Density Residential	13.00	65%	8.45	8.45	0.00	0.00	0.00	0.00	15.00	2	195	0	0	0	0
(HDR) High Density Residential	0.00	75%	0.00	0.00	0.00	0.00	0.00	0.00	30.00	3	0	0	0	0	0
(VHDR) Very High Density Residential	3.08	85%	2.62	2.62	0.00	0.00	0.00	0.00	50.00	4	154	0	0	0	0
(RMU) Residential Mixed Use	15.21	85%	12.93	11.64	1.29	0.00	0.00	0.00	20.00	4	274	56,317	0	0	0
(CMU) Commercial Mixed Use	37.64	85%	31.99	16.00	9.60	6.40	0.00	0.00	15.00	4	282	418,098	278,732	0	0
(OPMU) Office Professional Mixed Use	0.00	75%	0.00	0.00	0.00	0.00	0.00	0.00	15.00	3	0	0	0	0	0
(OIMU) Office Industrial Mixed Use	32.59	30%	9.78	0.00	1.47	0.00	8.31	0.00	0.00	2	0	63,883	0	362,003	0
(P/QP/I) Public/Quasi-Public/Institutional	4.40	25%	1.10	0.00	0.00	0.00	0.00	1.10	0.00	2	0	0	0	0	47,916
(OSD) Open Space	1.20	20%	0.24	0.00	0.00	0.00	0.00	0.24	0.00	1	0	0	0	0	10,454

Land Use Type Name	Total Acreage	Lot Coverage	Building Footprint	Residential	Commercial Retail	Commercial Office	Industrial	Other	Maximum Du/Acre	# Stories	Residential Units	Retail Area	Office Area	Industrial Area	Other
	107.12										905	538,297	278,732	362,003	58,370
Opportunity Sites															
(CMU) SPA #1 - Tennent/San Pablo	0.96	85%	0.82						18.00	2	17	12,441	0	0	0
(RMU) SPA #2 - Tennent Ave.	0.69	80%	0.55						15.00	2	10	0	0	0	0
(VHDR) SPA #3 - At Madrone Ave.	3.72	80%	2.98						50.00	4	186	0	0	0	0
(VHDR) SPA #4 - At Roble and Madrone	0.00	80%	0.00						50.00	4	0	0	0	0	0
(OIMU) SPA #5 - At Pinole Shores Dr.	1.28	30%	0.38						0.00	1	0	836	2,509	13,382	0
(OIMU) SPA #6 - Sugar city	4.50	30%	1.35						0.00	1	0	2,940	8,821	47,045	0
(OIMU) SPA #7 - At Pinole Shores Dr.	1.04	30%	0.31						0.00	1	0	680	2,039	10,873	0
(OIMU) SPA #8 - At Pinole Shores Dr.	3.99	30%	1.20						0.00	1	0	2,607	7,821	41,713	0
	16.18										214	19,504	21,190	113,012	0
San Pablo Avenue Subtotal	123.30										1,119	557,801	299,921	475,015	58,370
Pinole Valley Road															
(LDR) Low Density Residential	0.00	65%	0.00	0.00	0.00	0.00	0.00	0.00	7.00	3	0	0	0	0	0
(MDR) Medium Density Residential	6.80	65%	4.42	2.21	1.33	0.88	0.00	0.00	15.00	3	102	0	0	0	0
(HDR) High Density Residential	1.70	75%	1.28	0.19	0.06	1.02	0.00	0.00	30.00	3	51	0	0	0	0
(VHDR) Very High Density Residential	0.00	85%	0.00	0.00	0.00	0.00	0.00	0.00	50.00	4	0	0	0	0	0
(RMU) Residential Mixed Use	0.00	85%	0.00	0.00	0.00	0.00	0.00	0.00	30.00	4	0	0	0	0	0
(CMU) Commercial Mixed Use	15.01	85%	12.76	0.00	0.00	0.00	0.00	12.76	15.00	4	113	166,728	111,152	0	0
(OPMU) Office Professional Mixed Use	5.04	75%	3.78	0.00	0.00	0.00	0.00	0.00	0.00	3	0	8,233	131,725	0	0
(OIMU) Office Industrial Mixed Use	0.00	30%	0.00	0.00	0.00	0.00	0.00	0.00	0.00	2	0	0	0	0	0
(P/QP/I) Public/Quasi-Public/Institutional	35.60	25%	8.90	0.00	8.90	0.00	0.00	0.00	0.00	3	0	0	0	0	387,684
(OSD) Open Space	3.40	20%	0.68	0.68	0.00	0.00	0.00	0.00	10.00	1	34	0	0	0	29,621
	67.55										300	174,961	242,877	0	417,305
Opportunity Sites	•														
(CMU) PVR #1 - At Ramona	0.45	30%	0.14						0.00	1	0	5,881	0	0	0
(HDR) PVR #2 - 2885 & 2995	1.70	75%	1.28						30.00	3	51	0	0	0	0
(OPMU) PVR #3	1.75	50%	0.88						0.00	3	0	0	38,115	0	0
(CMU) PVR #4 - At I-80	2.24	40%	0.90						0.00	4	0	3,903	35,127	0	0
(OPMU) PVR #5 - At Henry Ave.	4.51	40%	1.80						0.00	4	0	7,858	70,724	0	0
	10.65										51	17,642	143,966	0	0
PVR TOTAL	78.20										351	192,603	386,843	0	417,305

Land Use Type Name	Total Acreage	Lot Coverage	Building Footprint	Residential	Commercial Retail	Commercial Office	Industrial	Other	Maximum Du/Acre	# Stories	Residential Units	Retail Area	Office Area	Industrial Area	Other
Appian Way															
(LDR) Low Density Residential	0.00	65%	0.00	0.00	0.00	0.00	0.00	0.00	7.00	3	0	0	0	0	0
(MDR) Medium Density Residential	0.00	65%	0.00	0.00	0.00	0.00	0.00	0.00	15.00	3	0	0	0	0	0
(HDR) High Density Residential	1.90	75%	1.43	0.00	0.00	0.00	0.00	0.00	30.00	3	43	0	0	0	0
(VHDR) Very High Density Residential	0.00	85%	0.00	0.00	0.00	0.00	0.00	0.00	40.00	4	0	0	0	0	0
(RMU) Residential Mixed Use	14.90	85%	12.66	3.80	8.86	0.00	0.00	0.00	15.00	4	171	55,150	0	0	0
(CMU) Commercial Mixed Use	51.68	85%	43.92	13.18	0.00	30.75	0.00	0.00	15.00	4	329	573,996	382,664	0	0
(OPMU) Office Professional Mixed Use	8.40	75%	6.30	0.00	0.95	5.36	0.00	0.00	0.00	3	0	13,721	219,542	0	0
(OIMU) Office Industrial Mixed Use	0.00	30%	0.00	0.00	0.00	0.00	0.00	0.00	0.00	2	0	0	0	0	0
(P/QP/I) Public/Quasi-Public/Institutional	11.50	25%	2.88	0.00	0.00	0.00	0.00	0.00	0.00	3	0	0	0	0	125,235
(OSD) Open Space	0.00	20%	0.00	0.00	0.00	0.00	0.00	0.00	10.00	1	0	0	0	0	0
	88.37										543	642,867	602,206	0	125,235
Opportunity Sites											•	•			
(CMU) AW #1 - Appian 80	13.00	40%	5.20						30.00	1	156	158,558	0	0	0
(CMU/RMU) AW #2 - Doctor's Hospital	7.41	40%	2.96						30.00	3	178	0	90,378	0	0
(CMU) AW #3 - At Canyon	1.92	50%	0.96						0.00	4	0	6,273	35,545	0	0
	22.33										334	164,831	125,923	0	0
AW TOTAL	110.70										877	807,698	728,129	0	125,235
TOTAL	312.20										2,346	1,558,102	1,414,894	475,015	600,910